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The Dar es Salaam City Council



CONSULTANCY SERVICES FOR THE CONCEPTUAL DESIGN OF A LONG TERM INTEGRATED DAR ES SALAAM BRT SYSTEM AND DETAILED DESIGN FOR THE INITIAL CORRIDOR

ANNEX VOLUME 8 IMPACT ANALYSIS AND MITIGATION CHAPTER 8.5 Resettlement Action Plan – Phase 1 – Part B

> Dar es Salaam November, 2007 Rev. 02







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ACRONYMS AND ABBREVIATIONS

BRT	Bus Rapid Transit
CBD	Central Business District
CBO	Community Based Organizations
DAI	Direct Area of Influence
DART	Dar Es Salaam Rapid Transit
DCC	Dar Es Salaam City Council
DSM ·	Dar Es Salaam
ESIA	Environmental and Social Impact Assessment
GOT	Government of Tanzania
PAP	Property Affected Peoples
PIC	Public Information Center
PMU	DART Project Management Unit
RAP	Resettlement Action Plan
ROW	Rights-of-Way
RPF	Resettlement Policy Framework
RPIT	Resettlement Planning and Implementation Team
TANROADS	Tanzania National Roads Agency
ToR	Terms of Reference
TZS:	Tanzanian Shillings
USD:	United States Dollars

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Dar es Salaam BRT System Phase I – Part B

Resettlement Action Plan (RAP)

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1.0 Introduction

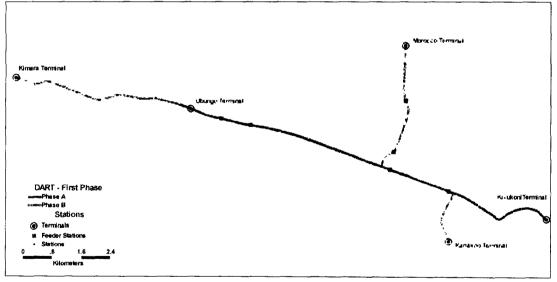
1.1

The Resettlement Action Plan of Phase I – Part B in the Context of the DART Project's Resettlement Policy Framework

This is the Resettlement Action Plan (RAP) for Phase I – Part B of the DART Project in Dar es Salaam and has been prepared according to the Resettlement Policy Framework (RPF) issued by the Dar es Salaam City Council in February 2007 (Rev. 01). It should be analyzed in conjunction with the RPF.

As established in the RPF, two (02) Resettlement Action Plans will be prepared for Phase I. Part B herein refers to the 9.90 Km. segment between Ubungo Terminal and Kimara along Morogoro Road, and to the branches along Msimbazi Road leading to Kariakoo Terminal, and Kawawa Road leading to Morocco Terminal. **Figure 1.1.a** below shows the location of DART Phase I – Parts A and B.

Figure 1.1.a Location of DART Phase I – Parts A and B



This RAP closely follows the guidelines established in the RPF. It is based on a Socio-Economic Survey and a Property Survey which were concluded in April 2007. This month has been adopted as the cut-off date for valuation purposes.

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The Property Survey was based on application of the Inspection and Valuation Questionnaire included in Annex 01 of the RPF. The Socio-Economic Survey was carried out through application of the Residential Resettlement Questionnaire and Commercial Activities Questionnaire (Annex 02 of the RPF).

A total of 191 properties will be affected for implementation of DART Phase I – Part B. The vast majority of these will be totally affected (172). Thus resettlement impacts resulting from implementation of Part B will be significantly greater than those for Part A.

Project Affected Peoples (PAP) include 459 entities (families, businesses, other). This includes 14 non-occupant owners of buildings, 228 resident families (including 135 owner-occupiers and 93 tenant families occupying a total of 146 properties with residential use, of which 80 exclusively residential and 66 mixed use), 212 businesses (including 63 businesses in owner-occupied premises and 149 businesses in rented premises, occupying 106 properties, 40 of which with exclusively commercial use) and 5 occupants whose activities were not identified, since their properties will be only marginally affected (fences and walls).

Of the 19 partially affected properties, ten (10) will suffer interference limited to fences or boundary walls. Current use in five (05) of these was not identified in the survey.

The 228 resident families include an estimated 1,465 people in total. The 212 businesses employ an estimated 496 persons (besides the business owners).

Most expropriation for Phase I – Part B will take place along Morogoro Road and at station locations along the Msimbazi and Kawawa Road branches. Significant relocation impacts will occur at Kariakoo Terminal, affecting several employee houses in a single property belonging to the TRC railway company.

Expropriation Plans showing project intervention limits and affected properties are included in ANNEX 04. Other back-up documentation for this RAP includes the Proposed Compensation Schedule (ANNEX 01), and the complete data bank resulting from application of the Property Inspection and Valuation Questionnaire (ANNEX 02) and the Socioeconomic Questionnaires (ANNEX 03).

The Proposed Compensation Schedule was prepared by the RAP team's certified valuer and will be submitted for approval by the City's Chief Valuer.

Summary tables with main Compensation Schedule total values per Ward and Municipality and per type of allowance are included in this RAP. Similarly, a summary budget for Complementary Compensation and Assistance measures as established in Section 5.2 of the RPF is presented, as well as a consolidated budget for DART Phase I – Part B RAP implementation.

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All figures are indicated in Tanzanian schillings (TZS) and main totals and sub-totals are also presented in US dollars. A 1.200 TZS per dollar exchange rate was adopted.

As will be noted in some sections of this RAP, a great deal of effort was spent with verification of data consistency. Not all data required in the questionnaires was completed in all cases. Some questionnaires were only partially considered due to inconsistencies between data in the Property Survey and the Socioeconomic Survey. All these cases are pointed out as pertinent. These shortcomings, however, are minor and do not affect the general results. Furthermore, all PAP with grievances affecting valuation and/or eligibility will have the opportunity to request revisions during this RAP's disclosure procedures (see Section 6.3).

Prior to this RAP's conclusion, the Environmental and Social Impact Assessment (ESIA) on the project was completed. This included a detailed Social Impact Assessment contemplating both construction and operation phases. Description of all expected socioeconomic impacts as described in the ESIA (and including impacts associated to resettlement) is included in **ANNEX 05**.

1.2

Socioeconomic Conditions along Phase I - Part B Alignment

Socio-Demographic Characteristics

The immediate DART impact area for Phase I - Parts A and B (DAI) covers 18 Wards in Kinondoni and Ilala Municipalities. The socio-demographic characteristics of the population in this area were analyzed on the basis of the 2002 Population and Housing Census and on field survey work concluded in April 2007. The total population of the project area was 479,219 in 2002. Given the annual growth rate of 4.3%, it can be estimated that this number is near 590,000 in 2007.

Table 1.2.a below consolidates the 2002 population in the Sub-Wards in the DAI of Phase I – Part B only. As can be seen, total 2002 population in this area was 196,399 inhabitants, equivalent to 41% of the population in the DAI for Phase I – Parts A and B. Most of this population is within Kinondoni Municipality.

The fertility rate for Ilala and Kinondoni Municipalities is 6.5%. Population is predominantly young, with 32% of people under the age of 15 years and an age dependency ratio of 51.24%.

				Population	1
District	Ward	Sub-Wards	Male	Female	Total
	Jangwani	1	3,728	3,322	7,050
Ilala	Gerezani	1	2,048	2,069	4.117
	Kariakoo	2	4,986	4,322	9,308
Total					20,475
Kinondoni	Magomeni	2	5,932	5,746	11,678
	Kimara	1	33,053	24,043	66,288
	Kinondoni	2	10,628	10,861	21,404
	Hananasifu	3	16,040	15,983	32 023
	Mwananyamala	3	21,946	22585	44,531
Total					175,924
Grand Total					196,399

Table 1.2.aTotal Population by Wards – Phase I – Part B

Source: United Republic of Tanzania 2002 Population and Housing Census.

The immediate DART impact area had a total of almost 102,000 households in 2002, with an average household size of 4.7. In the Ilala section, 27% of the households are female-headed while in Kinondoni 30% are female-headed (DCC, 2006). The high percentage of female-headed households has implications for poverty reduction since women have lower incomes compared to men, partly because of low educational attainment levels and limited access to other social and economic resources.

The two dominant family structures in the project area are monogamous and polygamous types, basically reflecting the marriage systems that exist in Tanzania. Family structures are also either extended or nuclear. A large proportion of the families in the project area are extended since their members include more than husband, wife and children. These extended families generally include other relatives and children living in households other than their own are very common. This is an accepted cultural practice and norm in the project area and in most other urban centers in Tanzania. These children are often attending schools, helping with household chores or working. Consequently, the large average household size in the project area is not only a reflection of high fertility rates but also the result of the extended nature of the households.

Survey work in the project impact area identified high ethnic diversity. There is no dominant ethnic group and Tanzanians of all racial or ethnic backgrounds are found. Among these are the indigenous people of the coastal area of Tanzania, the Wazaramo. Others ethnic groups include the Wachaga, Wasukuma, Wapare, Wahaya and many others. Asian ethnic groups (mainly Indian) have clusters of residences concentrated in Upanga and Kisutu Wards. The Jangwani Ward in Ilala district is home to a wide variety of Arabs, Indians and indigenous ethnic groups of Tanzania.

However, it must be pointed out that in spite of this diversity of origins, the majority of people in the DART impact area were born and raised in Tanzania.

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Economic activity in the Direct Area of Influence (DAI) is characterized by high levels of informality. Trade and commerce is the dominant economic activity. Medium to large-scale businesses are common in sections of Ilala Municipality, while small to medium-scale businesses of all types occur in the Kinondoni section of the DAI.

Small-scale businesses which are dominant include street vendors and small business structures built as front extensions of existing houses. This ribbon-like pattern of occupation is present particularly along significant portions of Morogoro Road segments of Phase I – Part B and will be significantly affected by the project.

Local Urban Structure

Phase I – Part B includes the extension of Phase I – Part A along Morogoro Road, and the two branches along Msimbazi Street and Kawawa Road (see Figure 1.1.a).

The Morogoro Road extension will cover the segment between Ubungo Terminal and Kimara. Along this segment, Morogoro Road is a double lane (2×2) avenue that serves the outskirts of the city, linking sub-centers that are located linearly along it. It also serves the upcountry areas of Kibaha, Mlandizi, Chalinze and beyond. Thus, the avenue intercepts very varied urban landscapes, going through densely occupied commercial centers to residential areas and even some unoccupied areas, particularly when approaching Kimara.

Ribbon-like development fronts the right-of-way in the densely occupied segments and this kind of occupation is largely the reason why most properties affected in Phase I – Part B are totally affected.

Msimbazi Street is a single lane two-way avenue which is among the most densely congested traffic roads in the city. It extends to the Kariakoo area and receives traffic from several arterial roads. All the buses plying into the Kariakoo market and its surroundings use the street. Hence, it is busy all day as it receives traffic from early in the morning and the same traffic flows out in the evenings. Adjacent land-use is consolidated and high density with very intense commercial activity.

Whereas expropriation at Msimbazi Street branch will be quite limited as a result of project adjustments, significant impacts to commercial activity may result from limited access during construction and reduced street parking space once DART operation begins. At Kariakoo Terminal, relocation will affect worker's housing within the property of the TRC railway company.

Kawawa Road is a double lane tarmac road that functions as a ring road that connects Morogoro and Bagamoyo roads passing through the Kinondoni areas and ending up at its junction with Bagamoyo Road. It has public transport running along it that serves either Mwenge – Mtongani or Kawe – Mbagala. It also carries traffic from Tegeta to Kariakoo.

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Generally, the traffic flow along this road is moderate, but it is poised to increase in the future as the redevelopment of the old and condemned city houses in Kinondoni area is implemented. High-rise apartments will replace the old structures thus increasing population and traffic.

Urban structure along the Kawawa Road branch is a predominantly rectangular urban pattern with medium density mixed-use occupation. Irregular urban pattern occurs mostly along the edges of the Sinza River valley which is intercepted perpendicularly. Major highlights are the open spaces near Morogoro Road and at Biafra Square, as well as the Sinza River valley.

Kawawa Road has a relatively wide right-of-way and accommodates the DART project with a less tight geometry than at Msimbazi Street branch. Expropriation here will concentrate at station locations.

Land Use

Along Morogoro Road between Ubungo and Kimara, predominant land use is a mix of residential, agriculture and livestock breeding areas. Commercial activities form ribbon developments fronting the right-of-way along commercial clusters near the main intersections and markets.

The Kariakoo sub-center, at the end of the Msimbazi corridor has more intense commercial activity and larger establishments than those that occur elsewhere along Phase I - Part B.

Land use along Kawawa Road is also dominated by commercial activities concentrated on some segments, with relatively homogeneous residential areas in between.

In order to better analyze current abutting land-use along the Phase I – Part B corridor, it was divided into three (03) segments, according to their particular characteristics, as follows:

Segment	Streets/Roads	
11	Morogoro Rd – from Ubungo to Kimara	
2	Kawawa Rd	
3	Msimbazi Street	

Segment 1

This segment begins at the Nelson Mandela/Sam Nujoma Road Junction, and goes up to Kimara. At this point, the general characteristics of land use change as the area contains mostly one story residential buildings and sparse commerce along the road. This commerce is not accessible directly from the road (only some petrol stations and few other services), and to gain access to the buildings it is necessary to take lateral streets.

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In this area there is still a significant number of street vendors, most of them concentrated next to bus stops and to commercial areas.

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The most significant points or buildings on this segment are:

- TANESCO Power Station and Offices
- Daladala Terminal
- Institutional Buildings close to the junction with Nelson Mandela/Sam Nujoma

Segment 2

Msimbazi Street is today a very congested axis, especially because of the concentration of daladala traffic. There are many tall buildings, some of them under construction, and there is a great concentration of commercial activities, both on the ground and upper floors. This segment is mainly characterized by the sales of spare parts and electronic articles, but there are also food stores, and it is possible to find many street vendors, fixed or transient, along the street. It is also near the Kariakoo Market, which will not be directly affected, but has an important influence in this area.

The most significant points or buildings in this segment are:

- Railway Crossing
- Scandinavian Bus Depot (after the Railway)

Segment 3

This segment comprises the Kawawa Road branch, starting at Morogoro Junction and ending at Ali Hassan/New Bagamoyo Road junction. The main general characteristics of this segment are the concentration of small commercial sites close to the road, and the existence of a few religious centers.

The road crosses the Sinza River Valley and, at this point, there is a great concentration of residential buildings, though most of them do not directly face the road.

There are also some open areas, one near the junction to Morogoro, that is used as a recreation park, and another one at Biafra, in front of the Biafra School Building, that is also used by the population for leisure activities.

The most significant points or buildings on this stretch are:

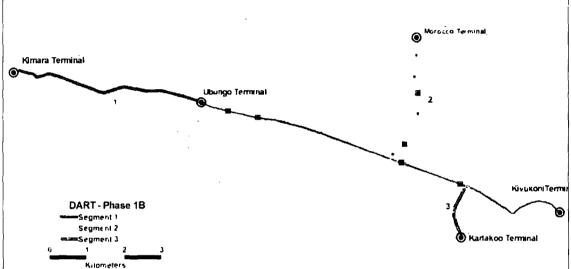
- Magomeni Health Center
- Magomeni Roman Catholic Church

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- Open University of Tanzania
- Daladala Terminal

Figure 1.2.a below provides indication of the limits of each one of the three segments analyzed above.





Historical, Cultural and Archeological Features

No historical or cultural buildings will be directly affected by Phase I – Part B. Furthermore, unlike Part A where many historical buildings front the project's alignment, this does not occur along Part B. Similarly, there are no records of archeological remains in the vicinity of DART Phase I – Part B alignments.

Utilities

Utility networks and service coverage along the Phase I – Part B corridor include 100% coverage of water supply grids. The sewer collection grid covers only the Msimbazi Street branch. There is no sewer collection grid along Kawawa Road branch and along Morogoro Road between Ubungo and Kimara.

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1.3 Consultation during RAP Preparation

Consultation and disclosure relative to DART Phase I - Part B has taken place in an effective way during RAP preparation. In this context, during execution of both the property survey and application of socioeconomic questionnaires as part of this RAP's planning process, an effective exchange of information with affected parties took place. As established in the RPF, the certified valuer and his assistants, as well as the interviewers that applied the Socio-Economic Questionnaire, ensured that:

- All PAP whose property will be directly affected were served notice as required by law and became aware of the fact and of the extent of necessary expropriation (partial or total);
- A preliminary understanding of valuation and compensation criteria was conveyed, as well as preliminary information on arrangements for payment of compensation, allowances and assistance;
- The possibility for grievance and the establishment of a legally required Grievance Committee was informed;
- The cut-off date for valuation purposes was made clear and all PAP became conscious that no speculative occupation of the future right-of-way will result in compensation and that any ongoing construction should stop;
- All PAP were informed of the likely timing of resettlement is conveyed.

Thus, it can be stated that at the time of submittal of this Resettlement Action Plan, all PAP have a preliminary understanding of the resettlement process, as well as of the main entitlements and respective eligibility criteria.

Further to the surveyed PAP, several local authorities were consulted during the RAP preparation process. As pertinent to Phase I – Part B, these included:

Ms. Elizaberth Lucas – Ward Executive Officer – Kinondoni Ward
Ms. Mary Kimatali – Magomeni Ward
Mr. Amani H. Kafugugu - Ward Executive Officer - Magomeni
Ms. Elizabeth E. Mbalale - Ward Executive Officer - Ndugumbi
Mr. Ally Mtambo - Ward Executive Officer - Kimara Ward
Ms. Magreth Mkandawile - Ward Executive Officer - Ubungo Ward
Mr. Zuhura Almasi - Ward Executive Officer - Hananasif Ward
Mr. Ratifa Ramadhani – Mwananyamala Ward
Mr. Tarimo – Ward Executive Officer – Kariakoo Ward
Mr. Faraja Machumu – Upanga West Ward
Mr. Bahati Shangama – Ward Executive Officer – Gerezani Ward
Mr. Suleman Anastasi - Transport Economist - DART

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Furthermore, extensive consultation was conducted during ESIA preparation which progressed parallel to the RAP preparation process. This included a wide variety of stakeholders, including public entities with direct or indirect involvement with the project, as well as residents and businesses located along the corridor, including those that will be directly affected by expropriation.

The main public entities / public officers consulted during ESIA preparation include:

- Vice President's Office (VPO)
- Prime Minister' Office (PMO)
- National Environment Management Council (NEMC)
- Tanzania Electricity Supply Company (TANESCO)
- Dar Es Salaam Water Supply and Sanitation Company (DAWASCO)
- Tanzania Telephone Communication Limited (TTCL)
- Tanzania Railways Corporation (TRC)
- Dar Es Salaam City Council (DCC)
- The Municipalities of Temeke, Kinondoni and Ilala
- Ward Executive Officers along the alignment

In addition to public entity consultation, several local experts and university professors specialized in transportation planning, urban planning, environmental management and other pertinent skills were contacted and contributed with their views toward adjustment of the DART project to better meet local needs.

Further to consultation described above, additional consultation will take place during the RAP implementation process, as described in Section 6.1 herein.

1.4 Legal Framework

<u>1.4.1</u> Land Tenure and Legal Status

The legal framework for resettlement is first and foremost based on the following articles of the Constitution of the United Republic of Tanzania (1977):

• Article 24 (1): Subject to provisions of the relevant laws of the land, every person is entitled to own property, and has a right to the protection of his property held in accordance with law.

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• Article 24 (2): It shall be unlawful for any person to be deprived of property for the purposes of nationalization or any other purposes without the authority of law which makes provision for fair and adequate compensation.

Therefore, payment of compensation is both a legal and constitutional right under Article 24 of the Constitution.

Further to the Constitution, the following are the main legal instruments affecting property acquisition and resettlement in Tanzania:

- Land Acquisition Act N^o 47 of 1967.
- Land Act N° 4 of 1999.
- Village Land Act N^o 5 of 1999.
- Complementary regulations issued in 2001.

The legal basis for compensation / acquisition of private properties or right-of-ways for infrastructure and other development projects is contained mostly in Land Act N^o 4 of 1999, Village Land Act N^o 5 of 1999 and Land Acquisition Act N^o 47 of 1967.

Land Act N° 4 of 1999 is basically about land tenure and land rights, but also addresses issues of compulsory acquisition, mortgages and regularization of unplanned areas.

Land Act N° 4 of 1999 and Village Land Act N° 5 of 1999 and their respective Land and Village Land Regulations, including in particular Supplement N_{c}° 16 of 2001, govern the procedures for assessment and payment of compensation for land and unexhausted improvements, loss of accommodation, loss of profit, disturbance and transport.

Section 3 (1) (a) of Land Act N° 4 of 1999 establishes that all Land in Tanzania is public land. Therefore, private land as such does not exist. Occupancy of Land by individuals is granted legally regular status through a "grant of a right to occupancy".

The Government leases Land under the Right of Occupancy system for varying periods at economic ground rents. Such rent is reviewed periodically (normally at 10 yearly intervals). The ground rent paid in respect to government Right of Occupancy is the Land Rent levied under section 7(2) of the Land Ordinance. All urban land in Tanzania is liable to a payment of Rates in lieu of Land Rent as per the Urban Authorities (Rating) Act of 1983.

Further to Public Land, the Land Act recognizes "Village Land" and other forms of customary tenure.

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Section 3 (1) (f) of Land Act N^{\circ} 4 of 1999 provides that "interest in Land has value and that value must be taken into consideration in any transaction affecting Land". Therefore the, acquisition of Land will include compensating owners for bare land in addition to unexhausted improvements.

Sub-paragraph (g) of the same section provides that full, fair and prompt compensation has to be paid to any person whose "right of occupancy or recognized long-standing occupation or customary use of land" is affected or interfered with to their detriment. Thus, compensation for land in project-affected areas (which are urbanized and over which no customary forms of tenure prevail), is forthcoming only where its occupancy has been legally granted or is long-standing.

However, in recognition of the existence of informal types of tenure in urban areas, the Land Act provides for the validation of interests in land other than a granted right of occupancy (s. 53-55). In order to address the question of many people acquiring and occupying urban land otherwise than through a granted right of occupancy, the act introduces the concept of a residential license as a derivative right (section 23 of the Act). Any person who at the commencement of the act (1999) has occupied land in an urban area or peri-urban area as his home for no less than three years, without any officially granted right of occupancy, can receive a "residential license" and is entitled for compensation of land should it be required for a public purpose. Residential licenses have limited validity (maximum of two years) and need to be continuously renewed.

1.4.2 Property Valuation Norms

Valuation Methods are established in Land Act Nº 4 of 1999.

<u>Land</u>

This type of compensation applies to all affected households that hold officially granted rights of occupancy or alternatively possess a residential license. Compensation is forthcoming regardless of whether there is need for resettlement or the affected party opts to rearrange within the remaining plot.

In accordance with section 3 of the 2001 Regulations of Land Act N^{0} 4 of 1999 the basis for assessment of the value of lands affected by the project is the market value of the respective land.

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Buildings

In the case of buildings, local practice in Tanzania adopts estimate of surrogate market value through Replacement Cost Methods. This method establishes the price on the basis of the estimated cost of erecting a similar unit at today's price. The observed condition of the building is taken into account and is used as a basis to allow depreciation to be discounted from the Replacement Cost figure in order to establish what would be the likely price a person may be prepared to pay for the subject property if given opportunity to buy.

The "Depreciated Replacement Cost" thus obtained is considered to be equal to "Market Value".

In cases where more than 50% of a property is affected, compulsory expropriation of 100% proceeds and valuation needs to proceed accordingly.

1.4.3 Other Legally Established Indemnification and/or Compensation

Regulations of Land Act N^o 4 of 1999, made under Section 179 of the Land Regulations which became operational in May 2001, contemplate several complementary compensation allowances for which expropriated parties are eligible. These include:

- Disturbance allowance;
- Transport allowance;
- Loss of profit or accommodation allowance;
- Accommodation allowance.

Disturbance Allowance is payable as a percentage of property valuation in compliance to the provisions of Act N° 4 of 1999. The percentage is the average commercial bank rates offered on fixed deposits during 1 year.

In accordance with Section 11 of the Regulations of Land Act N° 4 of 1999, a transport allowance is paid to all project-affected persons that will need to be displaced. The payment is intended to meet transport cost incurred by property owners. It is specifically noted that valuers may apply this compensation to all properties, i.e. to the vast majority of those who will not be resettled. It may also be applied to tenants. Transport allowance is computed on the basis of market rates for transportation services within the project area, considering the average cost of transporting 12 tons over a distance of 20 Km.

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In accordance with Section 9 of the 2001 Regulations of Land Act N^o 4 of 1999, the net monthly profit obtained from businesses associated with the affected properties needs to be assessed. Audited accounts need to be analyzed as proof of declared profits. In the absence of audited accounts, valuers can establish loss of profit compensation on the basis of official guidelines. In this case, the amount allowed by valuers does not exceed one million shillings per annum. Compensation is forthcoming on the basis of estimated loss of profit over a 36 month period. This calculation can apply also to businesses that are only temporarily affected during the project's construction phase. This compensation will be made to all businesses regardless of the status (owner or tenant) of the affected building. However, in the case of businesses that occupy own premises, accommodation and disturbance allowance is paid and this is understood to incorporate loss of profit.

As long as the respective business is affected by the project, the loss of profit shall be exclusively and directly paid to the person who loses his/her business premises.

Accommodation allowance applies to all structures or portions of structures that are to be demolished, regardless of whether resettlement is required or not. This allowance is meant to compensate affected peoples for the costs incurred during the period when construction of new structures is under way. In accordance with Section 8 of the 2001 Regulations of Land Act N^o 4 of 1999, accommodation allowance is calculated on the basis of monthly rent multiplied by 36 months. Monthly rent is equivalent to the market rental value of the affected part of the property per month.

1.4.4

Comparison with WB OP 4.12 - Gap Analysis

Based on review of pertinent Tanzanian legislation (Sections 3.1 through 3.3) and criteria adopted in the World Bank's OP 4.12, the following main gaps have been identified:

1. Land indemnification

Under Tanzanian law, all land belongs to the government and will be compensated only in cases where its occupation has been officially granted in some way or where a residential license has been obtained. Thus, some properties that are irregularly occupied will only be compensated for affected structures (unmovable assets) at replacement cost. In cases where the affected family or business needs to be relocated, lack of indemnification for land at market value may mean that an equivalent alternative may not be purchased with the amount of compensation to be received. Whereas World Bank policy does not require compensation for illegal land, it does require that affected peoples end up in a condition which is at least equivalent if not better than the original situation. One frequent solution to attain this is implementation of a Resettlement Project, where housing units will be

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produced and delivered at a cost that exceeds the value of compensation that eligible PAPs are entitled to receive. In other cases, where quantity or geographical dispersion of PAPs in his situation make this kind of collective solution unpractical, some form of bonus or complementation of the compensations and/or allowances foreseen in Tanzanian law may be necessary in order to comply with OP 4.12.

2. Loss of profit compensation for businesses to be relocated

Under Tanzanian law only businesses occupying rented premises will be compensated for loss of profit. This compensation is in fact quite generous, and allows for 36 months of net profit compensation, based on verification of audited accounting records. Where these records do not exist, a fixed assumption on the value of profit can be adopted.

However, no loss of profit compensation is foreseen in Tanzanian law in the case of displaced businesses occupying premises that belong to the owner of the business. Whereas it may be argued that this property owner is already receiving disturbance allowance (4% of the market value of affected property) and accommodation allowance (36 months of market rental value), lost profit can represent a value larger than these allowances. Under the principles of OP 4.12, loss of means of livelihood should be compensated separately from loss of property.

3. Lost of rental income

Compensation of non-occupant owners for lost rental income is meant to be covered by accommodation allowance. However, Tanzanian law is not clear as to whether or not the accommodation allowance is to be paid when only part of a structure is expropriated and the owner (or tenant) continues to live on the remaining part of the property. In practice in such cases no accommodation allowance is paid.

4. Compensation for additional living costs

Disturbance and accommodation allowances should jointly compensate for additional costs incurred during the transition to a new location. However, there may be some living costs which will be permanently altered as a result of resettlement. These include transportation costs to work and school, as well as onsite costs of living, such as rent, water and sewer collection fees, garbage collection fees, energy bills, land rent and property tax. Where such additional costs can be demonstrated to be a result of lack of option (as opposed to preference for the chosen new place of residence), some form of compensation or assistance should be forthcoming.

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1.5 Institutional Framework

The institutional framework affecting resettlement in the DART Project will involve the following actors / entities:

- The Dar es Salaam City Council Project Management Unit (PMU),
- The Resettlement Plan Implementation Team (RPIT),
- The resettlement consultants,
- The Grievance Committee,
- The Chief Government Valuer in the Ministry of Lands, Housing and Human Settlement Development,
- Municipal Valuers,
- Authorized Municipal Land Officers,
- Ward Executive Officers,
- District Commissioners,
- Regional Commissioner.

The PMU is the main executive agency responsible for the DART Project's implementation. It will centralize project management functions and this will include coordination of the RAP implementation process. In order to do this, it will create a Resettlement Planning Implementation Team (RPIT) that will execute key activities, including assistance to PAPs and supervision of other parties involved in the process. Responsibilities of the RPIT are detailed in Section 7.0.

Resettlement consultants were hired by PMU in order to conduct social and property surveys and to assist with Resettlement Policy Framework and RAP preparation.

The PMU, with support of the RPIT and consultants as seen fit, will establish PAP eligibility for each one of the Complementary Compensation Entitlements established in the Resettlement Policy Framework; i.e. assistance and/or compensation in excess of that which is provided for under the terms of Tanzanian Law (see Section 4.2). The Grievance Committee to be created as part of the RAP implementation process will also participate of this process, to the extent that it will analyze claims for complementary compensation and may recommend that they be attended.

The main steps for establishment of the entitlements that each PAP will be eligible for under the terms of Tanzanian Law (section 4.1) are the following:

1. Property Identification and Orientation meetings with key Personnel and the affected persons.

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- 2. Valuation of properties by the Consultant in collaboration with the Municipal Valuers.
- 3. Preparation of Resettlement Action Plan (RAP) by the Consultant.
- 4. Joint review of Valuation Report by Municipal Valuers and the Consultant. At this stage Municipal Valuers also Prepare Compensation Schedule.
- 5. Valuation report approval by the Chief Government Valuer in the Ministry of Lands, Housing and Human Settlement Development.
- 6. Submittal, by the Chief Valuer of the approved Valuation Report to Municipal Valuers.
- 7. Approval of the Compensation Schedule by the Authorised Land Officers and Ward Executive Officers.
- 8. Submittal of the Compensation Schedule to the District Commissioner for Endorsement.
- 9. Compensation Schedule forwarded to the Regional Commissioner for endorsement.
- 10. Submittal of the endorsed Compensation Schedule to the authorized Municipal Land Officer.
- 11. The Authorized Land Officers disclose the Compensation Schedule to the Ward Executive Officers (WEO) for display to the public and prepare the affected to receive compensation.
- 12. The Grievance Committee is created.
- 13. Payment of compensation as per approved schedule.
- 14. Claims are received, analyzed and decided upon by the Grievance Committee s per the procedure outlined in Section 6.2.

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2.0 Summary of Property Survey Results

2.1 Affected Property Statistics by Type

Table 2.1.a below shows the composition of affected properties by type of land-use, with indication of those that will be totally vs. partially affected. As can be seen, 191 buildings will be affected, of which 172 (90.1%) will be totally affected. 146 buildings have residential use, where 80 are exclusively residential and the other 66 are mixed-use buildings having some kind of commercial activity.

Only 40 buildings (20% of the total) are exclusively commercial. However, considering commercial uses in mixed-use buildings, it can be concluded that 55.5% of affected buildings (40 + 66) have some kind of commercial activity.

Use Type	7	Fotal	Partia	lly Affected		Fotally Affected
Residential	80	40%	4	2.1%	76	39.8%
Commercial	40	20%	10	5.2%	30	15.7%
Mixed use	66	35%	0	0,0%	66	34.5%
Other	5	5%	5	2.6%	1	0.1%
Total	191	100%	19*	9.9%	172	90.1%

Table 2.1.aAffected Properties by Type of Land Use

NOTE (*): 10 of these have only fences or boundary walls affected

Most of the buildings to be partially affected (10 of 19) have exclusively commercial use. Another four (04) partially affected buildings are residential. Current use in the remaining five (05) was not identified during the survey. However, in all these cases only fences or boundary walls will be affected.

Information regarding public utilities and other infrastructure servicing the buildings to be affected is summarized in **Table 2.1.b**. As can be seen, a significant percentage of interviewees did not respond to all the information requested, in particular with regards to sewer collection services. This is probably because many interviewees are tenants and/or partial occupants of buildings and are thus not necessarily involved with payment of utility bills.

Notwithstanding, it is clear that at least 67% of affected buildings are fully serviced by both water (DAWASCO) and sewer (DAWASA) systems.

A much larger percentage is served by the electricity grid. Only seven (07) properties reported having no connection to the grid.

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Another aspect worth highlighting with regards to infrastructure is that 65.4% of affected properties in Part B are fronting on non-paved roads.

Infrastructure			
	No Information	63	33,0%
	DAWASA	126	66,0%
Sewage treatment System	Septic Well	2	1,0%
	Ditch	0	0,0%
	River	0	0,0%
	No Information	63	33,0%
	DAWASCO	128	67,0%
Water	Well	0	0,0%
	Other	0	0,0%
	Not Informed	18	9,4%
Eletrical Network	Yes	166	86,9%
	No	7	3,7%
	Not Informed	0	0,0%
Is energy metered?	YES	166	100,0%
	No	0	0,0%
	Not Informed	22	11,5%
Pavement Type in front of the	Asphalt	44	23,0%
property	Earth	125	65,4%
	Other	0	0,0%

Table 2.1.bInfrastructure of Affected Buildings

3.0 Summary of Social-Economic Survey Results

3.1 Categorization of Surveyed Project Affected People (PAP)

Project Affected People have been classified in this RAP according to the typology established in Section 4.0 of the Resettlement Policy Framework. A total of 459 "entities" will be affected in Phase I – Part B, as shown in **Table 3.1.a** below. As can be seen, this table includes indication of whether affected PAP will receive indemnification for land as well as buildings under the terms of Tanzanian law. This information is necessary for establishing eligibility for complementary compensation measures defined in the RPF.

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Table 3.1.a

General Classification of Project Affected People

PAP Category		Total	%
Non-occupant owner of partially affected property	Will not receive compensation for land	3	0.7%
Total		3	0.7%
Non-occupant owner of totally	Will not receive compensation for land	4	0.9%
affected property	Will receive compensation for land	7	1.5%
Total		11	2.4%
Owner occupier of residence of	Will not receive compensation for land	1	0.2%
partially affected property	Will receive compensation for land	3	0.7%
Fotal		4	0.9%
Owner occupier of residence of	Will not receive compensation for land	22	4.8%
totally affected property	Will receive compensation for land	109	23.7%
Total		131	28.5%
Residential tenants of totally affected property	Will not receive compensation for land	93	20.3%
Total	93	20.3%	
Owner of business in own premises of partially affected	Will not receive compensation for land	6	1.3%
property	Will receive compensation for land	1	0.2%
Total	7	1.5%	
Owner of business in own premises of totally affected	Will not receive compensation for land	45	9.8%
property	Will receive compensation for land	11	0.4%
Total		56	12.2%
Owner of business in rented premises of partially affected property	Will not receive compensation for land	5	1.1%
Total		5	1.1%
Owner of business in rented premises of totally affected property	Will not receive compensation for land	144	31.4%
Total		144	31.4%
Uses not identified that will be	Will not receive compensation for land	1	0.2%
partially affected	Will receive compensation for land	4	0.9%
Total		5	1.1%
Grand Total	459	100.0%	

Table 3.1.a includes 5 PAP classified as "other". This includes properties that will be marginally affected (only walls or fences) and applicable compensation will be limited to indemnification of land and/or improvements, without implying in relocation or even re-installation within the same plot. Thus, a Socioeconomic Questionnaire was not applied in these cases, and data included in the Inspection & Valuation Questionnaire (Property

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Survey) did not always identify type of use of remaining building in the plot. In effect, as seen in **Table 2.1.a**, there are 10 properties that will be marginally affected in this way. In the other five cases, use of the remaining building was registered in the Inspection & Valuation Questionnaire and hence these PAP have been classified accordingly in **Table 3.1.a**.

Table 3.1.b consolidates data on the number of affected families in DART Phase I – Part B. As can be seen, a total of 228 families will be affected, 224 of which in totally affected properties and which will therefore need to relocate. The 228 families occupy a total of 146 buildings, indicating an average occupancy of 1.56 families per unit.

Table 3.1.c consolidates data on the number of family members in each one of the families to be affected. As noted, this information was retrieved in 180 of 228 cases.

On the basis of the families on which information was obtained, average family size is 7.9 dwellers per family in the case of owner-occupiers, and 4.28 dwellers per family in the case of tenant families. **Table 3.1.c** estimates total number of dwellers in the 228 residential units to be affected, by extrapolating on the basis of the average number of persons per family verified for the 180 families that supplied data. As can be seen, total population of affected families is estimated at 1,465 persons, including 1,067 in owner-occupier families, and 398 persons in tenant families. All tenant families will need to relocate. This is also the case with all but four (04) of the owner-occupier families.

Table 3.1.b

Number of Dwellers in Affected Residential Units as Reported in the Social-Economic Residential Questionnaire

Affected Residential Units	Total
Owner-occupant of totally affected property	131
Owner-occupant of partially affected property	4
Tenant of totally affected property	93
Tenant of totally partially property	0
Grand Total - Affected Units	228
NOTE: This number refers to affected units. There are 228 residential properties with residential use (80 exclusively residential and with 66 n	

Table 3.1.c	
Estimate of the Total Number of Dwellers in Residential Unit	ts

Number of Dwellers per Family Unit				
РАР		Only 180 Families with Data	Extrapolation for 228 Families in Total	
	Total Dwellers	976	1,067	
Owner-occupier of residence that will need to resettle	N° of Families Considered	123	135	
	Average Dwellers per Family	7.9	7.9	
	Total Dwellers	244	398	
Residential tenants that will relocate	N° of Families Considered	57	93	
	Average Dwellers per Family	4.28	4.28	
Grand Total		1,220	1,465	
Average		6.77	6.43	

Table 3.1.d presents data on average total family income. This information was obtained from 180 of 228 families. As can be seen, the average monthly family income of owner-occupier families that will relocate is TZS 852,060.00 equivalent to US\$ 710.05 per month (based on data from 123 families). The average monthly family income of tenant families that will relocate is TZS 801,352.94, equivalent to US\$ 667.79 per month (data from 57 tenant families).

Table 3.1.dAverage Monthly Income of Affected Families

РАР		TZS	
Owner-occupier families	Total	89.466.300,00	
Owner-occupier fammes	Average	852.060,00	
Residential tenant families	Total	40.869.000,00	
Accordential tenant families	Average	801.352,94	

NOTE: This information refers to 180 families, since data was not retrieved from 48 families.

Table 3.1.e consolidates data on the means of transport to work (for 1,220 dwellers belonging to 180 families) and **Table 3.1.f** totals transport costs per family. As can be seen, a significant part of persons in the sample did not inform the means used to go to work, or replied "none", meaning that they probably do not work on a regular basis (934 out of 1,220). This number seems reasonable, considering average family size and amount of children in school age.

Considering only the remaining 286 people in the sample, the most common means of transport to work is daladala (182 dwellers or 63.6%), followed by car or motorbike (57 dwellers or 20%). A total of 40 people reported walking to work.

PAP	Means of Transport to Work – Residential Unities	Total – dwellers	%
	Not Informed	739	75.7%
	0 = none	30	3.1%
Owner-occupier of	1 = walking	25	2.6%
residence	3 = daladala	126	12.9%
	4 = car - motorbike	49	5.0%
	5 = other	7	0.7%
Total	976	100.0%	
	Not Informed	154	63.1%
	0 = none	11	4.5%
Residential tenants	1 = walking	15	6.1%
	3 = daladala	56	23.0%
	4 = car - motorbike	8	3.3%
Total		244	100.0%
Grand Total		1,220	

 Table 3.1.e

 Means of Transport to Work of Dwellers in Affected Residential Units

Total average monthly transport costs per person for owner-occupier families is TZS 15,144.23 (US 12.62). In the case of tenant families, this monthly cost is TZS 7,662.50 (US 6.38). This represents only 1.8% of monthly family income in the case of owner-occupier families and 0.95% in the case of tenant families.

Data regarding main other monthly expenses which are dependent on location and may be affected by resettlement was gathered on owner-occupier and/or tenant families that will need to relocate and are therefore subject to alteration of these costs. As can be seen in **Table 3.1.f**, these costs, which include land rent, taxes and utility services (as well as rent in the case of tenants) can be quite significant.

In the case of owner-occupier families, these costs averaged TZS 102,278.50 per month (equivalent to US\$ 85.20) for families living in individual houses, and TZS 161,416.67 (US\$ 134.50) for families living in rooms or apartments. The weighted average monthly cost for owner-occupier families is TZS 116,702.44 (US\$ 97.25), which corresponds to 13.7% of the average monthly family income of this category of PAP.

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In the case of tenant residents that will relocate, rent and utility costs average TZS 128,500.01 per month (equivalent to US\$ 115.00) in the case of tenants in individual houses, and TZS 74,263.17 (US\$ 61.88) in the case of tenants in rented rooms or apartments. The weighted average monthly cost for tenant families is TZS 82,679.23 (US\$ 68.90), which represents 10.3% of the average monthly family income of this category of PAP.

It is worth noting that both in the case of owner-occupiers and tenants, monthly costs dependent on location represent a significantly lower percentage of total income than in the case of families to be affected along Phase I - Part A.

РАР	Type of residence:	Data	Total (TZS)	Answers	Average (TZS)
		Sum of Water + Sewer	3.534.500,00	93	38.005,38
		Sum of Garbage Collection	230.800,00	93	2.481,72
	Individual house	Sum of Energy	2.787.300,00	93	29.970,97
		Sum of Gas	2.593.000,00	93	27.881,72
		Sum of Land Rent	110.300,00	93	1.186,02
Owner-occupier		Sum of Property Tax	256.000,00	93	2.752,69
of residence		Sum of Water + Sewer	1.850.500,00	30	61.683,33
		Sum of Garbage Collection	110.000,00	30	3.666,67
	Room or apartment in individual house	Sum of Energy	1.260.000,00	30	42.000,00
	individual nouse	Sum of Gas	1.141.000,00	30	38.033,33
		Sum of Land Rent	314.000,00	30	10.466,67
		Sum of Property Tax	167.000,00	30	5.566,67
		Cost of Rent	378.000,00	9	42.000,00
		Sum of Water + Sewer	423.000,00	9	47.000,00
	Individual house	Sum of Garbage Collection	54.500,00	9	6.055,56
	Individual nouse	Sum of Energy	78.000,00	9	8.666,67
		Sum of Gas	207.500,00	9	23.055,56
Residential		Sum of Land Rent	3.000,00	9	333,33
tenants		Sum of Property Tax	12.500,00	9	1.388,89
i chanto		Cost of Rent	896.000,00	49	18.285,71
		Sum of Water + Sewer	879.500,00	49	17.948,98
	Room or apartment in individual house	Sum of Garbage Collection	199.200,00	49	4.065,31
	murviuual nouse	Sum of Energy	473.000,00	49	9.653,06
		Sum of Gas	907.100,00	49	18.512,24
		Sum of Land Rent	-	47	-

 Table 3.1.f

 Other Monthly Expenses Dependent on Location – Families to be Resettled

РАР	Type of residence:	Data	Total (TZS)	Answers	Average (TZS)
		Sum of Property Tax	272.500,00	47	5.797,87
		Cost of Rent	250.000,00	1	250.000,00
		Sum of Water + Sewer	50.000,00	1	50.000,00
		Sum of Garbage Collection	2.000,00	1	2.000,00
	Upper floor unit	Sum of Energy	30.000,00	1	30.000,00
		Sum of Gas	50.000,00	1	50.000,00
		Sum of Land Rent	-	1	-
		Sum of Property Tax	-	1	-

 Table 3.1.f - continued

 Other Monthly Expenses Dependent on Location – Families to be Resettled

With regards to affected businesses, the Socioeconomic Questionnaire for Commercial Activities was applied on 179 out of 212 affected establishments. Total number of employees in those 179 establishments reached 419 as shown in **Table 3.1.g** below. This total does not include business owners as such. 56.8% are registered employees. On average, affected businesses have 2.34 employees. Considering the total number of commercial establishments to be affected (212), the average of 2.34 employees per establishment would result in a total of 496 employees.

Table 3.1.g

Employees by Category in Affected Commercial Establishments

	Affected Comm	ercial Units	Affected Employees		
Business Category	Applied Questionnaires / Total PAP	%	Type of work contract	Total	%
Owner of business in own			Informal	1	0.2%
premises that will reinstall in same plot	4 / 7	2.2%	Registered employee	22	5.3%
Total				23	5.5%
Owner of business in rented			Informal	2	0.5%
premises that will reinstall	4 / 5	2.2%	Registered employee	10	2.4%
in same plot			Self employed	1	0.2%
Total				13	3.1%
			Informal	26	6.2%
Owner of business in own			not informed	15	3.6%
premises that will relocate	51 / 56	28.5%	other	12	2.9%
			Registered employee	49	11.7%
			Self employed	5	1.2%
Total				107	25.5%

Table 3.1.g - continued...

Employees by Category in Affected Commercial Establishments

	Affected Comm	ercial Units	Affected Employees		
Business Category	Applied Questionnaires / Total PAP	%	Type of work contract	Total	%
	120 / 144	67.0%	Informal	33	7.9%
			not informed	6	1.4%
Owner of business in rented premises that will relocate			other	61	14.6%
premises that will relocate			Registered employee	157	37.5%
			Self employed	19	4.5%
Total				276	65.9%
Grand Total	179 / 212	100.0%	Grand Total	419	100.0%

With regards to means of transport to work, **Table 3.1.h** below summarizes information for the 179 interviewed businesses. This information covers 579 persons, since it includes employers (160) as well as employees (419). As can be seen, 68.22% reach work by daladala, whereas only 8.45% does so by car or motorbike. A significant number of employees walk to work (9.67% of the sample).

Table 3.1.h Means of Transport to Work – Commercial Establishments

РАР	Means of Transport to Work - Commercial Units	Total	%
Owner of husiness in own premises that will reinstall in	l= walking	1	3,7%
Owner of business in own premises that will reinstall in same plot	3= daladala	22	81,5%
	4= car/ motorcycle	4	14,8%
Total		27	100,0%
Owner of business in rented premises that will reinstall	3= daladala	13	76,5%
in same plot	4= car/ motorcycle	4	23,5%
Total		17	100,0%
	0= none	49	31,6%
Owner of business in own premises that will relocate	l= walking	10	6,5%
s and of submote in own premises that will relocate	3= daladala	86	55,5%
	4 = car/motorcycle	10	6,5%
Total		155	100,0%
	0= none	29	7,6%
	l= walking	45	11,8%
Owner of business in rented premises that will relocate	3= daladala	274	72,1%
	4= car/ motorcycle	31	8,2%
	Not Informed	1	0,3%
Total		380	100,0%
Grand Total		579	

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As can be seen in **Table 3.1.i** below, the average monthly sales of commercial establishments varies most significantly as a function of the type of building occupied (i.e. independent commercial building vs. kiosk). Variation as a function of category of PAP is less significant, though it is important to point out that sales of businesses occupying own premises are significantly larger than those of businesses in rented premises.

With regards to average monthly profit, this pattern is not so clear. One aspect to highlight is that monthly profits of businesses in kiosks attached to buildings range between TZS 500.000,00 and 550.000,00 for both owner-occupied and rented facilities (US\$ 416 to US\$ 458). Profits of businesses in rooms within residential buildings are even smaller, in the range of TZS 260.000,00 to 350.000,00 (US\$ 217 to US\$ 292).

Table 3.1.i

Average Monthly Sales and Profit – Commercial Establishments

PAP	Property Type	Total	Average monthly sales per PAP category (TZS)	Average monthly sales per establishment (TZS)	Average net monthly profit per PAP category (TZS)	Average net monthly profit per establishment (TZS)
Owner of business in own premises that	No answer (property type not identified)	4	214.910.000,00	53.727.500,00	10.340.000,00	2.585.000,00
will reinstall in same plot	Not interviewed	3		-		-
Total		7	214.910.000,00	53.727.500,00	10.340.000,00	2.585.000,00
Owner of business in rented premises	No answer (property type not identified)	4	29.500.000,00	7.375.000,00	7.000.000,00	1.750.000,00
that will reinstall in same plot	Not interviewed	1		-		-
Total	,	5	29.500.000,00	7.375.000,00	7.000.000,00	1.750.000,00
	Independent Commercial Building	1	30.000.000,00	30.000.000,00	4.000.000,00	4.000.000,00
	Kiosk attached to building	17	39.420.000,00	2.318.823,53	9.500.000,00	558.823,53
Owner of business in own premises that	Room in residential building	2	1.800.000,00	900.000,00	700.000,00	350.000,00
will relocate	Stand-Alone Kiosk	7	215.550.000,00	30.792.857,14	104.040.000,00	14.862.857,14
	No answer (property type not identified)	24	445.567.000,00	18.565.291,67	50.879.000,00	2.119.958,33
	Not interviewed	5	-	-	-	-
Total		56	732.337.000,00	14.359.549,00	169.119.000,00	3.316.058,82
	Kiosk attached to building	62	572.352.000,00	9.231.483,87	31.079.000,00	501.274,19
	Room in residential building	1	400.000,00	400.000,00	260.000,00	260.000,00
in rented premises that will relocate	Stand-Alone Kiosk	8	28.700.000,00	3.587.500,00	6.780.000,00	847.500,00
	No answer (property type not identified)	49	155.089.667,00	3.165.095,24	29.152.789,70	594.954,89
	Not interviewed	24		•		-
Total		144	756.541.667,00	6.304.513,89	67.271.789,70	560.598,25
Grand Total		212	1.733.288.667,00	n.a.	253.730.789,70	n.a.

NOTE: Grand average of profits for all categories is not considered a relevant indicator due to diversity.

3.2

Other Project Affected People

Other categories of PAP which may be eligible for specific compensation and/or assistance under the terms of Section 5.0 of the Resettlement Policy Framework issued for the DART project in February 2007, and which are present in Phase I – Part B, include the following:

- Owners of businesses (in either own or rented premises) that will suffer provisional closure or provisional loss of business during construction.
- Authorized non-transient street vendors.
- Daily transient vendors.
- Owners of businesses to be indirectly affected by the project.
- Other informal users of the right-of-way to be relocated.

These categories of PAP were not subject to application of the Socioeconomic Questionnaires. However, they were identified and registered in the Detailed Land-Use Survey developed as part of the project's Environmental and Social Impact Assessment (ESIA).

Along the final segment of Morogoro Road there is a concentration of small businesses, small restaurants, some residential units, and sparse vehicle-related services and petrol stations. Daily transient street vendors at this segment are concentrated mostly near the daladala stops and areas of commercial activity, whereas non-transient street vendors are concentrated in consolidated commercial areas.

Transient street vendors will not be indemnified financially but will be assisted with relocation to areas established by the City Council. Authorized non-transient street vendors may be eligible for disturbance allowance as well as relocation assistance. Though very sparse, a large variety of minor kiosks within the right-of-way remain in place overnight and were identified in the ESIA's Detailed Land-Use Survey. Along the Morogoro Road segment of Phase I – Part B this survey identified 82 non-transient street vendors within the right-of-way, as well as 166 daily transient vendors.

Indirectly affected businesses and others informally using available space within the rightof-way are not eligible for financial indemnification under the terms of the RPF, but may be eligible for relocation search assistance. In the Morogoro Road segment of Phase I – Part B, this category of PAP includes 19 businesses which use the right-of-way for exposure of goods for sale and/or restaurants that put chairs and tables on the street, as well as 7 taxi waiting spots, 6 construction truck parking areas and 1 daladala parking area.

Along the Msimbazi Street segment, 108 daily transient street vendors were identified in the ESIA land-use survey, mostly concentrated at Uhuro St and Msimbazi St junction (34%), and near the railway (19%). Non-transient street vendors are concentrated along the railway and at a nearby area exclusively designated for them and will not need to be relocated. Due to lack of left-over spaces in this street, there is not a significant number of commercial activities using the right-of-way for exposure of goods. Only 3 commercial establishments and 1 restaurant do so. Along this segment, there is also 1 taxi waiting spot and 2 daladala parking areas.

Along the Kawawa Road segment commercial activities concentrate between the Sinza River and Alli Hassan Mwiny Road. Most of the 46 daily transient street vendors and 9 non-transient vendors in this segment concentrate in this area. There are also 14 business establishments as well as 2 restaurants that use the right-of-way for exposure of goods. Finally, 5 taxi waiting spots, 2 construction truck parking areas and 1 daladala parking area are distributed along the Kawawa Road segment.

Businesses that will suffer provisional closure or provisional loss of business during construction will only be identified during the construction phase, as they put forward their claims for compensation in the terms established in the Resettlement Policy Framework. The grievance redress mechanism described in Section 6.0 of the RPF (and detailed in Section 6.2 of this RAP), will be the procedure for reception and evaluation of all such claims.

3.3 Vulnerable PAP and others Requiring Special Assistance

Identification of vulnerable PAP and others requiring special assistance was contemplated in the Residential Socioeconomic Questionnaire. Of the 228 families to be affected by Phase I – Part B, 20 declared having members with special needs, but the type of disability was not informed.

4.0 RAP Budget for Phase I – Part B

4.1

Property Indemnification and Resettlement Allowances Under the Terms of Tanzanian Law

Property Valuation and Allowance Allocation Criteria

The certified valuer responsible for property valuations in this RAP verified market values for land along all of the DART Phase I – Part B alignment, as well as replacement costs for the various types of buildings and improvements that will be affected by the project. The results of this study, as applicable to Phase I – Part B, are consolidated in **Table 4.1.1.a** below. As can be seen, the main difference with regards to compensation values established for Phase I – Part A is the price of unbuilt land. The same value ranges for replacement costs of existing structures were adopted on Parts A and B.

^{4.1.1}

The procedure adopted for inspection and valuation of affected properties followed closely the methodology established in Section 8.0 of the RPF. Values in the table below were applied to land where legally occupied, and to buildings and improvements, on the basis of quantities that were calculated during inspection in the presence of the affected party.

Resulting valuations were then included in the Proposed Compensation Schedule (ANNEX 01) to be submitted to the City's Chief Valuer for approval. In this process, the Chief Valuer may request adjustments as deemed fit.

SN		Replacement item	Unit	Compensation rate in TZS
	а	Land, unbuilt, CBD (Gerezani in Ilala Ward)	Meter square	50,000
1	Ь	Land, unbuilt, planed area other than CBD (Mwananyamala and Kinondoni)	Meter square	35,000
1	c	Land, unbuilt, planed area other than CBD (Hannanasifu)	Meter square	30,000
	d	Land, unbuilt, unplanned area	Meter square	10,000
	a	Building, framed structure, concrete blocks infill, high quality finishing and cervices	Meter square	400,000-600,000
	b	Building, block construction, high quality finishing	Meter square	200,000-300,000
2	с	Building, block construction, poor quality finishing	Meter square	150,000-200,000
	d	Building constructed of mud and poles, ion sheet roof, sand cement screed floor	Meter square	100,000-150,000
	e	Building constructed of mud and poles, ion sheet roof, compacted earth floor	Meter square	50,000-100,000
	а	Kiosk, or storage room attached to building structure, good construction quality/material	Meter square	100,000
3	b	Kiosk, or storage room attached to building structure, of medium construction quality/material	Meter square	80,000
	с	Kiosk, or storage room attached to building structure, of poor construction quality/material	Meter square	60,000
	a	Verandah, concrete slab roof, cement screed floor	Meter square	80,000
4	b	Verandah, corrugated iron sheet roof, cement screed floor	Meter square	60,000
5	a	Steps, floor tiles finishing	Meter square	
	b	Steps, sand cement screed finishing	Meter square	50,000
6	a	Parking space, paved blocks floor finishing	Meter square	40,000
0	b	Parking space, concrete slab floor finishing	Meter square	60,000
	а	Fence, reinforced concrete columns, concrete blocks infill with grill	Running meter	180,000
	b	Fence, concrete blocks with grill	Running meter	150,000
7	c	Fence, concrete blocks 4 courses	Running meter	80,000
	d	Fence, barbed wire on concrete poles	Running meter	60,000
	e	Fence, other materials (makuti)	Running meter	10,000
	f	Hedge	Running meter	10,000

 Table 4.1.1.a

 Compensation Value Reference Table for Phase I – Part A

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SN		Replacement item	Unit	Compensation rate in TZS
	a	Inspection chambers	Piece	80,000
8	b	Standard septic tank	Piece	1,000,000
a		Storage tanks, plastic, over head on concrete tower	1000-10,000 litres	800,000
9	b	Storage tanks, plastic, over head on metal pole tower	1000-10,000 litres	500,000
	c	Storage tanks, concrete underground	1000-10,000 litres	1,000,000
10	а	Pumps, gasoline, manual	Piece	2,500,000
10	b	Pumps, gasoline, electronic	Piece	5,000,000

Further to land and building compensation, regulations of Land Act N° 4 of 1999, made under Section 179 of the Land Regulations which became operational in May 2001, contemplate several complementary compensation allowances for which expropriated parties are eligible. These include:

- Disturbance allowance;
- Transportation allowance;
- Loss of profit or accommodation allowance;
- Accommodation allowance.

Disturbance Allowance is payable as a percentage of property valuation in compliance to the provisions of Act N° 4 of 1999. The percentage is the average commercial bank rates offered on fixed deposits during 1 year. In the RPF it was established that a 4% per year interest rate will be adopted.

Transport allowance is computed on the basis of market rates for transportation services within the project area, considering the average cost of transporting 12 tons over a distance of 20 Km. In the RPF this cost was set at TZS 120,000.00.

Regulations of Land Act N° 4 of 1999 require that accommodation allowance be calculated on the basis of monthly rent multiplied by 36 months. Monthly rent is equivalent to the market rental value of the affected part of the property per month and was calculated by the certified valuer on the basis of market data on rental values along the Phase I – Part B alignment.

Loss of Profit Compensation is calculated on the basis of 36 months of net profit where audited accounting records. Where these records are absent, a fixed assumption of TZS 90,000.00 was adopted as established in the RPF.

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All these allowances were calculated by the certified valuer on a case by case basis and have been included in the Proposed Compensation Schedule to be submitted to the City's Chief Valuer (**ANNEX 01**).

4.1.2

Summary of Compensation Schedule and Budget Summary

Table 4.1.2.a below summarizes the main figures of the Proposed Compensation Schedule by Ward and Municipality as well as by type of compensation or allowance. As can be seen, most of the RAP cost associated to legally established compensation and assistance results from Phase I – Part B interferences in Ilala.

Kinondoni							
Location (Ward)	Value of Building	Accommodation Allowance	Transport Allowance	Disturbance Allowance	Loss of Profit		
Hananassif	189.928.800,00	9.720.000,00	5.040.000,00	80.416.944,00	77.760.000,00		
Kimara	730.740.300,00	133.267.500,00	17.160.000,00	-	246.995.640,00		
Magomeni	295.279.450,00	14.976.000,00	6.240.000,00	131.071.636,00	68.040.000,00		
Mwananyamala	118.304.600,00	-	720.000,00	127.321.728,00	-		
Ubungo	150.581.500,00	-	3.360.000,00	1.603.200,00	55.080.000,00		
Total	1.484.834.650,00	157.963.500,00	32.520.000,00	340.413.508,00	447.875.640,00		
Ilala		r	· · · · · · · · · · · · · · · · · · ·	I			
Location (Ward)	Value Of Building	Accommodation Allowance	Transport Allowance	Disturbance Allowance	Loss of Profit		
Gerezani	5.786.031.500,00	27.120.000,00	47.169.600,00	2.730.538.320,00	252.720.000,00		
Jangwani	31.428.000,00	-	240.000,00	-	36.000.000,00		
Kariakoo	22.546.000,00	14.850.000,00	480.000,00	10.822.080,00	6.480.000,00		
Total	5.840.005.500,00	41.970.000,00	47.889.600,00	2.741.360.400,00	295.200.000,00		

 Table 4.1.2.a

 Compensation Schedule Summary by Ward (values in TZS)

Table 4.1.2.b analyzes total compensation proposed by the valuer to the 228 families to be affected, according to value ranges. An aspect to be highlighted is that over 80% of owner-occupier families will receive compensation between TZS 50.000.000,00 and 200.000.000,00 (US\$ 41.666 and US\$ 166.667).

As can be seen, almost 97% of residential tenants will receive under TZS 5,000,000.00, equivalent to US\$ 4,167.00. This is also the case of 5 of the 135 owner-occupier families to be affected.

A similar analysis is conducted for commercial properties to be affected in **Table 4.1.2.c**. Here, it is worth highlighting that 144 out of 149 commercial tenants (almost 97%) will receive total compensation of under TZS 5,000,000.00 (US\$ 4,167.00). On the other hand,

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51 out of 63 commercial establishments in own premises (almost 81%) will receive under TZS 15,000,000.00 (US\$ 12,500.00).

Table 4.1.2.d analyzes total compensation value ranges for non-resident landlords and unidentified PAPs. Average compensation for non-resident landlords is proposed at TZS 27,705,106.00, equivalent to US\$ 23,088.00.

Table 4.1.2.b

Indemnification Value	Residential Owners	Total (TZS)	%	
TZS 250,000.00 to TZS 5,000,000.00	5	19.932.634,00	3.7%	
TZS 5,000,000.00 to TZS 15,000,000.00	4	46,453,900.00	3.0%	
TZS 15,000,000.00 to TZS 50,000,000.00	16	494.772.850,00	11.9%	
TZS 50,000,000.00 to TZS 200,000,000.00	109	8.860.893.226,00	80.7%	
TZS 200,000,000.00 to TZS 650,000,000.00	1	202.428.040,00	0.7%	
Total	135	9.624.480.650,00	100%	
Indemnification Value	Residential Tenants	Total (TZS)	%	
TZS 250,000.00 to TZS 5,000,000.00	90	76.296.000,00	96.8%	
TZS 5,000,000.00 to TZS 15,000,000.00	3	18.240.000,00	3.2%	
TZS 15,000,000.00 to TZS 50,000,000.00	0	-	0%	
TZS 50,000,000.00 to TZS 200,000,000.00	0		0%	
TZS 200,000,000.00 to TZS 650,000,000.00	0	-	0%	
Total	93	94.536.000,00	100%	

Statistical Analysis - Residential Compensation and Allowances

 Table 4.1.2.c

 Statistical Analysis – Commercial Compensation and Allowances

Indemnification Value	Commercial Owners	Total (TZS)	%	
TZS 250,000.00 to TZS 5,000,000.00	31	99.138.000,00	49.2%	
TZS 5,000,000.00 to TZS 15,000,000.00	20	156.081.298,00	31.7%	
TZS 15,000,000.00 to TZS 50,000,000.00	9	267.656.276,00	14.3%	
TZS 50,000,000.00 to TZS 200,000,000.00	3	214.796.600,00	4.8%	
TZS 200,000,000.00 to TZS 650,000,000.00	0	-	0%	
Total	63	737.672.174,00	100%	
Indemnification Value	Commercial Tenants	Total (TZS)	%	
TZS 250,000.00 to TZS 5,000,000.00	144	483.840.000,00	96.7%	
TZS 5,000,000.00 to TZS 15,000,000.00	3	20.160.000,00	2.0%	
TZS 15,000,000.00 to TZS 50,000,000.00	2	56.075.640,00	1.3%	
TZS 50,000,000.00 to TZS 200,000,000.00	0	-	0%	
TZS 200,000,000.00 to TZS 650,000,000.00	0	_	0%	
Total	149	560.075.640,00	100%	

Table 4.1.2.d

Statistical Analysis –Compensation and Allowances for Non-Resident Landlords and Unidentified PAPs

Indemnification Value	(property use not identified)	Total (TZS)	%
TZS 250,000.00 to TZS 5,000,000.00	4	13.886.776,00	80%
TZS 5,000,000.00 to TZS 15,000,000.00	1	11.510.080,00	20%
TZS 15,000,000.00 to TZS 50,000,000.00	0	-	0%
TZS 50,000,000.00 to TZS 200,000,000.00	0	_	0%
TZS 200,000,000.00 to TZS 650,000,000.00	0	-	0%
Total	5	25.396.856,00	100%
Indemnification Value	Landlord	Total (TZS)	%
TZS 250,000.00 to TZS 5,000,000.00	5	16.851.000,00	35.7%
TZS 5,000,000.00 to TZS 15,000,000.00	1	14.610.680,00	7.1%
TZS 15,000,000.00 to TZS 50,000,000.00	6	202.190.598,00	42.9%
TZS 50,000,000.00 to TZS 200,000,000.00	2	154.219.200,00	14.3%
TZS 200,000,000.00 to TZS 650,000,000.00	0		0%
Total	14	387.871.478,00	100%

Total compensation proposed in the Property Compensation Schedule in **ANNEX 01** is summarized in **Table 4.1.2.e.** As can be seen, the final figure amounts to TZS 11,430,032,798.00 (US\$ 9,525,027.00). This value is significantly larger than the value proposed for Phase I – Part A which totaled US\$ 4,466,111.00.

Table 4.1.2.e

General Summary of Proposed Compensation Schedule

Component	Ressettlement Cost (TZS)	US\$
Land and Building	7.324.840.150,00	6,104,033.00
Accommodation Allowance	199.933.500,00	166,611.00
Transport Allowance	80.409.600,00	67,008.00
Disturbance Allowance	3.081.773.908,00	2,568,145.00
Loss of Profit	743.075.640,00	619,230.00
Total	11.430.032.798,00	9,525,027.00

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4.2

Complementary Compensation Entitlements under the Term of the DART Resettlement Policy Framework

4.2.1

Eligible PAP per Compensation Measure

Compensation and allowances under the terms of Tanzanian law, as calculated in the Proposed Compensation Schedule (ANNEX 01) and summarized in Section 4.1.2 above, may not be fully compatible with World Bank OP 4.12 requirements and hence a series of complementary compensation and assistance measures was established in the RPF (Section 5.2) to ensure that the situation of all PAP is adequately addressed.

Eligibility of Phase I – Part B affected people with regards to each one of those complementary measures is established below:

Property compensation bonus

Property Compensation Bonus is meant to assist owner-occupiers of houses and businesses that will need to relocate and will be compensated only for buildings and improvements, but not for land since this is not regularly occupied. The compensation bonus is meant to bridge the gap between market value of equivalent substitute properties and the amount to be received as compensation as per the certified valuer's valuation of buildings and improvements.

As can be seen in **Table 3.1.a**, a total of 131 owner-occupier families of totally affected residences will need to relocate. Of these, 109 will be compensated for land and 22 will not.

Similarly, 56 owners of businesses that occupy their own premises will be located and most of them (45) will not be compensated for land.

Thus, 69 "entities", including 22 families and 45 businesses, will be eligible for *Property Compensation Bonus* in Phase I – Part B.

Complementary transition allowance

Complementary Transition Allowance will be forthcoming where PAP can demonstrate that the transitional costs of relocation were higher that the transition assistance allowances received as per the Compensation Schedule to be approved by the City's Chief Valuer.

As highlighted in the RPF, this complementary allowance is not redundant or in conflict with the grievance redress procedure. Grievance redress will take place prior to compensation and resettlement. The complementary transition allowance is an *ex-post* procedure, where the affected party is allowed the chance to prove that his transitional costs were higher than assessed and consequently receives complementary compensation.

Eligibility for this measure is likely to be limited to the 131 owner-occupier families and 56 owner-occupier businesses that will need to resettle; and to a lesser extent, to the 93 residential tenants and 144 commercial tenants that will also relocate.

Complementary allowance for additional cost of living

The 131 owners-occupier families of residences that will need to relocate may be eligible for *Complementary Allowance for Additional Cost of Living* at the new place of residence. As seen in **Tables 3.1.e** and **3.1.f**, additional transport costs are not likely to be an issue, but additional land rent, tax and utility fees may be. These expenses represent, on average, 13.7% of total family income.

Tenant unexhausted improvements compensation

All affected tenants will in principle be eligible for *Tenant Unexhausted Improvements Compensation* where they can demonstrate that they implemented improvements on the basis of multi-year rental agreements which will be cut short as a result of Phase I – Part B implementation. As can be seen in **Table 3.1.a**, this complementary measure may benefit 149 commercial tenants and 93 residential tenants.

First refusal rights over future rented commercial space in terminals and feeder stations

All businesses that will need to relocate, whether in rented or owner-occupied premises, will be eligible for this measure. This will include a total of 200 commercial establishments (**Table 3.1.a**).

As mentioned in the RPF, this entitlement may be extended, subject to availability of shops or boxes, to businesses that will be partially affected by Phase I - Part B but will not need to relocate.

Moving assistance

The RPF establishes that *Moving Assistance* will be provided for PAPs that will incur in moving costs but are not eligible for transportation allowance, or in cases where said transportation allowance is clearly insufficient.

This may include larger commercial establishments that will relocate. However, none of the establishments that will relocate as a result of Phase I – Part B implementation are likely to be eligible for this measure (i.e. none will receive total compensation above TZS 200,000,000.00 as per **Table 4.1.2.c**).

Authorized street vendors to be relocated will also be eligible for *Moving Assistance*. On the basis of the Detailed Land Use Survey included in the Environmental and Social Impact Assessment, their number in Phase I – Part B is estimated at 91 (see Section 3.2 above).

Similarly, indirectly affected businesses having to relocate as a result of altered conditions (basically the 39 establishments that currently make informal use of Phase I - Part B right-of-way), may also be eligible for *Moving Assistance* to the extent they do in fact relocate.

Search assistance

All owners and tenants of houses and businesses that will be totally affected and will need to relocate will be assisted in searching alternative locations. Thus, a total of 224 families and 200 commercial establishments will be eligible for *Search Assistance* (Table 3.1.a).

Legal assistance for purchase

All property owners that will need to purchase alternative property will be assisted during contract negotiations in order to insure that proper terms are subscribed. As per **Table 3.1.a**, PAP eligible for *Legal Assistance* in Phase I – Part B will include 131 owner-occupier families and 56 commercial establishments currently occupying their own premises.

Vulnerable PAP special support

Results of the Socio-Economic Survey Residential Questionnaires (ANNEX 03) indicate that 20 of the 228 families to be affected declared have members needing special assistance. Further to these, should any previously uninformed handicapped or otherwise vulnerable persons be directly affected, *Special Support* shall be forthcoming as pertinent.

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Unproductive labor compensation

Commercial activities whose level of business comes to be affected as a result of interference provoked by Phase I – Part B construction may claim Loss of Profit Compensation under the terms of Tanzanian law and will be compensated accordingly.

Unproductive Labor Compensation is a complementary measure that may apply in the case of commercial activities that are temporarily closed or suffer very significant loss of business during construction, but which intend to continue in business after the construction phase is over. This measure contemplates payment of wages of all or part of the establishments' employees for up to three months, in order to avoid unnecessary lay offs.

The quantity of commercial establishments eligible for *Unproductive Labor* Compensation cannot be known at this stage. However, it is possible to foresee that most cases are likely to occur along the Msimbazi Street segment of Phase I – Part B.

Income restoration assistance

Loss of employment may occur as a result of displacement of businesses, temporary loss of sales, or temporary closure of businesses. Only in the case of displaced businesses is it possible to estimate number of potentially affected employees. As per **Table 3.1.a**, 200 businesses will need to relocate. As per **Table 3.1.g**, these establishments have 2.34 employees on average (besides the owner). Furthermore, it is not unlikely that some loss of employment will also occur at the 7 commercial establishments that will be partially affected but will re-install in the same property.

Based on the above, it seems reasonable to assume that up to between 400 and 450 persons may be eligible for *Income Restoration Assistance* during Phase I – Part B.

Relocation assistance for informal users of the right of way

As indicated in Section 3.2, there are several types of PAP that make use of the right-of-way to be occupied by DART Phase I – Part A, whether authorized or not. These include non-transient street vendors, taxi spots, daladala parking areas, construction truck waiting areas, and others.

Based on the Detailed Land-Use Survey conducted as part of the project's Environmental and Social Impact Assessment, it is estimated that 320 non-transient vendors, 13 taxi spots, 8 truck parking areas and 04 daladala parking area will need to relocate as a result of Phase I – Part B. All these PAP will not be indemnified but

will be eligible for *Relocation Assistance* which shall result in the definition of alternative locations where affected activities will be allowed to continue in a legally regular manner, as established in the RPF.

Indirectly affected commercial relocation assistance

Abutting businesses informally occupying the right-of-way to be occupied by DART Phase I – Part B will not be able to continue doing so. On the basis of the Detailed Land Use Survey conducted as part of the project's Environmental and Social Impact Assessment (see Section 3.2 above), it is estimated that 39 such commercial establishments will be affected by Phase I – Part B.

Many of these establishments may find it unfeasible to continue in the same location. Whereas these indirectly affected businesses will not be eligible for compensation since property as such will not be affected, they will all be eligible for *Commercial Relocation Assistance*, which will include assistance with the search for alternative locations, as established in the RPF.

4.2.2 Estimated Budget for Complementary Compensation

As established in the RPF, a Resettlement Planning and Implementation Team (RPIT) will be structured in order to ensure that Resettlement Action Plans are properly implemented. The RPIT's required staffing for Phase I – Part A was established in the corresponding Resettlement Action Plan. Since it is assumed that Part A and Part B will be implemented simultaneously, some additional staff will need to be considered. Required additional staff and associated costs are detailed in Section 4.3.3. Only RPIT costs additional to those already included in the consolidated budget for RAP Phase I – Part A are being considered in the consolidated budget for RAP Phase I – Part B (Section 4.4).

This section includes an estimate of the budget necessary for implementation of the Complementary Compensation Entitlements described in Section 4.2.1 above. It is important to note that not all complementary entitlements will imply in additional RAP implementation costs, since some consist basically of assistance to affected parties to be provided by the RPIT team.

Table 4.2.2.a summarizes the budget for Complementary Compensation Entitlements. The basis for calculations for each entitlement is presented below:

Property compensation bonus

There are 22 residential and 45 commercial PAP's eligible for this measure. Based on the Compensation Value Reference Table (**Table 4.1.1.a**), an average land market price of TZS 32,000.00 per m^2 was adopted and an average complementary compensation area of up to 120 m^2 was considered for each residential PAP and 30 m2 for each commercial PAP, totaling TZS 127.680,000.00 (US\$ 106,400.00).

Complementary transition allowance

The budget for this entitlement considered that, at most, up to 125 PAP may demonstrate eligibility. The average *Complementary Transition Allowance* to be awarded was estimated at TZS 1,000,000.00 per PAP, which represents an up to 20% increase over average Disturbance and/or Loss of Profit allowances in the Proposed Compensation Schedule (ANNEX 01).

Complementary allowance for additional cost of living

The average cost of land rent, property taxes and utility fees for owner-occupier families to be resettled, is TZS 116,702.44 per month (**Table 3.1.f**). The budget for this item considers payment of a complementary allowance equivalent to up to 30% of this cost during 24 months (period established in the RPF), to all 131 families potentially eligible.

Tenant unexhausted improvements compensation

The budget for this item admits that up to 10% of potentially eligible tenants will receive this entitlement (i.e. 24 out of a total of 242 residential and commercial tenants). The average cost of *Tenant Unexhausted Improvements Compensation* was assumed at TZS 600,000.00 on the basis of the values in **Table 4.1.1.a** (Compensation Value Reference Table).

First refusal rights over future rented commercial space in terminals and feeder stations

No specific budget has been allocated to this entitlement since it implementation will depend on RPIT time and commercial space at stations and terminals is already included in the project's investment cost.

Moving assistance

The average cost of this type of *Moving Assistance* was estimated at TZS 90,000.00, which is 75% of the transportation allowance calculated by the certified valuer as per Land Act N^o 4 of 1999. On the basis of Section 4.2.1 above, it was assumed that up to 100 PAP will receive this assistance.

Search assistance, legal assistance for purchase and vulnerable PAP special support

These three entitlements will imply in no costs additional to those allocated to the RPIT in Section 4.3.3.

Unproductive labor compensation

Potential costs associated to this entitlement are very difficult to estimate. The summary budget in **Table 4.2.2.a** follows the same criteria adopted in the RAP for Phase I – Part A and considers an allowance of TZS 14,400,000.00 which is roughly equivalent to 100 man-months of wages for assistant employees in retail establishments.

Income restoration assistance

A cost of TZS 160,000.00 per person for training and outplacement assistance was considered. Total number of PAP to receive this entitlement was assumed to be up to 250 on the basis of the analysis contained in Section 4.2.1.

Relocation assistance for informal users of the right of way and indirectly affected commercial relocation assistance

These two entitlements should imply in no additional cost since they basically consist of assistance to be provided by members of the RPIT.

Application of the criteria specified above resulted in a total budget for Complementary Compensation Entitlements of TZS 440,671,440.00 (US\$ 367,226.00) as shown in **Table 4.2.2.a** below. This amount represents 3.86% of the total amount allocated in the Proposed Compensation Schedule (ANNEX 01) and summarized in **Table 4.1.2.e**.

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Table 4.2.2.a

Summary of Complementary Compensation Budget

Complementary Entitlement	Allocated Budget (TZS)				
Property compensation bonus	127.680.000,00				
Complementary transition allowance	125.000.000,00				
Complementary allowance for additional cost of living	110.071.440,00				
Tenant unexhausted improvements compensation	14.520.000,00				
First refusal rights over future commercial space					
Moving assistance	9.000.000,00				
Search assistance					
Legal assistance for purchase					
Vulnerable PAP special support					
Unproductive labor compensation	14.400.000,00				
Income restoration assistance	40.000.000,00				
Relocation assistance for informal users of the right of way					
Indirectly affected commercial relocation assistance					
TOTAL	440.671.440,00				

4.3 Other RAP Implementation Costs

4.3.1 Consultation and Disclosure Costs

Consultation and disclosure will be conducted by the RPIT, including implementation of the grievance redress mechanism during pre-construction and construction phases. Thus, most costs associated to this activity are already included in the Implementation Management and Monitoring Budget included in Section 4.3.3 below.

Furthermore, a consultation and disclosure budget has already been considered in the RAP for Phase I – Part A (TZS 24,000,000.00). In view of this, a complementary budget of only TZS 8,000,000.00 (US\$ 6,667.00) is being considered for consultation and disclosure in the RAP Phase I – Part B consolidated budget.

4.3.2 Property Valuation Adjustment Allowance

As described in Section 6.3 below, all PAP will have the opportunity to question proposed valuation and allowances as approved by the City's Chief Valuer. This may result in alterations of the Compensation Schedule and an allowance of TZS 571,500,000.00, equivalent to 5% of the total value in the Proposed Compensation Schedule (ANNEX 01), has been allocated to this effect.

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4.3.3

RAP Implementation Management and Monitoring Costs (Resettlement Planning and Implementation Team - RPIT)

RAP implementation management and monitoring costs will be associated mostly to the Resettlement Planning and Implementation Team (RPIT) wages and social benefits, plus fixed and variable office costs and expenses. Staffing established in the RPF, as calculated in the consolidated budget for the RAP of Phase I – Part A, resulted in a total RPIT budget estimate of TZS 170,200,000.00 (including post-resettlement monitoring costs). Since Phase I – Parts A and B are to be implemented simultaneously, an additional 30%, or TZS 51,060,000.00, has been allowed for in the consolidated budget for Phase I – Part B.

4.3.4 Other Allowances

Further to the costs considered in sections 4.1.2, 4.2.2, 4.3.1, 4.3.2 and 4.3.3, allowance should be made for contingencies. Based on the level of detail of this RAP and on problems verified in part of the survey work conducted for it, it seems that a 7.0% allowance for contingencies is adequate.

4.4 Consolidated Budget for the RAP of DART Phase I – Part A

Table 4.4.a below presents the final budget for implementation of the RAP for DART Phase I – Part A.

Table 4.4.a	
Consolidated Budget	
Budget Component	Allocated Budget (TZS)
Proposed Compensation Schedule for Land, Buildings and Allowances (ANNEX 01)	11.430.032.798,00
Budget for Complementary Entitlements	440.671.440,00
RAP Consultation and Disclosure Costs	8.000.000,00
Property Valuation Adjustment Allowance	571.500.000,00
RAP Implementation Management and Monitoring Costs (RPIT)	51.060.000,00
Sub-Total	12.501.264.238,00
Contingencies (7.0%)	875.088.497,00
TOTAL	13.376.352.735,00

As already indicated, businesses that will suffer provisional closure or provisional loss of business during construction may request loss of profit compensation under the terms of Tanzanian law. They may do this through the grievance redress procedure established in this RAP. No specific allowance for such compensation has been included in this RAP's consolidated budget. Thus, it is assumed that the total amount of any such compensation will fall within the limits allowed for contingencies.

The total amount established above (TZS 13,376,352,735.00), equivalent to US\$ 11,146,961.00, is approximately 32% larger than the total budget estimated for this RAP in the Resettlement Policy Framework, which amounted to US\$ 8,418,000.00.

5.0 RAP Implementation Schedule

The implementation schedule presented herein assumes that Phase I – Parts A and B will be implemented simultaneously.

With the exception of general public disclosure of the Resettlement Policy Framework and activities necessary to structure the RPIT and the Grievance Committee, implementation of the RAPs for Parts A and B will effectively begin once the Proposed Compensation Schedules (ANNEX 01) are approved by the City's Chief Valuer. The implementation process will cover three phases:

- Pre-construction activities;
- Resettlement during construction;
- Post-resettlement monitoring.

The first two phases are expected to last up to twelve (12) months. The third phase will extend for another year as established in the RPF.

A Preliminary Schedule of activities is presented below, covering exclusively the first two of three phases above. The construction start-up date has been assumed to coincide with the finalization of public consultation meetings (as would be ideal). It is worth pointing out that this is merely indicative since the project allows for construction to begin in several segments that do not require expropriation while RAP implementation procedures are implemented elsewhere.

Thus, it is important to state that whereas the Schedule below indicates Construction Start-Up before PAP compensation and relocation is concluded, this does not mean that PAP will be required to move before they have been compensated. The RPIT will ensure that all PAP are properly compensated and assisted with relocation before work at the corresponding construction front effectively begins.

Preliminary Schedule

Activities	Months											
	1	2	3	4	5	6	7	8	9	10	11	12
General public disclosure of the RPF												i
Finalization of key engineering details	-											
Structuring treeruitment, etc.) of the RPIT												
Implementation of the Public Information Center(s)										_		
Approval of Comp. Schedule by Chief Valuer						1						
Approval of this RAP by DART PMU				1								
Implem, of 2 nd Phase of the Disclosure Plan					1							
Implementation of the Grievance Committee			1				1	1				
Operation of the Public Information Center(s)			1		1	1		1				
Operation of the RPIT			1	1	1		1					
Public Consultation Meetings with PAP									1			-
Construction Start-Up					1	1	1					
Grievance Redress			-	1	1	1		1	1	1		
Payment of compensations and allowances					1		1			1		
Moving, relocation and or re-installation of PAP					1			1	1			
Demolition of affected properties			T		1				1	1 -		
Assistance to PAP and implementation monitoring		-			-	-	<u> </u>					

6.0 Consultation and Disclosure during RAP Implementation

As indicated in the Resettlement Policy Framework, disclosure and consultation will be continuous during the resettlement planning and implementation process and will take place at two levels: the general level of the RPF and the specific level of each individual Resettlement Action Plan.

Consultation and disclosure of RPF principles and compensation packages will be initiated earlier, through official websites (Dar Es Salaam City Council, Kinondoni and Ilala municipalities, World Bank, other) as well as through media press releases and printed prospects. Information to be conveyed will focus on basic principles, eligibility criteria affecting entitlements and grievance redress procedures.

Whereas this initial disclosure of the RPF will take place prior to disclosure of the Phase I - Part B Resettlement Action Plan, the bulk of disclosure will be simultaneous.

Effective disclosure and consultation of the Resettlement Action Plans for Phase I – Parts A and B will begin after approval by the DART Project Management Unit (PMU). This will in turn take place only after the City's Chief Valuer approves the Proposed Compensation Schedule in **ANNEX 01**.

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At the time of initiation of this 2^{nd} Phase of disclosure, the Grievance Committee will have been established and will be operational, as will the two (02) Pubic Information Centers (PIC) proposed for the RAP for Phase I – Part A (one at a strategic location at the Central Business District and the other at Ubungo Terminal) and the additional PIC hereby proposed for Part B (at Kariakoo Terminal).

6.1

Specific DART Phase I Part B Consultation and Disclosure

Given that implementation of DART Phase I Parts A and B will be simultaneous, there will be a common Consultation and Disclosure process which, in strict accordance with the RPF, will contemplate the following:

Once the Compensation Schedules are approved by the City's Chief Valuer and the RAPs by the PMU, printed prospects will be produced describing the DART project and explaining the resettlement procedure. Specific prospects will be prepared for each main group of PAP categories. Thus, there will be prospects for property owners, tenants, business owners, others. These prospects will include information about:

- General description of the DART project and its benefits for Dar es Salaam;
- Valuation methods, including each RAP's Compensation Value Reference Table (Table 4.1.1.a);
- Complete specification of all entitlements;
- Detailed explanation of eligibility criteria;
- Detailed explanation of the grievance redress procedure;
- Place and time at which Public Information Meetings will be held;
- Contact instructions for the Government entity responsible for DART implementation (Dar Es Salaam City Council);
- Contact instructions for the Resettlement Planning and Implementation Team (RPIT);
- Contact instructions for the Grievance Committee;
- Address and working schedule of the three (03) Public Information Centers (PIC).

Printed prospects will be distributed on the field and an effort will be made to deliver them personally to each affected party. Further to this, press releases on newspapers and other media will be programmed in order to ensure that the population at large is aware of the project's implementation schedule and of the places and dates at which Public Information Meetings will be held.

Three Public Information Centers (PIC) will be implemented: one at the Central Business District (precise location to be determined), one at Ubungo Terminal and the other at Kariakoo Terminal. These PICs will be staffed in order to properly attend affected PAP seeking information. Staff will be properly trained and the PICs will have at their disposal:

- Complete copies of the project's Environmental and Social Impact Assessment, the Resettlement Policy Framework and this Resettlement Action Plan;
- Illustrations and renderings of DART Phase I Parts A and B;
- Detailed Expropriation Plans showing the limits to be affected on a property by property basis (following **ANNEX 04** of this RAP);
- Detailed lay-out of stations and terminals, as necessary to allow for affected businessmen to assess the commercial spaces to be made available and over whose concession they may have first refusal rights;
- Complete list of PAP and of the entitlements each one is eligible for under the terms of each RAP;
- Copy of the specific Valuation Reports as approved.

All consultation to the Public Information Centers will be recorded in data banks and will be consolidated on a monthly basis and analyzed by the Resettlement Plan Implementation Team (RPIT). Based on results of this analysis, the contents of printed materials and/or press releases may be altered to clarify aspects which are not being clearly conveyed.

At least seven (07) Public Information Meetings will be held, three (03) of which aimed specifically at Phase I – Part B segments.

These meetings will convey detailed explanations of all entitlements, eligibility criteria and valuation methodologies. Similarly, grievance redress procedures will be explained. All meetings will be recorded and minutes summarizing the main aspects discussed will be produced and signed by City Council Representatives, the Ward Executive Officer and participating community leaders.

On the 31st of August 2007, consultation and disclosure of the RAP was initiated through news releases and display of the approved Compensation Tables at the DART office and at the Municipalities of Ilala and Kinondoni. THE RAP Report has also been made accessible to the public at the DART office, TANROADS and the two Municipalities. A report by the DART PMU on disclosure and consultation activities as of November 2nd, 2007, is included in **ANNEX 06**.

6.2 Grievance Redress during RAP Implementation

As legally required and established in Section 6.0 of the RPF, all PAP may question compensation and allowances allocated to them under this RAP. These claims will be addressed to a Grievance Committee that will be operational throughout the process starting prior to disclosure of individual resettlement and compensation plans, and will decide on the basis of the information at hand. A single Grievance Committee will address both Parts A and B.

Based on results of the property survey and socioeconomic survey, and taking into account the shortcomings of the data retrieved, it is expected that grievance related to resettlement compensation exercise for DART Phase I – Part B may arise principally from the following:

- Mistakes related to identification of affected property and people within the rightof-way;
- Disagreements on land and asset valuation;
- Disagreements on other compensation allowances.

As established in the RPF, all grievances will be treated according to the following schedule:

- Once received, any requests for additional information from the complainant will be issued within 15 days.
- The complainant will in turn have up to 30 days to provide the requested information, but may request for an additional period of up to 60 days on the basis of proper justification.
- Once full documentation is at hand, the Grievance Committee will notify the complainant of its decision within 20 days.

Once established, the Grievance Committee will meet weekly. However, this frequency may be increased or decreased depending on the amount of grievances at hand.

Other aspects of the grievance redress procedure, as specified in Section 6.0 of the RPF, will fully apply.

7.0 Monitoring and Reporting Plan

The DART Project Management Unit (PMU) will, under delegation from the Dar Es Salaam City Council, be responsible for RAP implementation as established in the RPF. In order to comply with this mandate, a Resettlement Planning and Implementation Team (RPIT) will be made operational with the scope of responsibilities established in Section 7.0 of the RPF. This will take place within one month of approval of the Compensation Schedule by the Chief Valuer and before initiation of disclosure and consultation as described in Section 6.0.

At the beginning of the RAP implementation process, the PMU will establish a set of indicators to facilitate assessment. As a minimum, these indicators will include:

- Grievance frequency, expressed as the percentage of valuations subject to formal questioning by PAPs.
- Grievance response, expressed as the percentage of grievance that were attended to vs. those whose claims were denied.
- Compensation payment progress, expressed both in terms of the percentage of PAPs that have been fully compensated up to a given date, and as the percentage of the total budget in the Compensation Schedule that has been effectively disbursed.

The RPIT will coordinate RAP implementation of DART Phase I – Parts A and B. This will include:

- Setting up and staffing the Public Information Centers (PIC) and training the personnel.
- Organization of the public meetings.
- Production of printed materials and implementation of all other disclosure activities.
- Ensuring proper participation of all affected peoples in the process.
- Assistance to each individual PAP in understanding corresponding valuations and complementary compensation.
- Identifying and dealing with problem cases.
- Participation in the Grievance Committee.
- Follow-up on grievance redress mechanisms and feedback to the interested PAPs.
- Follow-up on payment of compensation.
- Ensuring that all PAP receive all entitlements established for it in this RAP.
- Providing special assistance to vulnerable PAPs.
- Assistance with search for new locations in cases where relocation is necessary.
- Assistance with planning of re-installation within same plot.

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- Coordination of other social assistance as per the matrix of eligibility established in this RAP.
- Monitoring of third party activities as they interfere with RAP implementation.
- Reporting as per the *Reporting Plan* described herein.
- Continuous monitoring and evaluation on the basis of pre-established indicators.
- Post-resettlement monitoring as established in the RPF.

The RPIT team will carry out inspections in every area where resettlement is taking place, with the frequency deemed necessary by the RPIT Manager. Since full documentation of all assistance provided is necessary, the key steps of each PAP relocation will need to be directly supervised.

Based on the fact Phase I Parts A and B will be implemented simultaneously, the RPIT will be staffed as follows:

- 1 Manager / coordinator
- 6 Social assistants during peak months, and 2 as a minimum during the complete process (until construction end).
- 1 Real estate specialist (during the initial months and as necessary to assist all PAPs that will relocate in finding alternative locations).
- 1 Outsourcing specialist during 5 to 8 months.
- 1 Data bank administrator during all the implementation process.
- 2 Administrative assistants during all the implementation process.

Staffing of the RPIT for post-resettlement monitoring will be limited to one (01) social assistant on a full-time basis, reporting directly to the PMU.

The RPIT Manager will be responsible for all procedures in the RAP. However, payment of compensation and allowances as per the Property Valuation and Allowance Schedule approved by the Chief Valuer will be paid directly by local authorities. The RPIT Manager will interact with authorities responsible for payment as necessary to program subsequent assistance measures.

The RPIT Manager will also administer the budget allocated in the RAP for implementation of the complementary compensation and assistance measures established in Section 4.2 herein. In cases where eligibility is based on verified results of RAP implementation, he will decide which PAP should receive such benefits on the basis of RPF principles and in consultation with the PMU.

The RPIT Manager will also interact with the responsible construction company in order to ensure that premises are only taken over after payment of compensation and after the respective PAP has been able to dismantle and remove reusable assets, except where the time limit allowed for this has been exceeded.

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RPIT Social Assistants will be in charge of day-to-day supervision of RAP implementation. They will provide special support to vulnerable peoples and will assist families and businesses as necessary to ensure that applicable entitlements are properly delivered. If deemed necessary, the RPIT Manager may request recruitment of additional Social Assistants during critical periods of the resettlement process.

The real estate specialist will constitute a databank of eligible properties in order to facilitate PAP relocation. He will provide general search assistance and will also assist with review of property legal documentation in order to ensure that PAPs resettle into legally regular buildings as applicable.

The outsourcing specialist will assist employees of businesses that will be laid off, in the process of seeking new employment. When necessary, he may request that specific training be provided in order to facilitate re-employment.

The data bank administrator will permanently update information on each PAP ensuring that proper follow up on delivery of each entitlement is forthcoming.

The RPIT will also be responsible for documenting the complete resettlement process, according to the following *Reporting Plan*:

Monthly Progress Reports will be prepared and submitted during the complete RAP Phase I – Parts A and B implementation cycle, and will include at least:

- Detailed activities list for all members of the RPIT team;
- Minutes of any public meetings held;
- Total number of people and businesses relocated during the month;
- Total amount of compensation and allowances paid and explanation of any deviation from the initial budget;
- Justified description of any deviation from the proposed implementation schedule;
- List of questions registered at the Public Information Centers;
- List of grievances received by the Grievance Committee and status of resolution;
- Summary table with the situation of each PAP category at the end of the month;
- Detailed description of the situation of any vulnerable PAPs at the end of the month;
- Status of the evaluation indicators at the end of the month and discussion of variation with regards to prior months;
- Description of main problems with RAP implementation to date and suggestions on how to deal with them;

• Planned activity list for the following month.

A *RAP Implementation Phase Final Report* will be submitted at the end of the compensation and assistance process. This will include at least:

- A Consolidated *Entitlements Report*, listing all entitlements effectively delivered on a PAP by PAP basis, and comparison with the initial situation described herein.
- A Consolidated Public Consultation and Disclosure Report, describing all consultation and disclosure activities and summarizing main shortcomings identified.
- A *Consolidated Budget Report*, presenting the detailed breakdown of all expenses incurred during RAP implementation, as well as a comparative analysis with the initial budget.
- A Grievance *Redress Report*, listing all claims and their respective resolution, with statistical analysis and discussion of the impacts on the final cost of the RAP.
- An Implementation *Process Evaluation Report*, based on the pre-established indicators and their evolution throughout the RAP implementation process.
- Other information which the RPIT deems pertinent to include.

An *Ex-Post Report*, focusing on documentation of resettled and/or re-installed PAP, will be submitted within 60 days after conclusion of resettlement and/or re-installation activities. This will consist of individual before and after photographic records as well as a comparative table of building characteristics, available utilities and services, and costs affected by resettlement (transport, taxes, rent, utilities, other). Based on evaluation of costs affected by resettlement, eligibility for complementary allowance for additional cost of living will be established.

A *Post-Resettlement Monitoring Report* will be submitted one year after conclusion of resettlement and/or re-installation activities, documenting the result of monitoring of resettled PAP.

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ANNEXES



ANNEX 01

Proposed Compensation Schedule



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ANNEX 02

Property Survey Data Bank (Inspection and Valuation Questionnaire)

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Annex 02 – Part 1

Sections 01, 02, 05 and 06 of Property Survey – Inspection & Valuation Questionnaire

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Annex 02 – Part 2

Section 04 of Property Survey – Inspection & Valuation Questionnaire

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Annex 02 – Part 3

Section 07 of Property Survey – Inspection & Valuation Questionnaire

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ANNEX 03

Socio-Economic Survey Data Bank (Residential Resettlement Questionnaire and Commercial Activities Questionnaire)

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Annex 03 – Part 1

Sections 01, 02 and 06 of Socio-economic Questionnaire – Residential Resettlement

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Annex 03 – Part 2

Sections 03, 04 and 05 of Socio-economic Questionnaire – Residential Resettlement

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Annex 03 – Part 3

Sections 01 and 02 of Socio-economic Questionnaire – Commercial Resettlement

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Annex 03 – Part 4

Section 03 of Socio-economic Questionnaire – Commercial Resettlement

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Annex 03 – Part 5

Section 04 and 05 of Socio-economic Questionnaire – Commercial Resettlement

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ANNEX 04

DART Phase I – Part B Expropriation Plans

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ANNEX 05

Social Impact Assessment included in the ESIA

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ANNEX 06

Social Impact Assessment included in the ESIA

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VALDART IL GRN 019	GEREZANI	This is a single stores semi-detached Residential property with Two out buildings								
VAL DART IL GRN 019	GFREZANT	Built up area - main building	6.8 80	250 000 00	17 200 000 00					17 200 000 00
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VAL DART II (BN 020	GEREZ ANI	This is a single storey semi-detached Residential property with Two out buildings								
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VALDART IL GRN 020	GEREZANI	Land	636.00	50 000 00	1 200 000 00		120 000 00			
VAL DART IL GRN 020	GEREZANI	Commercial This is a single storey semi-detached					120.000.00		· · · ·	
VALDART II GRN 021	GEREZANI	Revidential property with Two out buildings								
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AL DAKT IL GRN 021		This is a single storey semi-detached								
VAL DART B GRN 022	GEREZINE	Residential property with Two out buildings								
VAL DART IL GRN 022	GEREZANI	Built up area - main building	68.80	250 000 00	17 200 000 00					17 200 000 00
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VAL DART E GRN 023	GEREZ ANI	This is a single stores semi-detached Residential property with I wo out							·	
VAL DART II GRN 025	GEREZANI	buildings Built up area - main building	68 80	250 000 00	17 200 000 00				<u> </u>	17 200 000 00
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VAL DART II GRN 024	GEREZ ANI	This is a single storey semi-detached Residential property with Two-out buildings	<u> </u>							
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VALDART IL GRN 027	GERF7 AM	Built up area - main building	68 80	250 000 00	17 200 000 00					17 200 000 00
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		This is a single storey semi-detached								+
VAL DART IL GRN 028	GERLZ ANT	Residential property with Two out)			
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VAL DART II. GRN 031	GFREZ AN I	This is a single story semi-detached Residential property with Two out	l							
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VALDART IL GRN 961	OFRI ZANI	This is a single storey semi-detached Residential property with Two out buildings								
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VAL DART II GRN 072	GEREZANI	This is a single stores somi detached Residential property with Two out buildings								
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VALDARI IL GRN 080	GEREZ ANI	Land This is a single storey semi-idetached Residential property with Two out	636.00	50 000 00	31 800 000 00		120 000 00	26 983 200 00		58 90 5 200 0
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ANVEX 01 - RAP COMPENSATION SCHEDULE

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VAL DART KN HN F 006						· · ·	120 000 00		240 000 00	3 360 000 00
VAL DART KNHNF 000	HANNANASSII HANNANASSIF	lewelry shop electrical hardware	<u>`</u>				129 800 00		3 240 000,00	1 360 DU0 D0
VAL DART KN HNF on)	HANNAN ASSIF	Thus is part of a single states residential committal property with a retaining wall. The hipped tool as owered with corregard on out locks on tunker members and hardboards colling underscall. Wall- er of single come to blocks, which are plastered and painted. Windows are of glavel one error wooden frame: Doon are of placed panels and tunks: balens. Floor finishing is of emical screed and floor tiefs haccommodiles 1 shops in 2006.								
	HANNAN ASSIE	condition								
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VAL DART KN HNF 008	HANNAN ANSIF	Part of roofed verandah covered with corrugated iron sheets on timber members and hard boards, eeling undermach. It is supported to the ground by tubular metal poles Hoas funking is of coment screed There is a retaining wall of about 2 meters high								
	· ·									
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			25,60	10 000 00	768.000.00		120.000.00	1 755 552,00	3 240 000,00	5 883 552 00
VAL DART KN JINF 009	HANNAN ASSU	The is pair of a single stace commercial property with a semi-nul-need versical. The hipped sum to covered with sourced the senior multiple semi-nul- and hardboard, coling understand with the plastered and pairetid. Windows see of plast lower in worker frame Doors are of glazed panels and inder bitters. Flow finding is of covere in second allow title It accommodates a restaurant 4 kitchen two inder and a store as good condition								
VAL DART KN HNF 009	HANNANASSIF	Built up area	61.50	250 000 00	15 375 000 00					15 375 000 00
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VAL DART KN HNF 010	<u></u>	Outbuilding 2 VIP	3.10	°* 000,00	232 500 00					232 500 00
VAL DARTEN HNF 010		Out building	59.51	150 090 00	8 926 590 88					8 926 500 00
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ANNEX 01 - RAP COMPENSATION SCHEDULE

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AL DART KN KMR 003	KIMARA MATANGINI	Built up arca - main building	100 \$7	250 000 00	25 217 500 00					25 217 500 0
AL DART KN KAIR 2013	KIMARA MATANGINI	Outbinlding	1,40	150 400 00	4 72" 500 00					172* 590 0
AL DART KN KMR 003	KIMARA MATANGINI	VIP	8 50	90.000.00	765 004 04	2 160 000 00	120.0%0.00			3 p45 000 i
AL DART KN KMR 003	KIMARA MATANGINI	Tunant				2 160 000 00	120.000.00			2 280 000 (
AL DART KN KNR 003	KIMARA MATANGIM	Office accommodation					120 000 00		3 240 000 00	3 360 000 (
AL DART KN KMR 604	KIMARA MATANGIN	This is a single storey residential property								
AL DART KN KMR 004	KIMARA	Built up area - main building	73,24	250 000 00	18 310 000 80					18 310 000 1
11 DART KN KNIR 004	MATANGINI KIMARA	Outbuilding	8.41	159 000 00	1 261 500 00					1 261 500 (
AL DARI KN KMR.004	MATANGIM KIMARA	VIP	4,00		560 b60 00	2 160 000,00	120.000,00			2 640 880 8
AL DARI KN KMR 004	MATANGINI	Commercial					120.000,00		3 240 000 00	3 360 (H0H (
AL DART KN KMR 004	KIMAR V	Tenant + residential				452 000 00	120 000 00			552 000 0
AL DART KN KMR (604	MATANGINI KIMARA	Tenant - revidential	_ 			432.000.00	124 000 00			552 000 (
AL DART KN KMR 004	MATANGINI KIMARA	Tenant - residential				4+2 008-00	120 900 00			552 000 0
AL DART KN KMR 004	MATANOIN KIMARA	Tenapt - considercial			~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~		120 000 00		3 240 000 00	3 360 000 1
ALD ART KNKAR 004	MATANGINI KIMARA	Tenant - residential				43.1 990 90	120.000.00			552 000 0
	MATANGINI KIMARA	This is a single storey residential property				412 000 00				13, 0001
AL DART KN KNIR 005	MATANGINI KIMARA	<u> </u>								
AL DART KN KMR 605	MAT ANGINI KIMARA	Built up area - main building	\$1.40	250 000 00	7 850 040 00					7 850 000
AL DART KN KNIR 005	MATANOINI	Outbuilding I	100 51	200 000 00	20 102 009 00					20 102 000 0
AL DART KN KMR 005	MATANODI	Outbuilding 2	3.90	150 040 00	594 000 00	1 800 000 00	120.000.00			2 514 000.
AL DART KN KMR 005	KIMARA MATANGINI	Jewelers					120.000.00		3 240 000 00	3 360 MHO 0
AL DART KN KNR 005	KIMARA MATANGIM	Shop					120 000 00		3 240 000 00	3 360 (HH) (
ALDARI KN KMR 005	KIMARA MATANGINI	Shop					120 000 00		5 240 000 OU	3 360 000 1
AL DART KN KMR 005	KIMAPA MALANGINI	Shop					120.900,00		3 240 000 00	3 360 (667 F
ALD ART EN KAIR 405	KIMARA MATANGIM	Shop					120 000 00		3 240 000 00	\$ 360.000
AL DARTES KMR.005	KIMARA MATANGINI	Residential tenant				360.000.00	120 000 00			480 000 1
AL DART KN KNIR 005	KIMARA MATANGINI	Residential tenant				360 000 00	1 20 000 00			480 000 1
AL DARI KN KNIR 005	KIMARA MATANGINI	Residential tenant				360 000 00	120 000 00			480.000 (
AL DART KN KNIR 005	KIMARA MATANGINI	Residential tenast				360 000,00	130.000.00			480 000 0
AL DARI KN KMR 005	KIMARA MATANGINI	Residential tenant				360 000 00	120.000.00			480 000 1
AL D'ART KN KMR 006	KIMARA MATANGINI	This is a single storey commercial property								
AL DARI KN KMR 005	KIMARA MATANGINI	Built up area - main building	98 79	250 000 00	24 697 500 00					24 697 500 (
AL DART IN KMR.000	KIMARA MATANGINI	Vorandah	94.00	70 009 00	6 580 000 00					6 560 000 0
AL DART KN KMR 000	KIMARA MATANGINI	Toslet	11 70	89 000 10	936-000-00		120 000 00			1 056 000,0
AL DART KN KNIR 006	MATANGINI	Bar and restaurant					120 000 00		3 240 000 00	3 360 000 (
AL DART KN KMR 005	KIMARA	Shop					120 000 00		3 240 000,00	3 360 000 1
AL DART KN ENIR 007	MATANGPT KIMARA	This is a single storey continential								
AL DART KN KAIR 007	MATANGINI KIMARA	property Built up area - main building	157,"*	250 000,00	39 345 000 00					34 345 (00) (
AL DART KN KMR 007	MATANGINI KIMARA	Outbuilding	32.96	100 000 00	3 296 000 00	· · · · · · · · · · · · · · · · · · ·	129 080 90		3 240 000 00	6 650 000 1
AL DARTKY KAR 007	MATANOM KIMARA	Bai and testaurant					120.000.00		3 240 000 00	5 60 000
	MATANGINI LIMARA									1 100 000
AL DART KN KMR 007	MATANGINI	\hop					120 000 00		3 240 000 00	
AL DART KN KMR 008				-			120.000.00		5 240 000 00	3 ×60 000

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V 4L DART KN MAR 040		Built up area - main building	1X + X	259-000-00	00.005.236.WC					20 952 598,00
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VAL DART KN KMIR 009	-	Shop					120.000 00		00 UGU 177 -	1 100 000 Do
VAL DART KN KMR 000	 - -	Shap					00,000 02.1		10 000 017 1	1 160 000 00
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VALDARLES KNR 010		The re-arraye toers revisential property under construction. Walls are constructed of said cement blocks to the Jactel In Jar condition								
V AL DART KN KNIR BID	-	Built up area - main huilding	21 12	100 (00,00	7 947 (MNI UD					7 947 000 00
VAL DARTENKAR 010	:	V craudah	315 31	70 004 04	1 29% a00 00		0.000.001			1 41 \$ 600 00
V AL DART KN KNIR 011	:	This is a surgle stores commercial thum residential property								
V U DART KN KMR 011		Built up area	78.00	258 884 89	19 500 000 00					19 500 000 00
VAU DART EN KMK 011		V crandah	8.45	20 MAI 00	591 500 00	576 000 00	120.000.00			1 287 500 00
VAL DART KVENR 011		Tenant - revolential				288 000,00	60 800 8C1			408.000,00
VAL DART KN KNIK UIT	 - 	Tenant - residential				288 000 00	00.000.021			408 (00 00
VAL DARF KNEMR HI		Tenant - revolential				288 000 00	120.000.00			408 000,00
V AL DARFAN KMR 011	-	Tenant - rustdentraf				288 000 00	120.000.00			00 000 X0F
VAL DARI KN KMR 011		Tenant - resydential				288 000 00	120.000.00			40.000 304
V 4U DART KN KNR 011		Tenant - commenual					120.000		3 240 000 00	3 360 000 00
VAL DART KN KNIK 012	-	This is a single storey commercial from perdential perperty.								
V AL DART KN KAIR BIZ	-	Built up area - matti bistiding	118,16	250 000,00	29 540 000 ND					29 540 000 00
V AL DART KN KNIR 012		Dutbuilding	6.46	160 000 001	696 000 88					696.000.00
V AL DART KN KNIR 012		\1F	12,04	00'000 04	849,000,00					A40.000,00
V AL DART EN KAR 012		Fenuing wall	22.04	PU 049 04	176 1441 110	1 800 000 00	1.20.000 bu			2 096 000 00
VAL DART KN KNR 012	-	Residential to need				900.000	120.000.00			1 020 000 000
VAL DART ENEMR 012	-	Re-idential totiant				1411 000 000	120.000 021			10 000 020 I
V 4FDART KN KMR 012	-	Retail shup					120 008 09		• 240 000 00	3 360 000 HD
VAL DAKT KN KNR 912		Read they					129.900.00		1 244 944 90	3 360 (Ben 16)
V AL DART KNKNR 012		Retail shop					120 000 00		3 240 000 00	3 360 000 00
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A W. DAKT KNEWR 012		Rutail shop					1,20.000.04		3 240 900 00	3.60 N00 10

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VALUE TO NO.	tin sjoch	GEN RIPTION	sid, s oi Bair iz Na	RAJE PEN 1-1-16 - 21 ABA	n y Er groß Billi offen	ALCONDUCTION ALLOWANCE	TRANSPORT ALTONICACE	DOTORIAN E ALCONANCE	Loss of	(103)Pr 5 - 5 (7)-15 5 - 6 - 1 (
AL DARTKNEMR 013	KIMARA MATANGINT	This is a single storey commercial Cum residential property								
AL DARI KN KMR 513	KIMARA MATANGINI	Built up area - main building	79,21	250 060,00	19 862 500 00					19 802 500 3
AL DART KN KMR. 013	KIMARA MATANGINI	Outbuilding	29 674 00	150 000 00	4 451 100 00					4 451 100 0
AL DART KN KMR 013	KIMARA MATANGINI	V srandah	19.635,00	50 000 00	981 750 00	2 160 000 00	120 000 00			3 261 750 1
AL DART KN KMR 013	KIMARA MATANGIM	Residential tenant				5 tu 000 nn	120 000 00			660 000 0
AL DARTENEAR 013	KIMARA MAJANGINI	Residential Icnant				540 000 On	120 000 00			000 0aa
VALDARI KN KMR #13	KIMARA MATANGINI	Residential tenant				540.000 nu	120 000 00		_	oon 000,0
A M. DART KN KMR 013	MARA MALANGINI	Residential tenant				540 000 00	120.000.00			660 000 (
VAL DART KN KMR.013	KIMAR V	Failoring mart					120 000 00		3 240 000 00	3 360 000 0
VAL DART KN KMR 013	MATANGINI KIMARA	TV shup					120.000.00		3 240 000 00	• 360 000 f
VAL DART KNKMR 013	MATANGIM KIMARA	Auto parts					120-000-00		3 240 000 00	3 360 000 5
VAL DARI KN KMR 013	MATANGINI KIMARA	Music studio					120 000 00		3 240 090 00	3 360 000 1
VAUDART KN KMR 015	MALANGPT KIMARA	Phone vauchers and accessories					120 000 40		3 240 000 00	4 360 000,0
VAL DART KN KNR 014	MATANGIM KIMARA	This is a single stores commercial Cum								
VAL DARTEN ENR 014	MATANGINI KIMARA	residential property Built up area - main building	15471	250 000 00	38 677 500 00					38 677 500 (
VAL DART KN KMR.014	MATANGINI KIMARA	(Suthuilding	43.856.00	150 000,00	6 575 400 00					6 575 400 8
VAL DART KN KMR 014	M ATANGIM KIMARA	Verandah	18.00	50 000 00	900 000 00					900 000 (
VAL DART KN KMR.014	MATANGINI KIMARA		44.20	30 000 00	1 226 000 00	1 620 000 00	120 000 00		3 240 000 00	6 306 (44) 7
VAL DART KN KNR 014	MATANGINI KIMARA	Kiosk	44.20		1.00000	540 000 00	120 000 00		5 240 000 00	1 (99) 60* 0
	MATANGINI KIMARA	Revidential tenant								
V (1 D (RT KN K)(R 014	MATANGINI KIMARA	Residential topant				540.000.00	120 000 90			660 000 0
VAL DART KN KMR 014	MATANGINI KIMARA	Revidential tenant				540 000,00	120.000,00			660.000,0
VALDARTENKAR 014	MATANGIN KIMARA	Phone Couchers					120 000 00		3 240 000 00	3 360 000 (
VAL DART KN KMR 014	MATANGNI	Auto sperca					120 000 00		3 240 000 00	¥ 364) (HHI K
VAL DARTEN KMR 014	MATANGP I KIMARA	Kiosk					1 20 000 00		3 240 000 00	3 360 860 1
V AL DARI KN KAIR 014	MATANGINI	Workshop					120 000.00		3 240 000,00	• 360 ONP 0
VAL DARI KAAMR 014	KIMARA MATANGINI	Whole vales					120 000 00		27 917 820 00	28 037 820,0
VAL DARI KN KMR 015	KIMARA MATANGINI	This is a single stores commercial Cum residential property,								L
VAL DARLEN KNIR 015	KIMARA MATANGINI	Built up area - main building	80,20	250 000,00	20 050 000,00					20 050 OKIO (
VALDARTEN KMR 015	KIMARA MATANGINI	Outbuilding	31 84	150 opu 66	4 776 000 WI					4 776 090 0
VAL DARI KN KMR 015	KIMARA MATANGINI	V esandah	40.56	50 000 00	2 0 28 000 00					2 028 000 0
VAL DART KN KAJR 815	KIMARA MATANGINI	7 Ib	20.16	30 000 00	604 800 00	aa aau ona F	120.000.00			4 324 800,0
A AL DART KN KNIK 015	k IMAR A	Commercial					120 000 00		3 240 009 00	3 360 000,0
VAL DART KN KMR 015	MATANGINI KIMARA	Residential tenant				3 600 100 00	120.000.00	<u> </u>		3 720 000 0
VAL DART KN KMR 015	MATANGINI	Store and utilize					120.000.00		27 917 820 00	28 837 820.0
A AL DARI KN KAROL?	KIMARA MATANGINI	This is a single storey commercial property installed with heavy machinery								
VALDART KN KMR 017	KIMARA MATANONI	Built up area - main building	6 0.91	250,000,00	22 477 500 00					2 1 477 500 0
VALDARI KN KMR 017	KIMARA MATANGINI	Verandah	21.57	54 000 00	1.078.659.00		120.000.00		1 240 000 00	4 478 640 1
VALDART KN KNR 017	KIMARA MATANOINI	Hardware shop					120-000-00		3 240 000 00	• અને ભાગ છ
VALDART KN KMR.017	KIMARA MATANGPA	Whole sale and rstaal					120 000 00		3 240 000 00	3-460 NOO 0
VAL DART KN KMR.018	KIMARA MATANGINI	This is a single storey commercial ('um residential property								

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VALUATION NO	LOR ATION	UL · REPORT	AREA (IP B) 11 (DIN),	RATETER METER MULAN	A AL CRIMA Plan De Nor	COMPLETION GOOD INCL	TRANSPORT ALLOW INCK	UPT FRANCE ALLOWANT	Loss of Peters	COMPENSATION Fails E
AL DART KN KNIR 018	KIMARA MATANGINI	Built up area - main building	140.12	250.060.09	35 030 000 00					35 030 000 00
AL DART KN KMR 018	KIMARA MATANGINI	Outharlding t	58,70	[\$0 000 00	\$ 805 000 00					8 805 000 00
VAL DART KN KMR (18	KIMARA MATANGINI	Outhurlding 2	68 78	100 000 00	6 878 000 06	\$ 640 000 00	t 20.000 mb			15 638 000 00
AL DART KN KMR 018	EIMARA MATANGINI	Revidential tenant				720-000-00	120 009 00			840 000 00
AL DART KN KMR 018	KIMARA MATANGINI	Residential tenant				720 000 00	120 000 00			\$40 000 00
AL DART KN KAR OLS	KIMARA MATANGINI	Residential Ionant				720 000 00	120.000.00			840 000 00
VAL DARTEN KMR 018	KIMARA MATANGINI	Residential tenant				726-000-00	1.29 009 00			840 000 00
VAL DART KN FMR 618	KIMARA MATANGINI	Residential tennai				720 000 00	120.000.00			840 000 00
VAL DARTEN KMR 018	KIMARA MATANGINI	Rendential tenant				720.999,00	120.009,00			840.000,00
VAL DART KN KMR 018	KIMARA MATANGINI	Residential tenant				720 000 00	120.000.00			840 000 00
VAL DART KN KMR 018	KIMARA MATANGINI	Residential tenant				720 000 00	120 000 00			840 000 00
VAL DART KN KNIR 018	KIMAR A	Revidential tenant				-20 000 UG	120.000.00			840 เหตุ 90
VAL DART KN KMR 018	MATANGINI KIMARA	Residential tenant				*20 000 pp	120.000.00			\$40,000.00
VAL DART KN KNIR 018	MATANGINI KIMARA	Residential tenant				720 UMI NO	120 000 00			840 000,00
VAL DARTEN KMR 018	MATANGINI KIMARA	Residential tenant				720 000 00	120 000 00			840 000 00
VAL DART KN KMR 019	MATANGIN KIMARA	This is a single storey residential property								
AL DART KN KAIR 019	MATANGINI KIMARA	Built up area	51 84	250 000 00	12 959 600 00	540 000 00	120.000.00		r	1 < 619 600 00
VAL DART KN KNR 019	MATANGINI KIMARA	Residential tenant				540 000.00	120-000-00			660 000 90
VAL DART KN KNIR 020	MATANGIN MMARA	This is a single stores commercial Cum								
VAL DART KN KNIR 020	MATANOINI KIMARA	residential property Built up area	42.25	250 000 00	10 562 500 00					10 562 500 00
VAL DART KN KNR 029	MATANGINI KIMARA	shed	48.36	50 000,00	2 418 000 00					2 418 000 00
	MATANGINI KIMARA					720 000 00	120 000 00			1 375 290 00
VAL PART KY KMR. 020	MATANGINI KIMARA	Outbuilding	5 332 00	100 000 00	\$33,200,00	- 20 000 00	120 000 00			
VAL DART KN KNIR 020	MATANGINI KIMARA	Bai and restaurant							3 240 000 00	3 360 000 00
VAL DART KN KMR 020	MATANGINI KIMARA	Residential lonant				720 000 00	120.000.00			840 000 00
VAL DART KN KMR. 020	MATANGINI KIMARA	Retail shop					120.000.00		3 240 000 00	1 360 100 00
VAL DART KN KNIR 021	MATANGINI KIMARA	This is a single storey residential property								
VAL DART KN KMR 021	MAT ANGINI KIMARA	Built up arca	91 83	250 000 00	21 457 500 00					2+ 457 500 00
VAL DART KN KMR 021	MATANGINI KIMARA	V crasidah	9,02	\$0.000,00	451 000,00					451.000,00
VAL DART KNIKMR 621	MATANGINI	VIP	. 14	70 000 00	241 500 00	24 150 000 00	120 000 00			24 511 500 00
VAL DART KN KNIR 022	KIMARA MATANGINI	These are single stores residential properties								
VAL DART EN KMR 022	KIMARA MATANGINI	Built up area	18.00	250 000 00	4 500 000 Du		120 000 00			4 6 26 000 06
V AL D ART K∿ K\(R 02:	KIMARA MATANGINI	Rusiness	-	-	-		120 000 00		3 240 000 00	3 360 008 08
VAL DART KN KNIR 024	KIMARA MATANGINI	This is a single story residential Property with an out building								
VAL DART KN KNIR 024	KIMARA Matangini	Built up arca - main building	77 36	250 000 00	24 340 000 DB					24 349 060 00
VAL DART KN KMR 024	KIMARA MATANGINI	Outbuilding	14.40	150-000,00	2 140 000 00					2 160 009 00
VAL DARTEN KMR 021	KIMARA MATANGINI	Foundation	13.28	75 000 00	996 000 00	2 772 000 00	128.000.00			\$ 858 (Rol) ab
VAL DARTEN KAR 024	KIMARA MATANGINI	Residential tenant				720 Jun Ou	120.000.00			840 000 00
VAL DART KN KMR 024	KIMARA MATANGINI	Reindentsal tenant				o12.000.00	120.000.00			732 000 00
VAL DART KN KMR 024	KIMAKA MATANGINI	Residential lenant				720 000 00	120 000 00			840 000 00
VAL DART EN KNIR 024	KIMARA MATANGINI	Residential tenant				-20 Quu ob	120 009 00			840 (00,00

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ALL LEON No.	Instead	DE N.RITTION	NULL OF	NATE FER NETER MILLAND	V KLA K OK BY M BLSG	AL ON MOUNTION ALLOWING	TRANSPORT ALLOW AND R	MATERANCE ALLA DANCE	PROFIL	COMPLACTION (ALC)
AL DART KN KMR 025	KIMARA MATANGINI	This is a single storey residential Property with an out building	101 22	350 000 00	35 427 000 00					35 427 000 0
AL DART KN KNR 025	KIMARA	Built up arca				······································	120.000.00		1 240 000 00	3 360 000 0
AL DART EN KNIR 026	MALANGIN KIMARA	This is a single stores residential								<u></u>
	MMARA KIMARA	commercial Property Built up area	143.50	250 000 00	35 875 000 00					15 875 (NH) IN
	MATANGINI KIMARA									
VAL DARTENENIR 025	MATANGIN	Basement	72 70	200 000 00	14 540 000 00	1 620 000 00	126 000 00			Jis 289 090 0
VAL DART KN KMR 026 VAL DART KN KMR 026	KIMARA					<u> </u>	120 000 80 120 000 00		3 240 000 00	3 360 000 th 3 360 000 0
	MATANGINI MMARA	Bai and restaurant								
VALDARI KN KMR 026	MATANGINI	Office accommodation					120 000 00		3 240 000 00	3 368 000 0
VAL DART KN KMR 026	KIMARA MATANGINI	Residential tenant				540 00% 00	138 808 00			669 000 0
VALDARI EN KMR-026	KIMARA MA <u>TA</u> NGINI	Residential lenant				540 000 00	120 000 80			660.000.0
VAL DART KN KMR 026	MIMARA MATANGINI	Residential tenani				540 000 00	120 000 00			660 BDD 8
VALDARI EN EMIR 027	KIMARA MATANGINI	The is a single storey residential commercial. Property with a basement								
VAL DART KN KAIR 027	KIMARA MATANGINI	Huilt up arca	105.18	250 000 00	26 295 000 00					26 295 000 0
ALDARI KA KMR 027	EMARA MATANGINI	verandah	15,66	75 000,00	1 174 509 00	27 469 500,00	120 099 00			28 764 000 0
AL DART KN KAIR 027	KIMARA MATANGINI	Commental					120 000 OU		3 240 000 00	3 360 090 0
VAL DART KN KNIR 028	KIMARA MATANGINI	This is a single storey residential. Property								
VAL DART KN KMR 028	KIMARA	Built up area	102.97	250 0160 00	25 742 500 00				·	25 742 500 0
4 DART KN KNIR 028	MATANGINI KIMARA	veranda	5 49	75 000 00	411 750 00	> 520 800 00	120 000 00		3 240 000 00	6 291 750 0
ALDARTEN KMR 028	MATANGINI KIMARA	Residential tenant				2 520 000 00	120 009 00			2 640 000 0
	MATANGINI KIMARA	This is a single storey residential				2 3 20 1 000 100	170 000 00			2 040 000 (M
VAL DART KN KMR 029	MATANGINI	commercial Property								
VAL DART KN KNIR 029	KIMARA MATANGINI	Built up area	125.79	250 /100,00	31 447 500 00					31 447 500 00
4L DART KN. KMP 029	KIMARA MATANGINI	Foundation	9 88	75 000 00	741 000 00					741 000 0
VAL DART KN KNIR 029	KIMARA MATANGINI	Outbuilding I	21.60	150 000 00	3 240 008 00					3 240 000 00
V 4L D 4RT KN KNIR 029	KIMARA MATANGIM	Outbuilding 2	3 58	106 804 00	358 000 00	2 160 000 00	120.000.00			2 638 000 04
VAL DAREKN KMR 029	RIMARA MATANGINI	Revidential tenant				540 000 00	120.000,80			660 000 Q
VAL DART KN KNR 029	KIMARA MATANGINI	Residential tenant				\$40 000 00	120 000,00			669 000,0
VAL DART KN KNIR 629	KIMARA	Residential tenast				540.000.00	120 000 00			660 000 08
۰ ۱۰ ما. DART KA KMR 020	MATANGINI KIMARA	Residential tenant				540.000.00	120.000,00			060 080 0
VAL DART KN KMR 029	MATANGINI KIMARA	Retail shop					120.009.00		3 240 000 00	3 360 000 00
VAL DART KN KAIR 029	MATANGINI KIMARA	Soti drinks					120.000.00		3 240 000 00	360.009.0
	MATANGINI KIMARA									
VAL DART KN KAIR 029	MATANGINI KIMARA	Retail shop					120 000 00		3 240 000 00	1 360 000 00
VAL DART KN KAIR 029	MATANGINI	Stationary					120 000 00		4 2441 6610 00	3 360 000 0
	KIMARA MATANGINI	This is a single story residential Property		L						
ALDARTEN KARNAL	KIMARA MATANGINI	Buill up aren	87 59	75 000 00	6 569 250 00					6 569 250 U
VAL DAKT KN KMR 031	EIMARA MATANGINI	shade	40 8 (50 000 00	2 049 500 00		120.000.00		3 240 060 90	5 400 Ster (A
VAL DARTES KAIR 031	KIMARA MATANGINI	Residential ienant				185 000 no	120.008.00			300 000 0
VALDARI KN KMR 051	KIMARA MATANGINI	Residential tenant				180.000.00	120.000.00			500-000-00
VAL DART KN KMB (24	EIMARA MATANONI	Residential tenant				180 000 00	120.000.00			349 000 0
	KIM AR A	Residential ten ent				180.000 00	120.000.00			100 000 M
VAL DARTES KAIROSI	MATANGINI									

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VAL DART KN KNIK 631	KIMARA MATANGIM	Revidential tenant				180 000 00	120 000 00			-00 000 00
VAL DART KNIKNIR 032	KIMARA MATANGINI	Far is a ough storey continerent Property	1,90	150 000 00	494 u00 00		120 000 00		3 240 996 00	4 9\$4 (RHI (R
VAL DARTKN KMR (33	KIMARA MATANGINI	Foundation to flint level	85.90	75 (100-00	6 442 588 00		124 000 00		× 2 tu 000 00	9 802 500 0
VAL DART KN KMR 053	KIMARA MATANGINI									
VALDARI KN KND 001	KUMBU KUMBU	Part of lensing wall to a Residential property with A mital gate hung on Reinforced concrete poles								
VAL DARI KA KAD 002	KUMBC KUNIPI	Perking space and a permanent kosk tor- boant alsonabor, hospital it is constanted of faint to roof owered with 'Th on timber methors and off Baned vertices und-metal. Wall are of study careful Baleak which are postered Wathows are of table Mattern the patient of a study of Constantiation of the study of the study of Constantiation of the science of the study of the Constantiation of the science of the study of the spectra science of the science of the study of the spectra science of the science of the science of the science of the spectra science of the science of the science of the science of the spectra science of the science								
VAL TINET PARTY AND OUT	7 mil 13 mi				1-4					176000
	<u>····</u>	[haible storey commeteral property and a								
VAL DARTEN KND 004	KUMBU KUMBU	fencing wall								6 760 000 0
·	<u> </u>						·			5 760 000 0i 1 142 400,00
					• •		120 000 00	4 123 584 00		5 951 984 00
		· ·								
		<u> </u>			· -					
V AL DART KN KND 0%	NI MBC KI MBI	Part of fenung wall and a Melal gate hung on reinforced Concrete poles								
VAL DART KN KND 006	KUMBU KUMBI	Fencing wall	11.50	80.000.00	920 0KD 00					920-000-00
VAL DART KN KND 08a	KUMBI KUMBU				<u> </u>		120 000 00	1 197 699 00		2 892 600,00
VALDARI KN KND 007	KUMBU KUMBU KUMBU KUMBU	cement blocks		80 000,00	47 314 11					
. 	KUMBU KUMBU	Fencing wall	7,59	35 000 00	607 200,00 500 500 00		120 050 00	571 696 00		607 200 00 J 152 196 00
VAL DART KNAIG2.001	MAKULA	Single stores residential. Commercial property								
VALDARTEN MG2.001	MAKET A	Built up area	89.60	250 000 00	22 400 000 00			-		22 490 000 00
VAL DART KN MG2.001	MAKULA	Land	<u></u>		<u> </u>					
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										· ·
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VALDART KNAG2 002	MAKUT A	Single stores residential. Commercial property								
VAL DART KN MG2 002	MAKI TA	Built up nen	143 33	250 000,00	15 582 500 00					35 582 500 00
VALDAREKNAG2.002	MAKUT A	1 and	519.00	50.000.00	25 950 000 00					25 950 000 00
VAL DART KN MG2 002	MAKUT A	Outbuilding, rooted	₹1 01 ¥ 00	150 000,00	7 952 850 00					7 952 859 00
VAL DART KN MG2 002	мактта	Dutbuilding unmofed	23 781 00	100 000 00	2 378 100 00					2 378 100 00
VAL DART KN MG2 002	MAKUT A	Fencing wall	10.00	80.000.00	800-000-00		120.000.00	34 878 456 (8)		15 748 456 01
VAL D		····			· ·	·		L	- <u> </u>	
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<u> </u>	<u></u>			· · · · · · · · · · · · · · · · · · ·				·		
VAL DART KN MG2 002	MAKULA	Hotel and restaurant	– ·	<u>├</u>	 _	ł ———	120 000 00	ł·	1 240 000 00	3 360 000 00
VALDARI KNAG2002	MAKUT A	Single stores residential Commercial			<u> </u>		1		-	
NACIONENN MO2003	SIAN TA	property		ļ				ļ		
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VALDART KN MG2 003	MARITA	Outbuilding	86,40	150.000.00	12 960 000 00					12 960 000 0

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ANNEX 01 - RAP COMPENSATION SCHEDULE

Vallailes Se	Lain 41624	Diss kit Deriv	tak tié Biji Dis	RATE FER NETTRIMULAN	EXTERNA BUT DENG	A POMANCI A UMPINE OTO A	TRANSPORT ATOM OCK	MATTRAN, F	1085.07 200911	COMPEN-LINE COMPEN-LINE COMPEN-LINE
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VAL DART KN MG2 004	MAKUT A	Restaurant					120 090.00		3 244 960 88	ļ
VAL DART KN MG2 005	MAKUT A	Single storey residential Commercial								1
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<u>.</u>		······································								
VAL DART KN MG2 005	MAKI'T A	Workshop		L			120 000 00		3 240 000 00	1 160 000 0
VAL DART KN MG2 005	MAKUT A	Workshop					120 009 00		3 240 000 00	3 360 000 0
VAL DART KN MG2 005	MAKUTA	Phone accessories					120 000 00		3 240 000 00	3 360 000 0
VAL DART KN MG2 005 VAL DART KN MG2 005	MAKUTA MAKUTA	Soft dunks Hard ware			·		120 000 00		3 240 000 00	3 360 000,0
VAL DART KN MG2 005	MAKUTA	Boutique					120 000 00		3 240 000 00	1 760 000 0
VAL DART KNMG2.005	MAKET A	Residential tenant				504 000 00	120.000.00			624 000 0
VAL DART KNMG2 005	MAKUT A	Residential tenant				504 000,00				624 000 0
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	KIMARA	Part of office building Two pumps and								
VAL DART KN MNY 001	MATANGINI	pan ed Driveway								1
VAL DART KN MNA 001	KIMARA MATANGINI	Bult up area	11.50	250 000 00	2 875 (69) (40					2 875 (80) 0
VAU DART KNAINY 001	KIMARA MATANGINI	2pumps ä lömi			20 000 000 00					20.000.000.0
VALDART KNAJNY OUL	KIMARA MATANGINI	Land	28.90	\$\$ (00),00	1 011 \$00.00		120 000 00	1 \$65 520 00		2 997 020 0
VALDAREKNINY 003	KIMARA MATANGINI	Single stores (emidetached Rosidential property								
V M DARI KAMINY 003	KIMARA MATANGINI KIMARA	Built up area - man building	70.00	750 000 00	24 500 609 00					Z4 598 000 ti
VALDARTKNAINY 003	MATANOINI	\ crandah	19.32	75 000,00	1 449 000 00					1 449 000 i
VAL DART KN MNY 003	MATANGINI	Fencing wall	90.00	80.000.00	7 200 000 00					7 200 /000 0
VAL DART KNAINY 003	KIMARA MATANGINI	[and	988,00	50 800,00	49 400 000 00		120.000.00	119 759 040 00		169 279 040 0
VAL DARTEN MNY 664	KIMARA MATANGINI	Part of fencing walt to a residential property with a metal gate hung on reinfurced concrete poles. It is constructed of sand coment blocks, plastered and painted								
VAL DARTKN MNY 004	KIMARA MATANGINI	Fenemg wall	15 63	80 000 00	1 250 400 00					1 250 400 0
VALDARTEN MNY 004	KIMARA MATANGINI	Land	37.02	×\$ 000 00	1 295 700,00		1 20 000 00	1 223 128 00		2 657 828 0
V 41-12 VRT KN MNY 005	KIMARA MATANGINI	Part of tensing wall to a residential property with a metal gate hung on reinforced concrete poles. It is constructed of sand concent blocker plastered and painted								
VAL DART KN MN3 945	KIMARA MATANGINI	Ferenzy Wall	16.7%	\$ 0 000 00	1 336 000 00					1 33e 669 6
VAL DART KN MNY 005	KIMARA MATANGINI	l and	22 77	35 000 00	79n 950 00		120.000.00	1 023 816 00		1 949 766
VALDARI KN MNY 005	KIMARA MALANGPA	Part of fencing wall to a residential property								
VAL DART KN MNY 096	KIMARA MATANGINI	Fenung wall	18 60	80.000.00	1 488 000 00					1.488.0003
VAL DARTEN MNY 006	KIMARA MATANGINI	fand	27 20	50 000 00	1 560 000 00		120 000 00	1 367 040 00		2 847 040

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VAL DARTEN MNY 007	KIMARA MATANGINI	Part of commercial property bearing a								
VAL DART KN MNA 1997	KIMARA MATANGINI	Built up area	10,43	250 000,40	2 607 500 00					2 607 500 00
VAL DARTKN XINY 00"	KIMARA MATANGPU	V crandah	1245	75 AUA 00	933 750 00					933 "50.00
VAL DART KN MNY 007	KIMARA MATANGINI	l and	22 88	35.000.00	800 800 00		120 000 00	2 084 184 99		3 004 984 00
VALDARI KNUB2 001	NHC	Single storey commercial property								
V AL DART KN UB2 001	NHC	Built up area - main huilding	27 25	150.000.00	4 087 500 00					4 087 500 60
VAL DART KN UB2 001	NHC	Verandah	6.54	75 000 00	490 500 00		130 000 00			618 560 80
VALDARTEN (B200)	NH	Butchery					120 000 00		1 240 000 00	3 360 000 00
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<u></u>	ļ	Single storey commercial								
VIL D OKT KINT DE OUT	NHC	Built up area	11 50		· · · · · · · · · · · · · · · · · · ·					
VAL DART KN UB2003	NHC.	Tenaut - consumercial								
VAL DARI KNT B2 064	NHC	Single stores commercial property								
	NHC	Built up area	15 08	150 000 00	2 262 060,00		120 000 90			2 382 000 00
	NHC	Tenant - commercial	<u>.</u> .			<u> </u>	120 000 00		3 240 000 00	 +60 000 00
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	NHC	double storey commercial property		<u>├</u>						30 450 000 00
VALDARTENT B2 007	NHC	Built up area	\$7.00	\$50 000 00	30 450 000 00					
VAL DART KN UB2.007	NHC	<u> </u>					<u> </u>			
VAL DART KN UB2008	NHC			1						
VAL DART KN I B2 008	SHC	construction Built up area	15,75	150 000,00	2 362 599 00		120.000,00		3 240 000 00	5 722 500 00
VM DARTEN UB2000	NHC.	Single tioes commonsul Property under construction								
VAL DARTEN UB2 909	NIC	Built up area	7,5 %	150 000,00	1 129 590 00		1,30 (60) (90		3 240 000 00	4 489 500 00
VAL DART KN UB2010	NHC	Single story commercial Property under construction It is about 50% complete								
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VAL DART KNT B2 014	NHC		14,64	150 000 00	2 196 000 00		┼─── ──┤	┝╼───┥	3 249 000.00	5 556 000 00
VAL DART KN UB2 014	NHC	Built up area	14,04	130 180 00	2 196 000 00				3 240 000,00	3 356 000 00
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VAL DAREKN (1B2 02)		Single stores commercial Property								
VAL DARTKN UB2021	NHC	Built up area	no 55	200 000 00	13 310 600 60		130 000 00			13 430 000 89
	NIC	Office accommodation							3 240 0/01 00	3, 249,000,00
VALDARTKNUB2 021										
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ANNEX 2.1 - RAP REPORT Property Survey - Properties

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ANNEX 2.1 - RAP REPORT Property Survey - Properties

> Annex 2 - Part I Dat Raik related to Scores

ા બંધ નં મેત જેમ્બા જે કે હાપ્તર મે મિક્રાબ્લ છે. કે થોયઈઆ સ્ટિમાન્લ	ליו אווי אין ויינאווי אין	li partially afteried hew munt	L and Lue	In Land Real regularty pard and accepted hy the fourtrainear?	լ. Բլսթւ Դ. 141 Իջվեւի թով	וי לשם יכנה אותר מנקרו בירור ורידה יין אורי גנקחו.	נ ווא שווייו אלונינים עווייי אנווים בישטוג געשי	אריי פרר וו כעו שרחו זיינו. ש	Wate	ן, אמור שפורוכל	אַכּראי גו	ון איז, וג כחב ידא היר די המ	Pavement type of the street in front of the projecty
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ANNEX 2 1 - RAP REPORT Property Survey - Properties

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Annex 2 - Part 1 Data Bark related to Sections 01 (02, 08 and 06 of the Troperty Survey Question) or a

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ANNEX 2.1 - RAP REPORT Property Survey - Properties

Annex 2 - Part 1 Data Bank related to Sections 101, 02, 08, and 06 of the Ferguerry Survey Operational and Data Bank related to Sections 101, 02, 08, and 06 of the Ferguerry Survey Operational Annex

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ANNEX 2 1 - RAP REPORT Property Survey - Properties

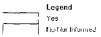
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ANNEX 2.1 - RAP REPORT Property Survey - Properties

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ANNEX 2.1 - RAP REPORT Property Survey - Properties

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ANNEX 2.2 - RAP REPORT Property Survey - Rooms

Annex 2 - Part 2

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Code of this Property Survey Inspection & Valuation Report	Room N*	Current Use	Rented or Owner-Occupied (1/0)
VAL/DART/KN/UB2/001	Room Nº 1	Other retail or services	R
VAL/DART/KN/UB2/001	Room N° 2	Other retail or services	R
VAL/DART/KN/UB2/002	Room Nº 1	Other retail or services	R
VAL/DART/KN/UB2/002	Room Nº 2	Other retail or services	R
VAL/DART/KN/UB2/003	Room Nº 1	Other retail or services	R
VAL/DART/KN/UB2/004	Room Nº 1	Other retail or services	R
VAL/DART/KN/UB2/005	Room Nº 1	Other retail or services	R
VAL/DART/KN/UB2/006	Room Nº 1	Other retail or services	R
VAL/DART/KN/UB2/008	Room Nº 1	Other retail or services	R
VAL/DART/KN/UB2/008	Room Nº 2	Other retail or services	R

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Code of this Property Survey Inspection & Valuation Report	Affected Party	Compensation for Land	Compensation for Structures	Disturbance Allowance	Accomod Allowance	Transport Allowance	Loss of Profit
VAL/DART II. GRN 001	Tenant - Busines - Owner						
VAL/DART IL GRN 001	On ner occupier - relideot						
VAL/DART IL GRN 001	Lenant - Pulline - Owner						
VAL/DART IL GRN 002	towner occupier - relident						
VAL DART IL GRN 002	Lenior - Buoness Owner						
VAL DART IL GRN 002	Tenant - Pasines (1950er						
VAL DART IL GRN 002	Lenant - Pusines Owner						
VAL DART IL GRN 003	finner occupier - rendent						
VAL DART IL GRN 003	Lenant - but mess Owner						
VALDARTH GRN 003	Tenant - Bulline Receiver						
VALDARTH GRN 003	Tetraft - Busine (2.05) bet						
VALDART IL GRN 003	Tenant - Business Owner						
VAL DART II. GRN 004	Usiner occupier - resident						
VAL DARTIL GRN 004	Tepanto Business Owner						
VAL/DART/IL/GRN/004	Ectivity - Business (1980)						
VAL/DART/IL/GRN/004	Tenant - Bulliness, Owner						
VAL/DART/IL/GRN/005	CONCENCEUPIER - resident						
VAL/DART/IL/GRN/005	Lenant - Bullines, Olympic						
VAL/DART/IL/GRN/005	Tenant - Busines Altistier						
VAL/DART/IL/GRN/005	Lenably Burney Unsher						
VAL DARFIL GRN 006	Owner occupier - rejudciil						
VALDART IL GRN 006	Tenad - Pusiness Using						
VAL DART IL GRN 006	Lenant - Bullance Owner						
VALDARTIL GRN 006	Length - Busines - Owner						
VAL DART IL GRN 006	Tenant - Business Owner						
VALDARLIL GRN 00"	O ober occupier - resident						
VALDART IL GRN 008	Îstiati - temistri					•	
VALDARI ILGRN 008	Lennut - Bulline - Owner						
VALDART IL GRN 008	Tennit - Burner Owner						
VAL DART IL GRN 008	Length - Burghass Uwinge						
VALDARI IL GRN 008	Tennot - Barmer Owner						
VAL DART IL GRN 008	Territo Busines Conner						
VALDARTH GRN 008	CONTRACTORIES OF IDEAL						

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Code of this Property Survey Inspection & Valuation Report	Affected Party	Compensation for Land	Compensation for Structures	Disturbance Allowance	Accomod Allowance	Transport Allowance	Loss of Profit
VAL/DART II. GRN 0009	Landlord (absent property Oscher)						
VAL/DART II. GRN 009	Tetraot - Business Owner						l
VAL/DARTH GRN 009	Lettind - Pusifices Owner						ļJ
VAL/DART II. GRN 009	Tetrant - Business Owner						
VAL/DARTIL GRN 0009	Ferant - Business O'sner				<u> </u>		L
VAL/DART IL GRN 010	Owner occupier - resident						
VAL/DART IL GRN 010	Tenant - Busine scorer						
VAL/DART H_GRN 011	Landlord (abjent propert, Owner)						
VAL/DART IL GRN 011	Tenant - re-ident						
VAL/DART IL GRN 011	Lenant - Busine - Owner						
VAL/DART II GRN 011	Exhant - Busine - Owner						
VAL/DART IL GRN 011	Lemant - Busine's Orsher					·	
VAL/DART II GRN 012	Owner occupier - resident						
VAL/DART II GRN 012	I cront - Business Owner						
VAL/DART IL GRN 012	Lenaut - Business Owner						
VAL/DART IL GRN 012	Len not - Buginess Owner						
VAL/DART IL GRN 013	Terrifit - Busiliess Unified						
VAL/DARTH GRN 013	Leo mit - Busilie (Chyber						
VAL/DARI II GRN 013	Tenani - Bullines) Owner						
VAL/DART IL GRN 013	Lenant - Bullines - Owner						
VAL/DART IL GRN 013	Ferrior - Busine (Classifier						
VAL/DARTH_GRN 013	owner occupier - refident	_					
VAL/DART IL GRN 014	owner occupier - readent			· · · · ·			
VAL/DART IL GRN 015	Tenant - Business Owner						
VAL/DART IL GRN 015	Tenant - Puside -: UPMet						
VAL/DART IL GRN 015	Lenard - Busines - Owner						
VAL/DART IL GRN 015	Chence occupier encodent						
VAL/DART II GRN 016	Comparison oper site (dent						
VAL/DART IF GRN 016	Tenant - Burane - Obster						
VAL/DART IL GRN 016	Lenard - Busines (1996)						
VAL/DART IL GRN 016	Lenant - Bulline - Ostier						
VAL/DART IL GRN 016	letint - Bullic - Ornet						
VAL/DART IL GRN 017	O vner occupier o positetit						

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Code of this Property Survey Inspection & Valuation Report	Affected Party	Compensation for Land	Compensation for Structures	Disturbance Allowance	Accomod Allowance	Transport Allowance	Loss of Profit
VALDART IL GRN 017	Isnant - Business Owner						
VALDART IL GRN 017	Ten inf - Busine is Owner						
VAL DART IL GRN 017	I chant - Busines: Owner						
VAL DART IL GRN 017	Lenant - Business Owner					L	
VALDART IL GRN 018	Uniter occupier - resident					L	
VAL DART IL GRN 018	I chant - Busines, Owber						
VALDART IL GRN 018	Lenant - Business (Dwiser					l	
VAL DART IL GRN 019	Usener occupier - relident						
VALDART IL GRN 020	Owner occupier - resident					· · · · · · · · · · · · · · · · · · ·	
VAL DART IL GRN 020	tenant - Bullines (Distort					<u> </u>	
VAL DARTH, GRN 021	O sher occupier - resident						L
VAL DART IL GRN 022	Owner occupier - Pussines- Owner					-	
VALDART IL GRN 022	Owner occupier - resident						
VAL DART IL GRN 023	Owner occupier - reoident						
VALDART IL GRN 024	Usiner occupier - resident	_				ļ	
VAL DART IL GRN 025	Tenant - Busilice (1990)					ļ	
VAL DART IL GRN 025	Owner occupier - relideof						
VAL DART IL GRN 026	Ovner occupier - re-ident		L				L
VAL DART IL GRN 027	Conner occupier - but the seconder					L	ļ
VAL DART IL GRN 027	Owner occupier - relident						
VAL DART IL GRN 028	Owner occupier - resident					<u> </u>	
VAL DARFIL GRN 029	Owner occupier - reordent					<u></u>	
VAL DART IL GRN 030	Owner occupier - relident					· ·	
VAL DART IL GRN 031	O ynet occupiet - readont		<u> </u>			ļ	
VAL DART IL GRN 031	Owner occupier - Bull one is Owner					· · ·	
VAL DARFIL GRN 032	Owner occupier - resident						
VAL DART IL GRN 033	O yner occupier - resident						
VAL DART IL GRN 034	Uniter occupier - relident			L	L	L	L
VAL DART IL GRN 034	OPTICLOCCUPICS - Bit one - Owner			L			L
VAL DART IL GRN 035	O sucroccupier - resident	•		l		L	
VAL DART IL GRN 036	Owner occupier - Bullomeos Olymer			l		L	L
VAL DART IL GRN 036	Owner occupier - readent						
VAL DART IL GRN 038	O sher of support of ended			1		l	

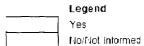
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Code of this Property Survey Inspection & Valuation Report	Affected Party	Compensation for Land	Compensation for Structures	Disturbance Allowance	Accomod Allowance	l ransport Allowance	Loss of Profit
VAL/DART/IL GRN 038	Owner occupier - Bussiness Owner						
VAL/DART/IL GRN 037	Owner occupier - resident						
VAL/DART/II GRN 039	viwner occupier - resident						i
VAL/DART/IL GRN 040	Conner occupier - resident						
VAL/DART/IL GRN 041	Owner occupier - resident					┢┛╌──────────	↓
VAL/DART/IL GRN 042	Owner occupier - resident					<u></u>	
VAL/DART/IL GRN 043	Owner occupier - relident					L	
VAL/DART/IL GRN 044	Owner occupier - resident						L
VAL/DART/IL GRN 045	owner occupier - resident					L	ļ
VAL/DART/IL GRN 046	Owner occupier - resident						L
VAL/DART/IL GRN 04"	Uniter occupier - Bussiness Usiner					L	
VAL/DART/IL GRN 047	Owner occupier - readent					L	<u> </u>
VAL/DART/IL GRN 048	Owner occupier - resident						<u> </u>
VAL/DART/IL GRN 049	Conner occupier - re-ident						L
VAL/DART/IL GRN 050	Owner occupier - readant			·		ļ	ļ
VAL/DART/IL GRN 051	Owner occupier - re-ident						L
VAL/DART/IL GRN 052	Conter occupier - reodent					L	L
VAL/DART/IL GRN 053	Owner occupier - relident						
VAL/DART/IL GRN 054	Owner occupier - resident				L	l	L
VAL/DART/IL GRN 055	Osciler occupier - relident					L	L
VAL/DART/IL GRN 056	Owner occupier - rendent						L
VAL/DART/IL GRN 057	O vier occupier - reodent					·	<u> </u>
VAL/DART/IL GRN 055	Owner occupier - readent		<u> </u>			Ļ	L
VAL/DART/IL GRN 059	Owner occupier - resident						L
VAL/DART/IL GRN 060	O vner occupier « te ident						L
VAL/DART/II GRN 061	Owner occupier - reaident						ļ
VAL/DART/II GRN 062	O sner occupier - resident					<u> </u>	↓
VAL/DART/II GRN 063	Owner occupier - Bull measurement				L	<u> </u>	<u></u>
VAL/DART/IL GRN 063	Uniter occupier - reliderit					L	<u></u>
VAL/DART/II GRN 064	U viter occupier - re odent			l	L	ļ	
VAL/DART/IL GRN 065	Owner occupter - resident					L	
VAL/DART/IL GRN 066	Competingupier - resident						
VAL/DART/IL GRN 067	O sucroccupici - resolcat					1	

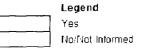


ANNEX 2.3 - RAP REPORT Property Survey - Allowance

Annex 2 - Part 3 Data Bank related to Section 07 of the Property Survey Questionnaire

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Code of this Property Survey Inspection & Valuation Report	Affected Party	Compensation for Land	Compensation for Structures	Disturbance Allowance	Accomod Allowance	Transport Allowance	Loss of Profit
VAL DART IL GRN 068	Owner occupier - Bussiness Owner						
VAL DART IL GRN 068	())) ner öccupier - fejident						
VAL DART II, GRN 069	Owner occupier - Business Owner					L	L
VAL DART IL GRN 069	conner occupier - recodent						L
VAL DART IL GRN 070	Owner Occupier - resident						
VAL DART IL GRN 071	Owner occupier - (c)(debt						L
VAL DART IL GRN 072	Owner occupier - relident						l
VAL DART IL GRN 073	Conner occupier - resident						
VAL DART IL GRN 074	Owner occupier - Bassine & Owner						<u> </u>
VAL DART IL GRN 074	Covner occupier - regident						
VAL DART IL GRN 075	Uwher occupier - Bussiness Owner						
VAL DART IL GRN 075	Cosner occupier - relident						
VAL DART H. GRN 076	Uwner occupier - readour						<u> </u>
VAL DART IL GRN 077	Concroccupier - Bussine - Concr						
VAL DART IL GRN 077	Owner occupier - resident						
VAL DART IL GRN 078	Owner occupier - Butsiness Owner						
VAL DART IL GRN 078	Et encroccupier - resident						
VAL DART IL GRN 079	Owner occupier - retadent						
VAL DART IL GRN 080	Concracupier - readent					<u> </u>	
VAL DART IL GRN 081	Tenuot - re-odent					L	
VAL DART IL GRN 081	Owner occupier - Bussiness Owner						
VAL DART IL GRN 082	Conter occupier - resident]
VAL DART IL GRN 083	Conter occupier - resident]
VAL DART IL GRN 085	Owner occupier - relident						<u> </u>
VAL DART IL GRN 085	Counce occupier - Buse mess Owner						
VAL DART IL GRN 084	Conter occupier - resident						
VAL DART IL GRN 086	Owner occupier - Bussilie s Diviner						
VAL DART IL GRN 086	Contraction upfor a resident						
VAL DART IL GRN 087	Obmer occupier - rendent						
VAL DART IL GRN 088	Conterport predent						
VAL DART II. GRN 089	Concrete upier - readent						
VAL DART IL GRN 090	Et suer occupier site ident						
VAL DART IL GRN 091	Uniter occupier - relident						



ANNEX 2.3 - RAP REPORT Property Survey - Allowance

Annex 2 - Part 3 Data Bank related to Section 07 of the Property Survey Questionnaire

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Code of this Property Survey Inspection & Valuation Report	Affected Party	Compensation for Land	ompensation for Structures	Disturbance Allowance	Accomod Allowance	Transport Allowance	Loss of Profit
VAL DART IL GRN 092	Owner occupior - re-ident						
VAL DART IL GRN 093	Owner occupier - Bussiness Owner						L
VAL DART IL GRN 093	Us net occupier - resident						
VAL DART IL GRN 094	Quanci occupier - re-ident						L
VAL DARI IL GRN 095	Us per occupier - realdeal					<u></u>	
VAL DART IL GRN 096	Alwhet occupier - resident						
VAL DART IL GRN 09"	Uniter occupier - Bussiness Usiner					L	
VAL DART II, GRN 09"	Unsper occupier - resident						
VAL DART IL GRN 098	Owner occupier - Bussiness Owner					L	L
VAL DART IL GRN 098	Owner occupier - readent						
VALDART IL GRN 099	Competinguage - feytdent						L
VAL DART IL GRN 100	Owner occupier - Bussiness Owner						
VAL DARI II. GRN 100	o where occupier - resident						
VAL DART IL GRN 101	Usener occupier - resident						
VAL DART IL GRN 102	O mer occupior - resident						
VAL DART H. GRN 103	Contendecupier - Bussiness Uniter						
VAL DART IL GRN 103	Concroccupier - re-ident						
VAL DART IL GRN 104	Conner occupier - resident						
VAL DART IL GRN 105	Owner occupier - revident						
VAL DART IL GRN 106	Owner occupier - resident	<u>,</u>				·	L
VAL DART KN HNF 006	Landford (absent property (Owner)						
VAL DART KN HNF 006	Lonint – resident				<u></u>		
VAL DART KN HNE 006	Tenaul - Business Owner						
VAL DART KN HNE 006	Tenant - Busines, Owner						
VAL DART KN HNL 004	Owner occupier - Bit (me > Owner						
VAL DART KN HNE 003	Owner occupier - Bu sine a Owner						L
VAL DART KN HNF 002	Owner occupier - Buy mess Owner						
VAL DART KN HNF 001	Owner occupier - Bit sine or Owner						
VAL DARI KN HNF 00"	Lenant - Buttpes - Owner						L
VAL DART KN HNE 007	Icrant - Busines Chance						L
VAL DARI KN HNF 007	Terant - Patiness Owner						L
VAL DARI KN HNE 007	Equal - Busine schwort						
VAL DART KN HNE 00"	Landford cablent property Owners						

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Annex 2 - Part 3

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Data Bank related to Section 07 of the Property Survey Questionnaire

Code of this Property Survey Inspection & Valuation Report	Affected Party	Compensation for Land	Compensation for Structures	Disturbance Allowance	Accomod Allowance	Transport Allowance	Loss of Profit
VAL DART KN HNF 008	Owner occupier - bussiliess Owner						
VAL DART KN HNF 009	Owner occupier - Bussiness Osner						
VAL DART KN HNF 010	Tenant - Busines Owner						
VAL DART KN HNF 010	Tenant - Busine - Owner						
VAL DART KN HNF 010	Tenant - Business Cityper						
VAL DART KN HNF 010	Tenant - Busines - Owner						
VAL DART KN HNF 010	Owner occupier - resident						
VAL DART KN HNI 011	Equalit - resident						
VALDART KN HNLOH	l chant - resident						
VAL DART KN HNF 011	Lenant - resident						
VAL DART KN HNF 011	l chatot - resident						
VAL DART KN HNE 011	len uit - resident						_
VALDART KN HNE011	f en int - reodent						
VAL DART KN HNE 011	Tenant - resident						
VAL DART KN HNE OLI	Tenant - Bulliness Using						
VAL DART KN HNF 011	Tenant - Business Owner						
VAL DART KN HNF 011	Lennit - Bullines, Owner						
VAL DART KN HNE 011	Estabil - Busids († Owner	T					
VAL DART KN HNE OFF	Lemmi - Bulloes, Owner						
VAL DART KN HNE OLI	Lenant - Bulliness Uwiter						
VAL DART KN HNE 011	Landlord (absent property Owner)						
VAL DART KN HNF 012	l'enaitt - resident						
VAL DART KN HNF 012	Ecoluti - relident						
VAL DART KN HNE 012	l'enant - revident						
VAL DART KN HNE012	Lenant - resident						
VAL DART KN HNF 012	Lettant - Bulline - Owner						
VAL DART KN HNE 012	I chant - Busines Owner						
VAL DART KN HNF 012	Undford (ab ent property Owner)						
VAL DARI KN KMR 001	Concroccupier - resident						
VAL DART KN KMR 001	Lea autore orderat						
VAL DART KN KMR 001	İ en mi - relident						
VAL DARI KN KMR 002	Characterization resident						
VAL DART KN KMR 002	Î etrant - te alent					ļ	

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No/Not informed

Annex 2 - Part 3

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Data Bank related to Section 07 of the Property Survey Questionnaire

Code of this Property Survey Inspection & Valuation Report	Affected Party	Compensation for Land	Compensation for Structures	Disturbance Allowance	Accomod Allowance	Transport Allowance	Loss of Profit
VAL DART KN KMR 002	Tenant - Business Owner						
VAL DART KN KMR 003	Owner occupier - resident						
VAL DART KN KMR 003	fenant - Busine, s Owner						
VAL DART KN KMR 003	fer mi - re ideni						
VAL DART KN KMR 004	Owner occupier - resident						
VAL DART KN KMR 004	Owner occupier - Bussiness Owner						
VAL DART KN KMR 004	Tenant - resident						
V VL DART KN KMR 004	Tenadi - resident						
V VE DART KN KMR 004	i en ant - refident						
VAL DART KN KMR 004	Ten int - re-ident						
V VI. DART KN KMR 004	Lenant - Business Owner						
VAL DART KN KMR 005	Owner occupier - re-ident						
VAL DART KN KMR 005	Echant - resident						
VAL DART KN KMR 005	I en m) - re_ident						
VALDART KN KMR 005	l'enant - resident						
VAL DART KN KMR 005	Lenant - revident						
VAL DART KN KMR 005	Len int - resident						
VAL/DART KN KMR 005	Tenant - Bulanes Unsper						
VAL DART KN KMR 005	Tenant - Provines (1) wher						
VAL DART KN KMR 005	Tenant - Bullines, Owner						
VAL DARI KN KMR 005	Ten into Busine of When						
VAL DART KN KMR 005	Tenant - Bulline: + Owner						
VAL DARI KN KMR 006	Landford (ab ent property Owner)						
VAL DART KN KMR 006	Tenant - Busine - Owner						
VAL DART KN KMR 006	Tenant - Bulliness Owner						
VALDARI KN KMR 00"	O ther occupier - relident						
VAL DARI KN KMR 007	Terond - Busines Covner						
VAL DART KN KMR 00N	Emidlord rabsent property Owners						
VAL DART KN KMR 008	Econt - Bulline - Owner						
VAL DART KN KMR 008	Ten nu - Business Covier						
VALDART KN KMR 009	Conter occupie) - relident						
VAL/DART KN KMR 009	Lenand - Bulances (Corper-						
VAL/DART KN KMR 009	Istund - Bulunds - Const						

Legend

Yes

No/Flot Informed

Annex 2 - Part 3 Data Bank related to Section 07 of the Property Survey Questionnaire

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Code of this Property Survey Inspection & Valuation Report	Affected Party	Compensation for Land	Compensation for Structures	Disturbance Allowance	Accomod Allowance	Transport Allowance	Loss of Profit
inspection & Valuation Report			Siructures	Anovance	Allowante	Allowance	
VAL DART KN KMR 009	Lenard - Busine, Owner					<u> </u>	
VAL DART KN KMR 009	Tenant - Business Owner						
VAL DART KN KMR 009	Letrint - Rusiness Owner					1	
VAL DART KN KMR 009	Tenant - Bulliness Chyper						
VAL DART KN KMR 009	Tenard - Business Disper						
VAL DART KN KMR 010	Conferenceupier - resident						
VAL DART KN KMR 011	Conter occupier - resident			_]
VAL DART KN KMR 011	Tetrabi - tesidetii						
VAL DART KN KMR 011	l'enunt - rejideni						
VAL DART KN KMR 011	Tenant - relident						
VAL DART KN KMR 011	l'en ant - resident					1	
VAL DART KN KMR 011	Echant - re ident						
VAL DART KN KMR 011	Lenant - Bulline, «Owner					1	
VAL DART KN KMR 012	Ivratit - Buline - Ovrier					· · ·	
VAL DART KN KMR 012	Lenant - Bulline at twper						
VAL DART KN KMR 012	Tenant - Busines Owner						
VAL DART KN KMR 012	I en ant - Bullates Owner						
VAL DART KN KMR 012	i chant - residero						
VAL/DART/KN/KMR 012	Concroccupier - relideot						
VAL/DART/KN/KMR 012	Î chant - re-ident						
VAL/DART/KN/KMR 012	Lettant - Business Owner						
VAL/DART/KN/KMR 013	d) oner occupier - relident						
VAL/DART/KN/KMR 013	Î const - readem						
VAL/DART/KN/KMR 013	Lensol - Jesodeni						
VAL/DART/KN/KMR 013	È chant - resident						
VAL/DART/KN/KMR 013	ferant - readerr						
VAL/DART/KN/KMR 013	Ten mi - Pusiness Owner						
VAL/DART/KN/KMR 013	Tennot - Business Owner						
VAL/DART/KN/KMR 013	Lenant - Bulanci - Obvier						
VAL DART KN KMR 013	Ich ml - Business Owner						
VALDARI KN KMR 013	Detrant - Busine (g. Owner						
VAL DART KN KMR 014	O studioscupier - Buillatic - Owner						
VAL DARI KN KMR 014	Tenant - rendent						

Annex 2 - Part 3

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Data Bank related to Section 07 of the Property Survey Questionnaire

Code of this Property Survey Inspection & Valuation Report	Affected Party	Compensation for Land	Compensation for Structures	Disturbance Allowance	Accomod Allowance	Transport Allowance	Loss of Profit
VALDAREKN KMR 014	Len int - resident						
VAL DART KN KMR 014	Lenapi - resident						
VAL DART KN KMR 014	Ten int - Pusines at wher						
VAL DART KN KMR 014	Len mt - Business Owner						
VAL DART KN KMR 014	ten of - Business Owner						
VAL DART KN KMR 014	Lengut - Basiness Owner						<u></u>
VAL DART KN KMR 014	Tenant - Pulines, Owner						
VAL DART KN KMR 015	Owner occupier - resident			<u> </u>		L	
VAL DART KN KMR 015	Uniter occupier - Bussiness Usiner						
VALDART KN KMR 015	l'enant - resident						
VAL DART KN KMR 015	I chaft - Business Owner						
VAL DART KN KMR 017	Tenant - Burmes: Owner						l
VAL DART KN KMR 017	Tenant - Bullines, Uwher						
VAL DART KN KMR 017	Owner occupier - Busoness Owner						
VAL DART KN KMR 018	Usiner occupier - resident						
VAL DART KN KMR 018	Fenant - resident						
VAL DART KN KMR 018	Leuant - resident						
VAL DART KN KMR 018	l chant - resident					L	
VAL DART KN KMR 018	leuant - relident					L	
VAL DART KN KMR UIS	Lenant - resident						
VAL DART KN KMR 018	Leitant - rehident						
VAL DART KN KMR 018	l'enunt - resident		l				
VAL/DART KN KMR 018	Leo int + re-ident				L	L	
VAL/DART KN KMR 018	bebaut - resident						
VAL DART KN KMR ØIN	Tenant - relident						
VAL DART KN KMR 018	Telenit - relident						
VAL DART KN KMR ON	l etcot - resident						
VAL DARI KN KMR 019	Owner occupier - relident						
VAL DART KN KMR 019	len oft - relident						L
VAL DART KN KMR 020	Concrocopier - readont						
VAL DARI KN KMR 020	Lebant - regident						
VAL DART KN KMR 020	Extrant - Balline Constient						
VAL DART KN KMR 020	I chant - buones - Owner						· · ·

Legend Yes No/Not Informed

Annex 2 - Part 3

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Data Bank related to Section 07 of the Property Survey Questionnaire

Code of this Property Survey Inspection & Valuation Report	Affected Party	Compensation for Land	Compensation for Structures	Disturbance Allowance	Accomod Allowance	Transport Allowance	Loss of Profit
VAL DART KN KMR 021	Owner occupier - resident						
VAL DART KN KMR 022	Owner occupier - resident						
VAL DART KN KMR 023	Owner occupier - Bus measurement]
VAL DART KN KMR 024	Uwner occupier - resident						
VAL DART KN KMR 024	lenant - resident						
VAL DART KN KMR 024	l'enant - resident						
VALDART KN KMR 024	lictiont - relident					-	
VAL DART KN KMR 024	letant - resident						
VAL DART KN KMR 025	Uniter occupier - relident					·	
VAL/DART KN KMR 025	Quantr occupier - Bull the schwher						
VAL/DART KN KMR 026	Owner occupiet - resident						
VAL DART KN KMR 026	Owner occupier - Bull mess Owner						
VAL DART KN KMR 026	l'enant - resident						
VAL DART KN KMR 026	Tenant - refident					·	
VAL DART KN KMR 026	<u> </u>					L	L
VAL DART KN KMR 026	Lenant - Busine Statuter		<u> </u>			L	
VALDART KN KMR 026	Fenant - Bir mess Owner						L
VAL DART KN KMR 027	O sherocouplet stellderd					·	
VAL DART KN KMR 027	Owner occupier - Bussiness Owner						
VAL DART KN KMR 028	Confect occupier - Bussiness Owner						
VAL DART KN KMR 028	Fenant - resident						L
VAL DART KN KMR 029	Concrocopier - readent				· · · · · · · · · · · · · · · · · · ·		
VAL DART KN KMR 029	Lenarit - re-ident					<u> </u>	
VAL DART KN KMR 029	len int - resident						L
VAL DART KN KMR 029	Í chant – rekident					L	
VAL DART KN KMR 029	l coant - resident		L				
VAL DART KN KMR 029	ben mt - Busines - Owner					L	L
VAL DARI KN KMR 029	Tenant Bit me Owner					L	
VAL DART KN KMR 029	Terrint - Burine - Owner						<u> </u>
VAL DART KN KMR 029	Evitable Busilies - Owlice					L	L
VAL/DART/KN/KMR/031	Ten aut - resident						
VAL/DART/KN/KMR/031	Tennot - rendent			L			ļ
VAL/DART/KN/KMR/031	Tercout - 18 odern						

Legend Yes

No/Not Informed

Annex 2 - Part 3 Data Bank related to Section 07 of the Property Survey Questionnaire

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Code of this Property Survey Inspection & Valuation Report	Affected Party	Compensation for Land	Compensation for Structures	Disturbance Allowance	Accomod Allowance	Transport Allowance	Loss of Profit
VAL DART KN KMR 031	l'enant - re-ident						
VAL DART KN KMR 031	l chant - re-ident						
VAL DART KN KMR 031	l'enant - re-ident						
VAL DART KN KMR 031	Owner occupier - Buildines Owner						
VAL DART KN KMR 032	Owner occupier - Bussiness Owner				<u> </u>		
VAL DART KN KMR 033	Owner occupier - bussifiest Coner						
VAL DART KN KND 001	Owner occupier - resident						
VAL DART KN KND 002	Owner occupier - Busside (s.0.5) ner					L	
VAL DART KN KND 004	Owner occupier - Busone o Ustier						
VALDARI KN KND 004A						L	
VAL DART KN KND 006	1.4hcr						
VAL DART KN KND 007	<u>e uher</u>					<u> </u>	
VAL DART KN MG2 001	Concrocopier - readent						
VAL DART KN MG2 001	Terrari - relident					Ļ	
VAL DART KN MG2 001	İvnant - Busines Owner						
VAL DART KN MG2 002	Uniter occupier - re-ideal						
VAL DART KN MG2 002	Lenard - Business Costlet					L	·
VAL DART KN MG2 002	Lenant - Busine (1995).r						
VAL DARI KN MG2 002	Tenant - Business Owner			·		l	
VAL DART KN MG2 002	Fenant - Bulline, Owner						[
VAL DART KN MG2 002	Lenant - Bullines (Owner						
VAL DART KN MG2 002	Lenant - Busines Owner			<u> </u>			· · · · · · · · · · · · · · ·
VAL DART KN MG2 002	Terrant - Bul messet woor					<u> </u>	
VAL DART KN MG2 002	Lettant - Busines Owner						
VAL DART KN MG2 003	Conner occupier - readent					L	
VAL DARI KN MG2 003	Lenant - re-ident		L			<u> </u>	
VAL DART KN MG2 003	len ad + resident						
VAL DARI KN MG2 003	Lenaut - resident						
VAL DART KN MG2 003	licitàrit - relident					L	
VAL DART KN MG2 003	for int - resident						
VAL DART KN MG2 003	Lenant - re-ident						
VAL DARI KN MG2 003	Letiant - resident						
VALDART KN MG2 003	benatif - resident						

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Annex 2 - Part 3

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Data Bank related to Section 07 of the Property Survey Questionnaire

Code of this Property Survey Inspection & Valuation Report	Affected Party	Compensation for Land	Compensation for Structures	Disturbance Allowance	Accomod Allowance	Transport Allowance	Loss of Profit
VAL DARI KN MG2 003	fictiont - relident						
VAL DART KN MG2 003	Terratit - resident						
VAL DARI KN MG2 003	Lettant - Busine - Owner						
VAL DART KN MG2 003	Tenant - Busines - Owner					[
VAL DART KN MG2 003	Tenant - Pusines - Owner						
VAL DART KN MG2 003	Ten int - Bulines, Owner						
VAL DART KN MG2 004	Landford (absent property Usener)						
VAL DART KN MG2 004	Letiant - Busilies Owner						
VAL DART KN MG2 005	Owner occupier - Bussilie (s.) Otter						
VAL DART KN MG2 005	Ten nu - rendeni						
VAL DART KN MG2 005	Èchant - resident						
VAL DART KN MG2 005	l'enant - resident						
VALDART KN MG2 005	Tenant - re-ident						
VAL DART KN MG2 005	Terrate Business Owner						
VAL DART KN MG2 005	Leoand - Busines, Owner						
VAL DART KN MG2 005	for into Bullie set wher						
VAL DART KN MG2 005	Lettant - Business Uniter						
VAL/DART KN MG2 005	Lenant - Burther - Owner						
VAL/DART KN MG2 005	Tenant - Businets Owner						
VAL/DART KN MG2 006	Owner occupier - Bussine Owner						
VAL/DART KN MG2 007	Landford (abject) property (Concre-						
VAL/DAR1 KN MG2 007	Ten int - resident						
VAL/DART KN MG2 007	Lenant - resident						
VAL/DART KN MG2 007	lien out - resident						
VAL/DART KN MNY 001	Unit occupier - Bussilies Owner						
VAL/DART KN MNY 003	Conternee open - resident						
VAL/DART KN MNY 004	Owart occupier - readent						
VAL/DART KN MNY 005	Quality occupies - to ident						
VAL/DARI KN MNY 006	the for a complete to beat						
VAL/DART KN MNY 007	Owner occupier - Bussines, Owner						
VAL/DART KN UB2 001	Landford (absent property (23) user)						
VAL/DARI KN UB2 001	Leptont - Buchberg Unsher						
VAL/DART KN UB2 001	Lenant - Bin mes exposition						

Legend Yes No/Not Informed

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Annex 2 - Part 3

Data Bank related to Section 07 of the Property Survey Questionnaire

Code of this Property Survey Inspection & Valuation Report	Affected Party	Compensation for Land	Compensation for Structures	Disturbance Allowance	Accomod Allowance	Transport Allowance	Loss of Profit
VAL DART KN UB2 002	Owner occupier - Bullone of Owner						
VALDARI KNUB2 003	Other occupier - Business Owner						
VAL DART KN UB2 003	Lenant - Patoneso Ossici						
VAL DART KNUB2 004	Emdlord rate ent property Oscoera						
VAL DART KN UB2 004	Tenant - Bulanco Covner						
VAL DART KN UB2 005	Owner occupier - Bussiness Owner						
VAL DARI KN UB2 006	Landford (absent property O speci-						
VAL DART KN UB2 006	Tenant - Business Owner						
VAL DART KN UB2 00"	Owner occupier - Bussiness Owner						
VAL DART KN UB2 008	Owner occupier - Bussiness Owner						
VAL DART KN UB2 009	Owner occupier - Bulsoness Owner						
VAL DART KN UB2 010	Owner occupier - Bussine (10) her			-			
VAL DART KNUB2 011	Cluster occupier - Bit stness Usener						
VAL DART KN I B2 012	Owner scorpter - Bussiness Owner						
VAL DART KN UB2 013	Owner occupier - Bussine's Owner						
VALDARTKNUB2014	Owner occupier - Bit one solwher						
VAL DARI KN I B2 014	Ictuat - Publics Owner						
VAL DART KN UB2 015	Unvner occupier - Buseme - Uwner						
VAL DART KN UB2 016	Owner occupier - Bussiness Owner						
VAL DART KN UB2 017	Owner occupier - Bussiners Owner						
VAL DART KN UB2 020	Owner occupier - Bussine o Owner						
VAL DART KNI B2 021	I chant - Business Owner						
VAL DART KN 1 B2 021	Owner öseupier - Bussine - O Oner						
VAL DART KNUB2022	Uther						
VAL DART IL IGN 001	Owner occupier - Bussines Owner						
VALDART IL JGN 002	Owner occupier - Por sine of Owner						
VALDART IL KKO 001	- ther						
VAL DART II, KKO 002	Landlord (absent property Owner)						
VALDART IL KKÖ 002	Leitant - Bullines Uwner						
VALDART IL KKO 002	Durant - Busines - Usingr						

Annex 03 - Part 1

Data Bank related to Section 101.02 and 00 of the Sociercon and Satve or Regioneric Recentering Onests increa-I there any Family Type of Cosi of Rentul Gathage commercial Code of Corresponding Property Survey Code of this Social Economic Unit: Type of readence Others Water + Sewer Gas I and Rept Property Tax Energy Head occupant. tenanti Collection use of residence ' 11.1 VAL/DART/IL/GRN/001 or institute to or • • • 6.1 1.16 1.1.101 11.1 VAL/DART/IL/GRN/002 The interview was not done 10.0 VAL/DART/IL/GRN/003 The role to review was in it dotter Owner-VAL/DART/IL/GRN/004 SOC/DART/IL/GRN/004 Male Individual house occupant Owner-35 000 00 VAL/DART/IL/GRN/005 SOC/DART/IL/GRN/005 Female Individual house 90,000,00 2 000,00 35 000 00 occupant Owner-VAL/DART/IL/GRN/006 SOC/DART/IL/GRN/006 Male 90.000,00 2 000,00 50.000,00 40.000,00 Individual house occupant Owner-VAL/DART/IL/GRN/007 SOC/DART/IL/GRN/007 12 000,00 119.000,00 Female Individual house occupant Owner-Room or apartment VAL/DART/IL/GRN/008 SOC/DART/IL/GRN/008 Male 72 000,00 10.000,00 56 000,00 44.000,00 20.000,00 20.000,00 in individual house occupant VAL/DART/IL/GRN/008 SOC/DART/IL/GRN/008A Male 250.000,00 50 000,00 2 000,00 30 000 00 50.000,00 Tenant Upper floor unit Owner-Room or apartment VAL/DART/IL/GRN/010 SOC/DART/IL/GRN/010 Male 90.000.00 2 000 00 100.000,00 40.000,00 occupant in individual house VAL/DART/IL/GRN/011 sol por no estern 1 ... 1 -106.1.1.11 1 g 11 (A) VAL/DART/IL/GRN/012 ocarrei no et ca. ж. а 10.001 10 10.100.10.00 and field · · · . 1.01 VAL/DART/IL/GRN/013 or fraction of from *1 a 1.00000 1 1 11 Owner-Room or anartmen VAL/DART/IL/GRN/014 SOC/DART/IL/GRN/014 Male 90.000,00 2 000,00 50.000,00 occupant in individual house 1.1.1 VAL/DAFT DOCKNOU we present of the 1.1 1101 00110 10.111 1.11 1.1.12.11 ... · VAL/DART/IL/GRN our or particulation 110 and analyzing 1 -... 114 VAL/DART/IL/GRN/017 SOC/DART/I The test to 1 400.000.00 1.00.000 لو وت . 10 VAL/DA6 [11 + KN 018 the Trip Diric North اللوالة ما ما 10.01 de come њ. н. 10 m m VAL/DART/IL/GRN/019 aperie de la constante de la seconda 5 . . . 1.1.11.11.1 dia ara here a _ 99 <u>_ 1</u> TT 1 7 VAL/DART/IL/GRN/020 on na fin debour 21.1 1.6.1.16.5 4 1 10 10 41.11.1 Owner-30.000.00 VAL/DART/IL/GRN/021 SOC/DART/IL/GRN/021 Male Individual house 2 000,00 occupant Owner-VAL/DART/IL/GRN/022 SOC/DART/IL/GRN/022 25 000.00 2.000.00 35 000 00 30.000 ... Female Individual house occupant Owner-SOC/DART/IL/GRN/023 2 000 00 30.000.00 30,000,00 VAL/DART/IL/GRN/023 Male Individual house occupant VAL/DAF OF KNOCK Ils of the way and a 19.00 Owner-VAL/DART/IL/GRN/025 SOC/DART/IL/GRN/025 Male 75 000,00 2 000,00 100.000,00 25.000,00 67.000,00 67 000,00 Individual house occupant

Annex 03 - Part 1

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Data Bank related to Sections 01, 02 and	Ub of the Socio-economic Survey - R	esidential	kessetllement (Juestionnarie		1		·· — · ī				·	
Code of Corresponding Property Survey	Code of this Sacio-Economic Unit	Family Head	Type of uccupant:	I ype of residence	Other	Cost of Rentul tenant)	Waler + Sewer	Garbage Collection	Energy	Gas	Land Rent	Ргоретту Тах	l there any commercial use of residence?
VAL/DART/IL/GRN/026	SOC/DART/IL/GRN/026	Male	Owner- occupant	Individual house			90			•, ,			
VAL/DART/IL/GRN/027	SOC/DART/IL/GRN/027	Female	Owner- occupant	Individual house				2 000,00	25 000,00				
VAL/DART/IL/GRN/028	SOC/DART/IL/GRN/028	Female	Owner- occupant	Individual house			20 000,00	4 000,00	30 000,00	30 000,00			
VAL/DART/IL/GRN/029	SOC/DART/IL/GRN/029	Male	Owner- occupant	Room or apartment in individual house		1	60 000 00	2 000,00			2 000,00	1 000,00	
VAL/DART/IL/GRN/030	SOC/DART/IL/GRN/030	Male	Owner- occupant	Individual house									
VAL/DART/IL/GRN/031	SOC/DART/IL/GRN/031	Female	Owner- occupant	Individual house			1,1110011.1		1				
VAL/DART/IL/GRN/032	SOC/DART/IL/GRN/032	Female	Owner- occupant	Individual house		1	60 000,00	2 000,00	20 000,00	30.000,00			
VAL/DART/IL/GRN/033	SOC/DART/IL/GRN/033	Male	Owner- occupant	Individual house									
VAL/DART/IL/GRN/034	ок ја кана куло л	1.1		16 J 17 16					4	, transm			
VAL/DART/IL/GRN/035	HE BUILTING PLOT		11	1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1					· · · · · · · · · ·	4			
VAL/DART/IL/GRN/036	an astītušeta	•11		the follow			1, 000101		1				
VAL/DART/IL/GRN/037	SOC/DART/IL/GRN/037	Female	Owner- occupant	Room or apartment in individual house			6 000,00	2 000,00	40 000,00	60.000.00	2 000,00	1.000-00	
VAL/DART/IL/GRN/038	SOC/DART/IL/GRN/038	Spouse	Owner- occupant	Individual house			95 000,00	7 000 00	50 000,00	45 000,00	2 000,00	1.000,00	
VAL/DART/IL/GRN/039	SOC/DART/IL/GRN/039	Male	Owner- occupant	Individual house			150 000 00	2 000 00		60 000,00			
VAL/DART/IL/GRN/040	SOC/DART/IL/GRN/040	Spouse	Owner- occupant	Individual house			360 000,00	2 000,00	30 000,00	25 000,00			
VAL/DART/IL/GRN/041	SOC/DART/IL/GRN/041	Male	Owner- occupant	Individual house			90.000,00	2 000,00	24 000,00	40 000,00			
VAL/DALT H CRAME.	an an a fan cath ca,	•1.1					1			11 410 10			
VAL/DART/IL/GRN/043	SOC/DART/IL/GRN/043	Male	Owner- occupant	Room or apartment in individual house		T	90.000,00	2 000,00	20 000,00				
VAL/DAMT IL GENIGI	Divention was not diffe		11 - 2 (11)										
VAL/DART/IL/GRN/045	SOC/DART/IL/GRN/045	Male	Owner- occupant	Individual house		T	32 000,00	2 000,00	40 000,00	51 000,00			
VAL/DART/IL/GRN/046	SOC/DART/IL/GRN/046	Male	Owner- occupant	Individual house			90.000,00	2 000,00	25 000,00	50 000,00			
VALPARTIC GROOP	те парти средски	۰. ·		t e aj de t te t tra			1	1	*+ 01 i i	1.0			
VALDART JE GRN 048	un frikt gehlun	· ·	11 	6.3 6.716				1					
VAL/DART/IL/GRN/049	SOC/DART/IL/GRN/049	Male	Owner- occupant	Individual house									
VAL/DART/IL/GRN/050	SOC/DART/IL/GRN/050	Female	Owner- occupant	Individual house			20 000,00	4 000,00	50 000,00	45 000,00			

Data Bank related to Sections 01, 02 and 06 of the Socio-economic Survey - Residential Ressetllement Questionnarie

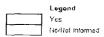


Annex 03 - Part 1

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Data Bank related to Sections 01, 02 and 0	6 of the Socio-economic Survey - R	esidential	Ressetllement (Questionnarie									<u> </u>
Code of Corresponding Property Survey	Code of this Socia-Fronomic Val	Family Head	Type of occupant	Type of residence	Other	Crea of Repluf tenants	Waler + Sewer	Garbage Collection	Energy	625	Land Reni	Реоретту Тах	l there any commercial use of residence?
VAL/DART/IL/GRN/051	SOC/DART/IL/GRN/051	Female	Owner- occupant	Individual house				20 000.00	20 000,00				
VAL/DART/IL/GRN/052	SOC/DART/IL/GRN/052	Female	Owner- occupant	Individual house				2 000,00	55 000,00				
VAL/DART/IL/GRN/053	SOC/DART/IL/GRN/053	Male	Owner- occupant	Individual house			60.000,00	4 000,00	25 000,00	25 000,00			
VAL/DART/IL/GRN/054	SOC/DART/IL/GRN/054	Male	Owner- occupant	Individual house			226 000,00	2 000 00	32 000,00	44 000,00			
VAL/DART/IL/GRN/055	SOC/DART/IL/GRN/055	Female	Owner- occupant	Individual house			45 000 00	2 500,00	25 000,00	30 000 00			
VAL/DART/IL/GRN/056	SOC/DART/IL/GRN/056	Male	Owner- occupant	Individual house				2 000,00	10 000,00	30 000,00		-	
VAL/DART/IL/GRN/057	SOC/DART/IL/GRN/057	Male	Owner- occupant	Individual house			17 000,00	2 000,00	80.000,00	52 000,00			
VAL/DART/IL/GRN/058	SOC/DART/IL/GRN/058	Female	Owner- occupant	Individual house			17 000 00	2 000,00	80.000,00	27 000,00			
VAL/DART/IL/GRN/059	SOC/DART/IL/GRN/059	1.1		1.46.51.201.5			100 101	. · · · · ·					
VAL/DART/IL/GRN/060	SOC/DART/IL/GRN/060	Male	Owner- occupant	Room or apartment in individual house			60 000 00	4 000 00	20 000,00				
VAL/DART/IL/GRN/061	SOC/DART/IL/GRN/061	Male	Owner- occupant	Individual house			12 000,00	2 000,00	30 000,00	27 000,00			
VAL/DART/IL/GRN/062	SOC/DART/IL/GRN/062	Male	Owner- occupant	Individual house			12 000,00	2 000,00	30 000,00	27 000,00			
VAL/DART/IL/GRN/063	SOC/DART/IL/GRN/063	Male	Owner- occupant	Individual house									
VAL/DART/IL/GRN/064	SOC/DART/IL/GRN/064	Malc	Owner- occupant	Individual house									
VAL/DART/IL/GRN/065	SOC/DART/IL/GRN/065	Male	Owner- occupant	Individual house			32 500,00	2 000 00	28 000,00	30 000,00			
VAL/DART/IL/GRN/066	SOC/DART/IL/GRN//%6	Male	Owner- occupant	Individual house			10 000,00	2 000,00	10.000,00	30 000,00			
VAL/DART/IL/GRN/067	or for englishing	•1.1		ויידי קיודי הו היידי לב 11									
VAL/DART/IL/GRN/068	en ferrir er en	-10		ا متر م بالمرا إما			1						
VAL/DART/IL/GRN/069	SOC/DART/IL/GRN/069	Spc -		ا ئىلىرى بىر يالىلايا بى				. 101101	4	1			
VAL/DART/IL/GRN/070	SOC/DART/IL/GRN/070	Male	Owner- occupant	Room or apartment in individual house			90.000_00	2 000 00	7 000,00	40 000,00	135 000,00		
VAL/DART/IL/GRN/071	SOC/DART/IL/GRN/071	Male	Owner- occupant	Room or apartment in individual house			20 (100_00	6 000,00	30 000,00	22.000,00			
VAL/DART/IL/GRN/072	SOC/DART/IL/GRN/072	Malc	Owner- occupant	Room or apartment in individual house			36 000,00	2 000,00	35 000,00	22 000,00		95 000,00	
VAL/DART/IL/GRN/073	SOC/DART/IL/GRN/073	Spouse	Owner- occupant	Individual house			45 000,00	2 000,00	35 000,00	44 000 00			
VAL/DART/IL/GRN/074	SOC/DART/IL/GRN/074	Spouse	Owner- occupant	Room or apartment in individual house			75 000,00	_ 10.01.101	1001 01	4.0.0			
VAL/DART/IL/GRN/075	SOC/DART/IL/GRN/075	Male	Owner- occupant	Individual house			25 000,00	15 000,00	45 000,00	10.000,00			

Data Bank related to Sections 01, 02 and 06 of the Socio-economic Survey - Residential Ressettlement Questionnarie



ANNEX 3 1 - RAP REPORT Residential Residential

, Code of Curresponding Property Survey	Code of this Socio-Economic Unit	Family Head	Type of occupant:	Type of residence.	Other [.]	Cuse of Rentisf tenants	Waler + Sewer	Garbage Collection	Energy	(jas	Land Rent	Property Tax	l there ar-v commercial use of residence '
VAL/DART/IL/GRN/076	SOC/DART/IL/GRN/076	Female	Owner- occupant	Room or apartment in individual house			45 000,00	2 000,00	15 000 00				
VAL/DART/IL/GRN/077	SOC/DART II + + ? II''	•11	• 11	tan jada						40.05.0			
VAL/DART/IL/GRN/078	SOC/DART ILLI	• • •		س=د. ر ∙ البرانياتين			. m i i i i i i i i i i i i i i i i i			. 400 19			
VAL/DART/IL/GRN/079	oo jerraa ke o			н., уль.) 			1 10 101			••••			
VAL/DART/IL/GRN/080	910 1011 1 1 1 1 ^{1 1} 000	•••1	··· .	1.1.1.11						1			
VAL/DART/IL/GRN/081	SOC/DART/IL/GRN/081	Male	Tenant	Room or apartment in individual house			20 000 00	2 000,00	40 000,00	40 000,00		200 000,00	
VAL/DART/IL/GRN/082	SOC/DART/IL/GRN/082	Male	Owner- occupant	Individual house				2 000,00	15 000,00	22 000,00			
VAL/DART/IL/GRN/083	SOC/DART/IL/GRN/083	Male	Owner- occupant	Individual house			45 000,00	2 000,00	20.000,00	80.000.00			
VAL/DART/IL/GRN/084	SOC/DART/IL/GRN/084	Female	Other	Individual house			90,000,00	2 000,00	35 000,00	36 000 00			
VAL/DART/IL/GRN/085	an isti⊺njir se	1.1.1	11 11					0.0 × 0.					
VAL/DART/IL/GRN/086	në përdatë i so	191.	19 s	11.11.									
VAL/DART/IL/GRN/087	SOC/DART/IL/GRN/087	Male	Owner- occupant	Individual house									
VAL/DART/IL/GRN/088	SOC/DART/IL/GRN/088	Female	Owner- occupant	Individual house									
VAL/DART/IL/GRN/089	SOC/DART/IL/GRN/089	Male	Owner- occupant	Individual house			15 000.00	2 000 00	35 000.00	27 000 00			
VAL/DART/IL/GRN/090	SOC/DART/IL/GRN/090	Female	Owner- occupant	Individual house			25 000 00	2 000,00	40.000,00	27 000,00			
VAL/DART/IL/GRN/091	SOC/DART/IL/GRN/091	Male	Tenant	Individual house		130 000,00	63 000,00	2,000,00	10 000,00	30.000,00			
VAL/DART/IL/GRN/092	SOC/DART/IL/GRN/092	Other	Owner- occupant	Individual house									
VAL/DART/IL/GRN/093	on Artein gebo	с р	0. 111	1.1.1.11				4 501101	1	11.16.10.11			
VAL/DART/IL/GRN/094	SOC/DART/IL/GRN/094	Other											
VAL/DART/IL/GRN/095	SOC/DART/IL/GRN/095	Male	Owner- occupant	Individual house									
VAL DART IL GKNUW	on Tellipitek' or			a a 196									
VAL/DART/IL/GRN/097	SOCALITIES	1 1		1.1.1.11			1		I	*. ••• •••			
VAL/DART/IL/GRN/098	SOC/III FT III FF FF	• • •		i ji taala			1	101101	1				
VAL/DART/IL/GRN/099	SOC/DART/IL/GRN/099	Female	Owner- occupant	Individual house			15 000,00	2 000,00	35 000,00	27 000,00			
VAL/DART/IL/GRN/100	SOC/DART/IL/GRN/100	Male	Owner- occupant	Individual house			90 000 00	2 000 00	38				

Data Bank related 1. Section 01. 02 and 06 of the Solicecom and Survey - Residential Respectively Operation pric

Annex 03 - Part 1



Annex 03 - Part 1

DitteBackrelated (Section) (11.01) and (The data Sectores of day Sarvey - R	tolenter!	Kevrelle ment	tor should the		T						·	
Fode of Corresponding Property Survey	Code of this Socio-Economic Linu:	Family Head	Type of occupant.	Type of residence	Other	Cost of Reniuf tenant)	Water + Sewer	Garbage Coljection	Energy	Gas	I and Reni	Property Tax	l there any commercial use of residence?
VAL/DART/IL/GRN/101	SOC/DART/IL/GRN/101	Male	Owner- occupant	Room or apartment in individual house			8 500 00	2 000,00	150 000,00	50 000 00			
VAL/DART/IL/GRN/102	SOC/DART/IL/GRN/102	Male	Owner- occupant	Room or apartment in individual house			12 000.(**	1 • 101 • 17	1 . 101.11.7	j			
VAL/DART/IL/GRN/103	SOC/DART/IL/GRN/103	Male	Owner- occupant	Room or apartment in individual house			75 000 00	1.000.00	35 000,00	60 000,00			
VAL/DART/IL/GRN/104	on intélation (104	й т	•• •• •	به منه منه منه المنه br>المنه المنه			16 11 101	4000.000	40.001 10	1. • ••• •••		-	
VAL/DART/IL/GRN/105	SOC/DART/IL/GRN/105	Female	Other	Room or apartment in individual house			30 000,00	2 000,00	20 000 00	88.000,00			
VAL/DART/IL/GRN/106	SOC/DART/IL/GRN/106	Other	Other	Room or apartment in individual house			30 000 00	2 000,00	20.000,00	88 000,00			
VAL/DART/KN/HN+ IN#	The art of the Alis for the			_									
VAL/DART/KN/INF mu	on Traff F. 11 Follow			1.1.1.11.					500 A.A.A.	1	1 00 101	_0.000000	
VAL/DART/KN/HNF/011	SOC/DART/KN/HNF/011	Male	Tenant	Room or apartment in individual house		20 000,00	15 000,00	2 000,00	5 000,00	30.000,00			
VAL/DART/KN/HNF/011	SOC/DART/KN/HNF/011A	Male	Tenant	Room or apartment in individual house		40.000,00	60 000 00	5 000 00	35 000,00	40 000,00			
VAL/DART/KN/HNt	CONTRACTOR OF THE	1.1	I · ·	1 1 1 1 1.1.1.1			1 1001111	1110-11-161					
VAL/DART/KN/HN+ IIII		н I	11	н ц.п <u>1.1.1.</u>			·		41000111				
VAL/DART/KN/HNF/011	SOC/DART/KN/HNF/011D	Male	Tenant	Room or apartment in individual house	_	20 000 00	3 000,00	2 000,00	4 000,00	3 000 00			
VAL/DART/KN/HNF/011	SOC/DART/KN/HNF/011E	Female	Tenant	Room or apartment in individual house		25 000,00	3 000,00	2 000,00	4 000,00	15 000 00			
VAL/DART/KN/HNF/011	SOC/DART/KN/HNF/011F	Male	Tenant	Room or apartment in individual house		20 0(H),(H)	10 000,00	1 000,00	5 000,00	25 000,00			
VAL/DART/KN/HNF/012	SOC/DART/KN/HNF/012A	Female	Tenant	Room or apartment m individual house		20 000,00	30 000,00	1 000,00	48 000,00	30 000,00			
VAL/DART/KN/HNF/012	SOC/DART/KN/HNF/012B	Female	Tenant	Room or apartment in individual house		20 000 00		5 000,00	12 000,00	50 000 00			
VAL/DART/KN/HNF/012	SOC/DART/KN/HNF/012C	Female	Tenant	Room or apartment in individual house			30 000,00	1 000,00	12 000,00	30.000,00		20 000,00	
VAL/DART/KN/HNF/012	SOC/DART/KN/HNF/012D	Female	Tenant	Room or apartment in individual house		20 000,00	30 000,00	1.000,00	48 000,00	30 000,00			
VAL/DART/KN/KMR/001	SOC/DART/KN/KMR/001	Male	Tenant	Room or apartment in individual house		15 000,00			15 000,00	15 000,00		12 500,00	
VAL/DART/KN/KMR/001	SOC/DART/KN/KMR/001A	Female	Owner- occupant	Individual house		ļ			15 000,00	22 000,00		12 500,00	
VAL/DART/KN/KMR/001	SOC/DART/KN/KMR/001B	Male	Tenant	Individual house		60 000.00			20 000,00	25 500,00		12 500,00	
VAL/DART/KN/KMR/002	SOC/DART/KN/KMR/002	Male	Owner- occupant	Individual house	<i>.</i>	L							
VAL/DART/5% 541k INI.			· . ·	1	L	· · · · · · · · · · · · · · · · · · ·				I 900.09		11110 1.1	
VAL/DART/NN KMR 100.	son as the test of the second	•1.1	10.0	ны арт ПоППБ					1 4			10000000	

Dita Bask related to Section (11) 11. and us of the Socione of the Survey - Recolution Reconflictment One approximation

Legend Yes Notifict informed

Annex	03 -	Part	1

Code of Corresponding Property Survey	Code of this Socio-Economic Unit:	Family Head	Type of occupant ²	Type of residence.	Other	Fost of Realist tensati	Waler + Sewer	Garbage Collection	Energy	Ga:	Land Ren	Ргоретту Тах	i there any commercial use of residence?
VAL/DART/KN/KMR/003	SOC/DART/KN/KMR/003	Male	Owner- occupant	Individual house					15 000,00	20 000,00		10 000,00	
VAL/DART/KN/KMR/003	SOC/DART/KN/KMR/003A	Female	Tenant	Room or apartment in individual house		60.000,00			15 000,00	22 000,00		10 000,00	
VAL/DART/KN/KMR/004	SOC/DART/KN/KMR/004	Female	Tenant	Room or apartment in individual house		12 000,00	15 000 00	3 000,00		19.000,00			
VAL/DART/KN/KMR/004	SOC/DART/KN/KMR/004A	Male	Tenant	Room or apartment in individual house		12 000,00	54 000,00	1 800,00		18.000,00			
VAL/DART/KN/KMR/004	SOC/DART/KN/KMR/004B	Male	Tenant	Room or apartment in individual house		12 000,00	24 000,00	8 000.00		19 000 00			
VAL/DART/KN/KMR/004	SOC/DART/KN/KMR/004C	Male	Tenant	Room or apartment in individual house		12 000,00	30 000,00	3 600,00		15 000 00			
VAL/DART/KN/KMR/004	SOC/DART/KN/KI 1F 199411	11,1	11 12	ا م. 16، سا سنا ، 1 ا ،			for the star				110 marite	1	
VAL/DART/KN/KMR/005	SOC/DART/KN/KL1+ ····	111	۰۰ . ۱۹۹۹ -	La totto -			•		1	.10000			
VAL/DART/KN/KMR/005	SOC/DART/KN/KMR/005A	Male	Tenant	Room or apartment in individual house		15 000,00	26 000,00	24 000,00	4 000,00	10 500 00			
VAL/DAKTKNKMR (8)	The interview cas but done		ī .u										
VAL/DARTEN MARINE	The interview was doit dome		T nu '										
VAL/DAFT KN KMK (807	In onterview was not dem		1 ,										
VAL/DART KN KMK mi	The interview was not done		• • •										
VAL/DAFTKNKM& 00°	Die uit die weerste Beitel nie		··· ·										
VAL/DART/KN/KMR/009	SOC/DART/KN/F 1 H IV	51 T	0	1.1.1.11.4									
VAL/DART/KN/KMR/010	SOC/DART/KN/KMR/010	Female	Owner- occupant	Individual house									
VAL/DART/KN/KMR/011	SOC/DART/KN/K*1E	F1		հայտ հայտել			11 101 1 101						
VAL/DART/KN/KMR/011	SOC/DART/KN/K*11	н. н	T 11	ե գոհառու ույն ռեռուհ		1.1	1 • • • •			1 901100			
VAL/DART/KN/KMR/011	SOC/DART/KN/KMR/011B	٩.,	ī1	ار عدر و تار _ thully		111 111 11	1		1011101	1 000 0			
VAL/DART/KN/KMR/011	SOC/DART/KN/KMR/011C	б хт	1.1.1	E i jiria 1. failt ,						10.10(1,11)			
VAL/DART/KN/KMR/011	SOC/DART/KN/KMR/011D	•1 •		16 a gant 19 a tra <u>ta</u> ti			1	1910-161					
VAL/DART/KN/KMR/011	SOC/DART/KN/KMR/011E	1 I	Ŧ	بې مېښې د او مېښې مېښې او مېښې				19.1.101		1			
VAL/DART/KN/KMR/012	SOC/DART/KN/KMR/012	21.1	الحياتي	F								1.1.1.1.1.1.1.1.1	
VAL/DART/KN/KMR/012	SOC/DART/KN/KMR/012A	Male	Tenant	Room or apartment in individual house		12 000,00			5 000,00				
VAL/DART/KN/KMR/012	SOC/DART/KN/KMR/012C		Tenant	Room or apartment in individual house		13 000,00			4 000,00				

Annex 03 - Part 1

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Data Bank related to Seed as or all and a	that the Secondermontes Survey - R	<u>entrettal</u>	Research	Purche Islandor				,					r
Code of Corresponding Property Survey	Code of this Socio-Economic Unit	Family Head	Type of occupant.	l upr of residence	Other	Cost of Restul tenant)	Water + Sewer	Garbage Collection	Energy	615	Land Reat	Property Tax	l there any commercial use of residence?
VAL/DART/KN/KMR/013	pipe (eta a tito e tae or	1.1	ī , ,				1 1011101						
VAL/DART/KN/KMR/013	en instruction (k. 1671) na	•1 1	T., 1	6.1.1.1		1 11111							
VAL/DART KN KMR III V	COLIMATE ENDING			11110		T.				1		111.18.11.10	
VAL/DAR1 KN KMR uus	NO DEFENSION	•1 1	1	6.0.160.		1	••••••			1 101101			
WAL/DART EN KMR uty	on interiet egite or u	• • • •	i	101-101									
VAL/DART KN KMR III 2	The interview was not done		T										
VAL/DART KN KMR opp	The this receives us as not done		انو (
VAL/DART KN KMK HI	The interview was net done		ī ,			T							
VAL/DART/KN/KMR/015	SOC/DART/KN/KMR/015	Female	Tenant	Room or apartment in individual house		40 000,00	45 000 00	15 000,00	5 000,00				
VAL/DART/KN 5 MR 405	The interview way not dony.		- 40 <u>-</u>										
VAL/DART/KN/KMR/018	SOC/DART/KN/KMR/018	Male	Tenant	Room or apartment in individual house		8 000,00			3 000,00				
VAL/DART/KN/KMR/018	SOC/DART/KN/KMR/018A	Male	Tenant	Room or apartment in individual house		10 000,00			3 000,00				
VAL/DART/KN/KMR/018	SOC/DART/KN/KMR/018B	Female	Tenant	Room or apartment in individual house		15 000,00							
VALOVETENEME	The one course to as not done.		۰ آ										
VAL DAF I KN KALEUR	The adviction was not \$100		1.1.1										
VAL/DAKT KN KMR of N	The other sew was in 1 done												
VAL/DAFT KN KMK 918	The mittreast was not define		Ι										
VAL/DAFT KN KMR OD	The interview was not done		1										
VALOVET KN KMK oux	The interview it is not d'inc		ī ı										
VAL/UART KN KMK UIA	The anti-train waves and done		1. I										
VAL/UAPTIKN KMR/05	The interview was used inc		F + +1										
VAL/UNRIANAMAUN	The interview was not done		ī . 1										
VAL/UARTEN KMKIIN	The interview is a not done		ي. ايم ري										
VAL/DART/KN/KMR/019	SOC/DART/KN/KMR/019	Female	Tenant	Room or apartment in individual house		15 000,00	5 000,00	2 000,00	5 000,00	5 000 00			
VAL/DART/KN/KMR/019	SOC/DART/KN/KMR/019A	Female	Owner- occupant	Individual house			15 000,00	5 000,00	15 000,00	21.000,00	15 000,00	10 000,00	

Data Bank related to Seed, or 01, 02, and 06 of the Second company, Subject V. Residented Reservement Questions re-

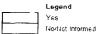


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Put Book related to Section 01/01 and 0	wolf the Soci-recolorable Shirvey - R	<u>esteniii</u>	Receillement	Questionals roe		· · · · · · · · · · · · · · · · · · ·							
Code of Corresponding Property Survey	Code of this Spece-Economic Luit	Family Head	Гурс об остиралі:	Type of residence.	Other	Cost of Rental tenants	Water + Sewer	Garbage Collection	Fnergy	Gu	L and Rent	Ргаренту Тат	l there any commercial use of residence '
VAL/DART/KN/KMR/020	on Tota Fact actions	• .		1.6.1.11									
VAL/DART/KN/KMŘ/020	The otherway was not dene	_											
VAL/DART/KN/KMR/021	SOC/DART/KN/KMR/021	Male	Owner- occupant	Individual house			30 000,00	500,00	20 000,00	30 000,00		30.000,00	
VAL/DART/KN/KMR/022	SOC/DART/KN/KMR/022	Female	Owner- occupant	Individual house			90 (00),00	2 000 00	15 000,00	22 000,00		10.000'00	
VAL/DART/KN/KMR/024	0.0.10.ETESEND.0.4			1.5.5.11.0		T					1.1.1.1.1.1.1.1		
VAL/DART/KN/KMR/024	SOC/DART/KN/KMR/024A	Female	Tenant	Room or apartment in individual house		20.000,00	12 090,00		5 000,00	21 000,00			
VAL/DART/KN/KMĸ ". i	The difference of the and district		1										
VAL/DART/KN/KMK #!+	The interview was not done		ī ·										
VAL/DART/KN/KNIK	The adverses to as not done		· . ·										
VAL/DART/KN/KMR/025	OF THE TENEN	1.1		64 1,01 5				1				1	
VAL/DART/KN/KMR/026	OF THE TAL ATTROP	1.1		1 1 1 11 ,	_	1111011101							
VAL/DART/KN/KMR/026	ten bereinen ette op s	н I	1.510	6.6 fair .		1	·			1 - 000110	1 10 00		
VAL/DART/KN/KMR/026	non fraktik sik Milogak	<u>•</u> •••		و الرومانا			·	5 n n n n			1 1110		
VAL/DAFT KN KMR 0.7	00 1126T (* 1871) (* 1	200	4.61.5	11.1.11.11.11		1	4			4,000.0	1 0111		
VAL/DAFT KN SMP #?"	COLUMN AND A DEC	1.		11.1.11.	_					1 .0.0111			
VAL/DART/KN/KMR/028	SOC/DART/KN/KMR/028	Male	Tenant	Individual house		80.000,00	124 000,00	4 000,00	16 000,00				
VAL/DART/KN/KMR/029	SOC/DART/KN/KMR/029	Female	1 1	F the protonom The first state		1 00 000	4		. · · ···	1 100 101			
VAL/DART KN KMK II. I	The INCLUSION WE PUT & DO		T .10										
VAL/DART KN KNR 020	The interview was not done												
VAL/DART/KN/KMR/029	the leafer of the open	11		нарти 16 ф. (16),		1	e 106-111	e 900-101		1 10110			
VAL/DART/KN/KMR/029	en região ha Maria de	11		11.111.						4 106 1			
VAL/DART/KN/KMR/031	SOC/DART/KN/KMR/031	Male	Tenant	Room or apartment in individual house		5 000,00	45 000 00	8 000,00	2 000,00	30.000,00			
VAL/DART/KN/KMR/031	on to kike a theory		1.1.11	ا ∾_ م م م م ار ار ما م				4					
VAL/DART/KN/KMR/031	SOC/DART/KN/KMR/031B	Male	Tenant	Room or apartment in individual house		5 000,00	45 000_00	4 800,00	3 000,00	30 000,00			
VAL/DART/KN/KMR/031	SOC/DART/KN/KMR/031C	Female	Tenant	Room or apartment in individual house		5 000,00	18 000.00	2 800,00	2 000,00	30 000,00			

Annex 03 - Part 1 Data Bank related to Sections 01, 02, and 07 of the Society undi. Survey . Residential Resouth n



Code al Corresponding Property Survey	Code of this Socia-Economic Unit	Family Head	Type of occupant	Type of readence:	Other	Cost of Remuf femants	Waler + Sewer	Garbage Collection	Energy	Gas	l and Reni	Ргоренту Тах	l there any commercial use of residence?
ALIDAR T/KN/KMK/031	The mean is a solution of		1 11			F							
VAL/DART/KN/KMR/03	The interview of a net store		Ť. I										
VAL/DART/KN/MG2/001	ern it ki k de jour		11	I to it. I to us			. 191 11 1	1 100 101				p	
VAL/DART/KN/MG2/001	regioner che pour -		1.	 For a start to For tool to a 		1,1.00.11	1 10111.1			1 0 1 00			
VAL/DART/KN/MG2/002	10.1011.1.1.1.1.1.1.1.1.1.1.1.1.1.1.1.1	•		11.1.1			1 1011111					111.00.010	
VAL/DART/KN/MG2/003	SOC/DART/KN/MG2/003	Male	Tenant	Room or apartment in individual house		10 000,00	30 000,00	15 000,00	8 000 00	1 461 (10)			
VAL/DART/KN/MG2/003	SOC/DART/KN/MG2/003A	Male	Tenant	Room or apartment in individual house			30 000,00	15 000,00	8 000,00	18 000,00			
VAL/DART/KN/MG2/003	SOC/DART/KN/MG2/003B	Male	Tenant	Room or apartment in individual house	-	10.000,00		-					
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VAL/DART/KN/MG2/005	SOC/DART/KN/MG2/005	Male	Tenant	Room or apartment in individual house		10 000,00	10.000,00	3 000,00	10 000 00	30 000,00			
VAL/DART/KN/MG2/005	SOC/DART/KN/MG2/005A	Female	Tenant	Room or apartment in individual house		10 000,00	10 000.00	3 000.00	10 000 00	45 000,00			
VAL/DART/KN/MG2/005	SOC/DART/KN/MG2/005B	Male	Tenant	Room or apartment in individual house		20 000,00	10 000.00	3 000,00	10.000.00	45 000,00			
VAL/DART/KN/MG2 uni	The rote way was not done		ī,										
VAL/DART/KN/MG2/007	SOC/DART/KN/MG2/007	Female	Tenant	Room or apartment in individual house		50 000,00		2 000,00	25 000,00	30 000 00		10.000,00	
VAL/DART/KN/MG2/007	SOC/DART/KN/MG2/007A	Male	Other	Individual house		100,000	90.000,00	2 000,00	40.000,00	10.000,00			
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VAL/DART/K 'S MINY 003	The interview was not done												
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Data Bank related to Sections 01, 02 and 06 of the Socio-economic Survey - Residential Ressetllement Questionnarie

Annex 03 - Part 1

Yes

Annex 03 - Part 1

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Code of Corresponding Property Survey	Code of ibis Socia-Economic Unit	Family Head	Type of occupant	Type of residence:	Other:	Cost of Rentuf tenants	Waler + Sewer	Garbage Collection	Energy	Gas	Land Reat	Ргоретту Тат	l there any commercia use of residence '
AL OAKT KN KND 000	FFNCE		··										
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VAL/DART/IL/GRN#01	SOC DART H. GRN 001	Other	other relative	54	18	Single	Basic School								I ull time	daladala	150	1 000 00
VAL/DART/II/GRV/001	SOC DART IL GRN on1	()ther	other relative	-		Single	Basic School								Full tune	cluðulað	150	1 500.00
VAL/DART/IL/GRN/001	SOC DARI IL GRN 601	Other	other relative	-	۳.	Single	Basic School								Full time	guidien	14	
V AUDART/IL/GRAMMA	SOC DART IL GRN 064	Famly Head	Not applicable	Σ	4	Manted	Intermediary School	Registered employ ee	other			other				none		
VAL/DART/IL/GRN/004	SOC DART IL GRN 004	Spoux	Not applicable	1 <i>4</i> -	Ŧ	Married	Intermediary School									none		
VALDART/IIJ/GRV@M4	SOC DART IL GRN m4	()ther	sou/daughter	- -	ก	Single	Intermediary School								Momug	ulabeleb	120	3 SHU (H)
VAL/DART/IL/GRN/004	SUC DART IL GRN 064	Other	sou/daughter		5	Single	Intermediary School								Morning	daladalu	100	00.000
V VL/D ART/IL/GRA/#04	SOC DARF IL GRN 004	Other	สติปิคธิม	-	21	Single	Basic School									none		
VAL/DART/IL/GR//005	SOC DART IL GRN 005	Famly Head	Nni applicable	ш	57	Widow	Basic School		Retirement	100.000		none				Dividio		
VAL/DART/IL/GRA/005	SOU DART IL GRN 1915	Other	son/daughter	Σ	Â	Single	Intermediary School									aone		
VAL/DART/IL/GRN#05	SOC DART IL GRN 005	Other	sou/daughter	Σ	#	Småle	Intermediary School									auru		
VAL/DAR E/H/GRN 1905	SOC DART IL URN 005	(Xher	grandson/grandaug hter	Σ	12	Single	Basic School			1					Full time	فاماتنامك		1 600 00
VAL/DART/IL/GR/0005	SOC DART IL GRN 005	Other	grandson/grandaug hter	×	20	Sngh	Hasic School	_							Full time	car-motorbike		11 509 00
VAL/DART/IL/GRA/005	SOC DART IL URN 805	Other	tather/mother	i.,	9L	Wadow	None									Ikelle		
VAL/DART/II/GRN/005	SOC DART IL GRN 005	(The	son/dunghter	v	96	Маглед	University	Registered employee	Uther							hoffe		
VAL/DART/IL/GRN/005	SOC DART IL GRN 005	Other	son/daughter	-	x	Mamed	Intermediary School									ftone		1
VAU/DART/IL/GRN/005	SOC DART IL GRN 905	Other	aggregules	¥	~	Single	Hinder 2 vears								Full time	Lar-motorbike		7 500 00
VAUDART/ILURN/005	SOC DART IL OKN 805	(the	ษฏิธีรายเรลง สมุขิ	щ	9]	Live together	Basic School	Informat		20.000,00		daladala				thouse		
VAL/DART/IL/GRN/806	SOC DART IL GRN 006	Family Head	Not applicable	×		Married	Intermediary School	Registered emplistee	Retarment	30.000.00	52	car-motorbike	60			itethe		
V AL/D ART/IL/GRN/006	SUC DART IL GRN 006	Spouse	Not applicable	ł	÷	Маттеd	Basu Schuid	Self-employed	Other	300 (NN) 00	1	ttone				none		
VAL/DART/IL/GRN/006	SOC DARI IL GRN 1046	Other	son/daughter	м	ň	Smgle	Intermediary School								Full time	دادئدام	120	2141100
VALIDART/IL/GRN/006	SOC DARI IL GRN 606	Other	son/daughter	Σ	ล	Live together	Live together Intermediary School					-			i ull tune	daladala	120	(X) (301-

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ANN: - RAP REPORT Residential - - - i-ment - Dwellers ,

Annex 03 - Part 2

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AAL/DARF/IL/GRN/008	· DARTH GRN 008	Spouse	Not applicable		2	Married	Intermediary School	Ň		4 500 000 00	-	car-motorbike	66	16 800 00		Bolic		
VAL/DART/IL/GRN/008	 DART IL GRN 008 	Other	son/daughter		9	Single	Intermediary School								Full tune	car-motorbike	5	25 001,00
VAL/DART/IL/GR//008	· · DART IL URN 008	Other	sou/daughter	×	=	Smgle	Basic School								Full time	other	120	20.250.00
AL/DARE/IL/GRN/008	· · · DART IL GRN 008	()ther	son/daughter	Σ	2	Smgle	[weirs School]								Full tune	other	921	20 250 00
CAL/DART/II/GRN/908	DART IL GRN 0.08	()ther	son/daughter	-	9	Single	Basic School								Full tune	other	021	20.250.00
AL/DARI/IL/GRN/008	- DART IL GRN INS	Other	aggregutes		61	Surgle	Bask School	Registered1		20.646.48	-	нопе				ane	,	
VAL/DART/IL/GRN/008	· · DART II, GRN 90%	Other	าธิธันธริง	×	17	Smgle	Basic School	employee		20.000.10	-	none				lante		
VAL/DART/IL/GRN/008	· · · DART IL GRN 008	Other	other relative	Σ	32	Single	Intermedian School	Self-employ ed		3661 (883) (90)	-	rar-molorhike	69	16 810 00	-	none		
VAL/DART/II/GRN/008	- DART IL GRN 008A	Family Head	d Not applicable	¥	с г	Маптед	Intermediary School	Self-employed		00.000.000.00	12	car-motorbike	021	16 800 00		опо		
VAL/DART/IL/GRN/008	· · DART IL GRN 188A	Spoure	Net apply, able	بدر	æ	Married	Internediary School	Self-employed		6 (NR) (NR) (N)	CI	ear-motorbike	8	10.800,00		none		
VAU/DART/IL/GRN/008	· · DART IL GRN 008A	Other	other relative	Σ	22	Married	Basic School	Self-employed		6 000 000 00	13	cur-motorbile	120	1o 800 nu		none		
VAL/DART/IL/GRN/008	 DART IL GRN 008A 	Crthei	wn/daughter	W	15	Single	Intermediary School								Full time	clabeleb	130	4140 00
VAL/DART/IL/GRN/008	V DART II CRN 008A	Other	son/daughter	L.	2	Single	Basic School								l'ull tanc	eirbtisb	120	00.001
VALDART/IL/GRN/008	• • DART IL GRN 008A	()ther	son/daughter	ж.	2	Single	Basic School								Full time	daladala	120	100.001
AL/DART/IL/GRN/008	••• DART IL GRN 008A	Other	sur/daughter	4	×	Single	Basic School								Full time	ulabeleti	120	400.00
AL/DART/IL/GR//008	· · DART IL GRN 098A	Other	wn/daughter	ш	\$	Smgle	Basic School								Full time	daladala	120	00 001
VAL/DART/IL/GRN/008	" DART IL GRN (08A	Other	sou/daughter	н.	3	Single	Basic School								} ull tune	daladala	120	(3) (NH
VAL/DART/IL/GRN/008	· · · DART IL GRN 108A	Other	son/daughter		91	Single	Basic School	Registered employee		(NI 19)1 ()†	0	car-motothike				Done		

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Annex 03 - Part 2 Data Bank fed nod to Sections (11, 02) and the of the Section Section Section (11, Research in the Orienting are

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WAL/DART/IL/GRN/013	SOC DART IL GRN 013	Other	son/daughter	-	27	Single	Basic School									none		

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Annex 03 - Part 2

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VAL/DART/IL/GRN/014	SOC DART IL GRN 014	Family Head	Nut applicable	Σ	ē	Married	Basic School	Registered employee		300.000.00	5	daladala	180	2 (60) (60		tanne		
VAL/DAR F/IL/GR/v014	SOC DART IL GRN 014	Spouse	Not applicable	u.	9	Married	None									bunc		
VAL/DART/IL/GRN/014	SOC DART IL GRN 014	Other	son/daughter	×	8	Single	Intermediary Schuol								Full time	car-motorbike		20.0141.00
VAUDART/IL/GRN/014	SINC DART IL GRN 014	(ather	son/daughter	Σ	61	Single	Intermediary School								Atternoom	tebeleb	180	600 IKI
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VAL/DART/IL/GRN@14	SOC DART IL GRN 014	Other	son/daughter	м	а	Single	Basic School								Aftermoom	ฐแม่โนพ		
VAL/DART/IL/GRN/014	SOC DART IL GRN 014	Other	son/daughter		z	Smgle	Basic School								Afternoom	wulking		
V.AL/DART/IL/GRN/015	SOC DART IL GRN 015	Family Head	Not applicable	Σ	×	Матед	Intermediary School	R agistered employ ce		200.000.00	32	sinter	99	3 500 00		none		
VAL/DART/IL/GRN/015	SOC DART IL GRN 015	Springe	Not applicable	- in	*	Married	Basic School									none		
VAL/DART/IL/GRN/015	SOC DART IL GRM 015	Other	son/daughter	N	2	Single	Intermediary School								Full time	clabalab	120	800,00
VAL/DART/IL/GRN/015	NOC DART IL GRN 015	Other	son/daughter		2	Single	Basic School						_		Full tune	daladala	120	801.00
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VAL/DART/IL/GRN/016	SOC DART IL GRN 015	Vamily Head	Not applicable	-	4	Single	Intermediary School	Registered employee		204-050-002	0F	daladala	ŝ	2 (00) 00		none		
VAL/DART/IL/GRN@16	SOC DART IL GRN 016	Other	son/daughter	Σ	2	Single	Intermediary School								Мопиле	daladalı	ΰ	100,00
VAL/DART/IL/GRN/016	SOC DARL IL GRN 016	Other	son/daughter	-	=	Single	Basic School								Мотив	alabelab	120	100 001
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V AL/DART/11/GRN/017	SOC DART IL GRN 017	Family Head	Not applicable	W	π	Married	Basic School	Registered employee	Other		ñ	daladala	<i>;</i> ;	3 000 00		pupe		
VALIDART/ILIGRN#17	SOC DAKI IL GRN 017	Spouse	Not applicable		э.	Matried	Basic School							-		none		
VAL/DART/IL/GRN/017	SOC DART II. GRM 017	Other	sou/daughter	¥	24	Smgle	Basic School									none		

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Annex 03 - Part 2 Dart Bark related to Sections (1), 112 and 106 of the Section rengention Survey Bevels prival Ressettly propt Outschemmerten

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		;									Intermediary School	Single			other relative	Other	SOC DART IL GRN 021	VAL/DART/IL/GRN/021
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VAL/DART/IL/GRN/031	SOC DART IL GRN 931	Other	son/daughter	ž	¥7	Smgle	(1) must stip						_		Full time	none		
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VAL/DART/II/GRN/043 SOL DAF	SOC DART IL GRN 043	Other	son/daughter	-	2	Single	Base School								Afternoom	walking	120	
V VLDAR F/IL/GRN/043 SUC DA	SOC DART IL GRN 043	Other	son/daughter	Σ	4	Single	Internediary School								Full tune	car-motorbike		00.000
VAL/DART/IL/GRA/043 SOC DAF	SOC DARI IL GRN 043	Other	son/daughter	W	•	Single	Basic School								Afternoom	walking	120	
VAL//DART/IL/GR/043 SOC DAF	SOC DART IL GRN 043	Other	son/daughter	u.	7	Single	None								Afternoom	walking	120	
VALDARI/IL/GRN/045 SOCIDA	SOC DART IL GRN 045	Family Head	Not applicable	×	65	Matried	Basic School	Registered employce			ŝł	riphalab	10	2 800 00		hone		
VALIDART/IL/GRA/046 SOC DAF	SOC DART IL GRN 045	Spouse	Not applicable	14	7	Маттеd	Basic School									none		
VALDART/IL/GRN@45 SCC DAF	SOC DARLIL GRN 045	Other	son/daughter	м	52	Single Ii	Intermediary School									nene		
VAL/DART/IL/GRN/HS SOC DAF	SOC DART IL GRN 045	Other	son/daughter	-	ព	Smglc	University								l ull time	anon		
VALIDART/IL/GRN/045 SOC DAF	SOC DART II, GRN 045	Other	son/daughter	W	17	Single	Intermediary School								Full time	none		
VAL/DART/IL/GRN/045 SOC DAF	SOC DART IL GRN 145	Other	son/daughter	1	Fi -	Single	Internediary School								Мотице	daladala	3	100.00
VAL/DART/IL/GRN/045 SOC DAF	SOC DART IL GRN 045	Other	satugar	-	5	Single	Basic School									THING		
VAL/DART/IL/GRA/045 SOC DA	SOC DART IL GRN 045	Other	grandson/grandaug hter	Ŀ	-	Single	None									none		
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VAL/DAR1/IL/GRN/046 SOC DAI	SOC DART IL GRN 046	Sphure	Not applicable	+	68	Married	Intermediary School									anne		
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VAL/DART/IL/GRN/046 SOY D/I	SON DART IL GRN 046	Other	other relative	м	tt.	Single	Basic School									none		
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VAL/DART/IL/GRA/054	SOC DARI IL GRN 054	Family Head	Not applicable	Σ	×	Matried	Intermediary School	Registered employce		001000	51	daladala	04	2 400,00		none		
VAL/DART/IL/GRN/054	SON DART IL GRN 054	Spouse	Not applicable	ы	न े	Matried	Basic School						_			none		
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VAUDARI/IL/GRN#54	SOU DARE IL GRN 054	Cither	sou/daughter	-	<u> </u>	Single	Intermediary School									alleti		
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VAL/DART/IL/GRN/#56	SOC DART IL GRN 056	Family Head	Not applicable	×		Married	University	Registered enployee			22	dahaha	97	000 t		none		
VAUDART/IL/GRN/056	SOC DART IL GRN 056	Spouse	Not applicable	н.		Маптед	Basic School									2UPU UPUC		
VAL/DART/IL/G.R.V.056	SUC DART IL GRM 050	Other	son/daughter	Σ	×	Single	Intermediary School								Moming	car-motorbike	9	
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VAL/DART/IL/GRN#61	SUC DART IL GRN 061	Spouse	Not applicable	ц.	ż	Live together	Intermediary School									none		
VAU/DAR1/IL/GRN/961	SOC DART IL GRN 061	Other	son/daughter	ч	ñ	Single	Intermediary School				-				Morming	duladala	921	100,00
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VAUDART/IL/GRN#61	SOC DART IL GRN 061	Other	son/daughter	-	*	Single	Internediary School								Morang	daladala	021	100.00
VAL/DART/IL/GRN/061	SOC DART IL GRN 061	Other	son/daughter	-	2	Single	Basic School								Merning	daladala	105	100.00
VALIDART/IL/GRA/061	SOC DART IL GRN 061	Other	grandson/grandang hter	W	7	Single	Intermediary School								Morrang	ulabulu	1100	IN) (K) I
VAL/DART/IL/GRN/961	SOC DARL IL URN 061	Other	grandson/grandaug	Σ	2	Single	Basic School	Q/N#							Morning	hlabalah	100	100,00
V AL/DART/IL/GRN/#62	SOC DART IL GRN 062	Family Head	Not applicable	×	(IS	Live together	Internuctiary School	Registered employee		200.000.00	20	dabalab	ŝ	2 000 00		none		
VAL/D4R1/II/GRN/062	SOC DART IL GRN 062	shods	Not applicable	и.	9	Live together	Intermediary School		-							none		
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VAL/DART/IL/GRN/962	SOC DARI IL URN 062	Other	son/daughter	is.	8	Single	Intermediary Schoul								Morning	daladala	120	100.06
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VAL/DART/IL/GRN/963	SOC DART IL GRN 063	Family Head		Σ	s.	Married	Basic School	Registered employee		120 000 00	35	car-motorhike	120	4 000 00		ацоп		
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VAL/DART/HL/GRN/066	SOC DARI IL GRN 866	Other	son/daughter	W	10	Single	Basic School									none		
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Annex 03 - Part 2

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A AUD ART/IL/GRN/079	SOC DART IL GRN 079	Other	son/daughter	Ŀ	R	Single	Intermediary School								Full time	walking	'n	
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VAL/DART/IUGRN#79	SOC DARI II. GRN 079	Other	sou/daughter		2	Single	Intermediary School									none	-	
VAL/DART/IL/GRN/080	SOC DART IL GRN 080	Family Head	Not applicable	Σ	3	Live together	Intermediary School	Registered employee		560.000.00		daladala	8	2 8/00 00		none		
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VAL/DART/IL/GRN/IM	SOC DART IL GRN 101	Other	son/daughter	Σ	5	Single	Intermediary School									none		
VAL/DART/IL/GRN/101	SOC DART IL URN 101	Other	other relative	Σ	ž,	Single	Basic School									none		
VALIDART/IJ/GRN/101	NOC DART IL GRN 101	Other	son/daughter	L .	12	Single	Intermediary School								Mortung	other	120	\$0.000
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VAL/DART/IL/GRN/102	SOC DART IL GRN 102	Spouse	Not applicable	ш.	Ş	Mairsed	Intermediary Schoul	Self-employed		100 000	12	daladala	180	15 (00:00	i	auou		
VAL/0ART/IL/GRN/102	SOC DART IL URN IN2	Öther	son/daughter	×	<u> </u>	Single	Basic School								Morning	gurdhrag	5 <u>7</u>	
VAL/DART/IL/GRN/102	SOC DART IL GRN 102	Other	son/daughter	Σ	2	Single	Under 2 vears								Monung	daladala	18()	45 1800 001
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VAL/DART/IL/GR/M02	SOU DART IL GRN 102	Other	other relative	ш. 	21	Sugle	Basic School									none		
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VAL/DART/IL/GRN/103	SOC DART IL GRM 103	Spouse	Not applicable	Σ	×	Murried	Intermediary School o	employ.cc		90 000 11	28	ելեներ	15	2 800140		tione		

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VAL/DART/IL/GRN/106	SOC DARI II. GRN 106	Other	other relative	ŀ	25	Single	Intermediary School								Ocasional	daladala	80	500.00
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VAL/DART/IL/GRN/106	SOC DART IL GRN 106	Other	son/daughter	м		Single	None									Isone		
VAL/DART/KN/INF/010	SOC DART KN HNF 010A	Family Head	Not applicable	F	53	Married	Intermediary School	Registered employee			31	daladala	-00	7 000 00		none		
VAL/DART/KN/UNF/010	SOC DAR1 KN HNF 010A	Spouse	Not applicable	м	-46	Matried	Internediary School									none		
VAL/DART/KN/HNF/010	SOC DART KN HNF 010A	Other	Not applicable	м	58	Married	Intermediary School									none		
VAL/DART/KN/HNF/010	SOC DARI KN HNF 010A	Other	son/daughter	м	35	Single	Basic School									none		
VAL/DART/KN/HNF/010	SOC DART KN HNF 010A	Other	son/daughter	м	27	Single	Intermediary School									none		
VAL/DART/KN/INF/010	SOC DART KN HNF 010A	Other	son/daughter	F	20	Single	Intermediary School									none		
VAL/DART/KN/HNF/010	SOC DART KN HNF 010A	Other	son/daughter	F	24	Single	Intermediary School									none		
VAL/DART/KN/HNF/010	SOC DARI KN HNE 010A	Other	son/daughter	м	14	Single	Intermediary School									none		
VAL/DART/KN/HNF/010	SOC DART KN HNF 010A	Other	son/daughter	м	18	Single	Intermediary School									flone		
VAL/DART/KN/HNF/010	SOC DART KN HNF 010A	Other	son/daughter	г	17	Single	Intermediary School									none		
VAL/DART/KN/HNF/010	SOC DART KN HNF 010A	Other	grandson/grandaug hter	м	-4	Single						1				none		
VAL/DART/KN/HNF/010	SOC DART KN HNF 010A	Other	other relative	м	-18											none		
VAL/DART/KN/HNF/011	SOC DART KN HNF 011	Family Head	Not applicable	м	-41	Live together	Basic School	Registered employee		100 000 00	3	daladala	64	30.000.00		none		
VAL/DART/KN/HNF/011	SOC DART KN HNF 011	Spouse	Not applicable	F	27	Lave together	Basic School									none		
AL/DAR1/KN/IINF/011	SOC DART KN IINF 011	Other	son/daughter	м	10	Single	Basic School								Full time	daladala	ykı.	15 000,00
VAL/DART/KN/INF/011	SOC DART KN HNF 011	Other	son/daughter	ł	9	Single	Basic School								Full ume	daladala	30	10 000 00
VAL/DART/KN/HNF/011	SOC DART KN HNF 011A	Family Head	Not applicable	м	18	Married	Basic School	Intormal		200.000.00	5	daladala	60	7 000 00		none		
VAL/DART/KN/HNF/011	SOC DART KN HNF 011A	Spouse	Not applicable	Г	38	Married	Basic School	Informal		200 000 00	5	daladala	60	7 000 00		none		
VAL/DAR1/KN/INF/011	SOC DART KN HNF ULLA	Other	son/daughter	F	14	Single	Basic School								Full time	car-motorbike	30	
VAL/DART/KN/INF/011	SOC DARI KN HNF OLIA	Other	son/daughter	1	- 11	Single	Basic School			1					Fuli time	car-motorbike	,30	
AL/DART/KN/HNF/011	SOC DARI KN HNF 011B	Family Head	Not applicable	м	60	Married	Intermediary School	Informal	Returnent	100 000 00	20	daiadala	60	30.000.00		none		
AL/DART/KN/BNF/011	SOC DART KN HNF 011B	Spouse	Not applicable	F	55	Married	Basic School	Informal	Retirement	80.000.00	20	daladala	60	30.000.00		none		
AL/DART/KN/HNF/011	NOC DART KN HNF 011B	Other	son/daughter	м	25	Married	University								l ull time	daladala	60	1 500,00
AL/DART/KN/HNF/011	SOC DART KN HNF 011B	Other	son/daughter	м	23	Single	Intermediary School			1					Full time	daladala	60	1 509 00
AL/DART/KN/HNF/011	SOC DART KN HNF 011B	Other	son/daughter	ŀ	20	Single	Intermediary School								Full time	daladala	60	1.500.00
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VAL/DART/KN/HNF/011	SOC DART KN HNF OLDB	Other	son/daughter	ł	16	Single	Interniediary School								Full time	dələdalə	60	1 500,00

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AL/DART/KN/KMR/013	SOC DART KN KMR 013C	Other	son/daughter	ł	10	Single	Basic School								Morning	daladala	120	100.00
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ALD ART/KN/KMR/013	SOC DART KN KMR 013D	Family Head	Not applicable	F	30	Married	Basic School	Registered employee		200 (00) 00	13	daladala	69	600.00		none		
AL/DART/KN/KMR/013	SOC DART KN KMR 013D	Spouse	Not applicable	F	28	Married	Intermediary School									none		
AL/DART/KN/KMR/013	SOC DART KN KMR 013D	Other	son/daughter	F	7	Single	Basic School			_					Morning	walking	10	
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AL/DART/KN/KMR/018	SOC DART KN KMR 018	Family Head	Not applicable	м	30	Live together	Basic School	Self-employed		400 000 00	8	daladala	20	400.00		none		
AL/DART/KN/KMR/018	SOC DART KN KMR 118	Spouse	Not applicable	F	29	Live together	Basic School									noue		
AL/DART/KN/KMR/018	SOC DART KN KMR 018	Other	son/daughter	F	1	Single										none		
AL/DART/KN/KMR/018	SOC DART KN KMR 018A	Family Head	Not applicable	м	40	Married	Basic School	Informal		350 000 00	4	walking	20			пове		
AL/DART/KN/KMR/018	SOC DARI KN KMR (184	Spouse	Not applicable	ŀ	35	Married	Basic School									none		
AL/DART/KN/KMR/018	SOC DART KN KMR 018A	Other	son/daughter	м	14			[Morning	daladala	٦ü	200.00
AL/DART/KN/KMR/018	SOC DART KN KMR 018A	Other	son/daughter	м	7										Morning	walking	10	
AL/DART/KN/KMR/018	SOC DART KN KMR 018A	Other	son/daughter	F	3											none		
AL/DART/KN/KMR/018	SOC DART KN KMR 018B	Family Head	Not applicable	ł	25	Separated	Basic School	Informal		300.000,00	3	daladala	30	400,00		none		
AL/DART/KN/KMR/018	SOC DART KN KMR (18B	Other	son/daughter	ŀ	-										Morning	walking	15	
AL/DART/KN/KMR/019	SOC DART KN KMR 019	Family Head	Not apphcable	F	25	Married	Basic School									none		
AL/DART/KN/KMR/619	SOC DART KN KMR 019	Spouse	Not applicable	м	30	Married	Intermediary School	Self-employed		80.000.00	9	daladala	80	1 500 00		none		
AL/DART/KN/KMR/019	SOC DART KN KMR 019	Other	son/daughter	м	5	Single	Basac School								Mortung	daladala	80	700,00
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AL/DART/KN/KMR/019	SOC DART KN KMR 019A	Other	son/daughter	F	37	Single	Basic School	Self-employed		70 000 00	U	walking	5			none		
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AL/DART/KN/KMR/019	SOC DART KN KMR 019A	Other	son/daughter	м	42	Marned	Basic School	Seil-employed		68 000 (K)	10	daladala	120	1.000.00		none		1
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AL/DART/KN/KMR/019	SOC DART KN KMR 0194	Other	son/daughter	м	20	Married	Basic School	Self-employed		40.000.00	7	daladala	48	1 000 00		none		
AL/DAR F/KN/KMR/019	SOC DART KN KMR 019A	Other	son/daughter	м	27	Single	Basic School	Self-employed		30.000.00	5	daladala	80	L 000 00		none		1
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Annex 03 - Part 2 Data Bark related to Sections 01, 62, and benefitie Social concerns Survey - Result retail Rescribing on the stormatic

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											Basic School	Married	±	F	Not applicable	Spouse	SOC DART KN MG2 005A	VAL/DAR F/KN/MG2/005
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Annex 03 - Part 3 Data Bank related to Sections 01 and 02 of the Socio-economic Survey - Commercial Activities Ouestionnarie

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Code of Corresponding Property Survey	Code of this Socio-Economic Unit	family Head	Oiner.	Commercial Activity Type	Type of occupancy	Property Type	Other
VAL/DAK LIL/GRN/001	Definition who is a dome					, i.u. 1	
VAL/DART/IL/GRN/001	SOC/DART/IL/GRN/001	Male		Vehicle related services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/002	SOC/DART/IL/GRN/002	Male		Vehicle related services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/002	The microscol was not done				ī	Not interviewed	
VAL/DART/IL/GRN/002	SOC/DART/IL/GRN/002A	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/003	SOC/DART/IL/GRN/003	Male		Vehicle related services	Tenant	Stand-Alone Kiosk	
VAL/DART/IL/GRN/003	SOC/DART/IL/GRN/003A	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/003	SOC/DART/IL/GRN/003B	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/003	SOC/DART/IL/GRN/003C	Male		Other retail or services	Tetnant	Kiosk attached to building	
VAL/DART/IL/GRN/004	SOC/DART/IL/GRN/004	Female		Other retail or services	Тспапt	Kiosk attached to building	
VAL/ TAKİ IL TIKNING	The outroien was not done			1 1 10 1	1	1910aa 1	
VAL/DART/IL/GRN/004	SOC/DART/IL/GRN/004A	Male		Other retail or services	Tenant	No answer	
VAL/DART/IL/GRN/005	SOC/DART/IL/GRN/005	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/005	The other new was not done			F I i al	Tenant	Not interviewed	
VAL/DART/IL/GRN/005	SOC/DART/IL/GRN/005A	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/006	SOC/DART/IL/GRN/006	Female		Restaurant/ bar/ food&leisurc/ hotel	Property Owner	Kiosk attached to building	
VAL/DART/IL/GRN/006	SOC/DART/IL/GRN/006A	Male		Vehicle related services	Tenant	Stand-Alone Kiosk	
VAL/DART/IL/GRN/006	SOC/DART/IL/GRN/006B	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/006	SOC/DART/IL/GRN/006C	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/008	SOC/DART/IL/GRN/008	Male		Other retail or services	Tenant	Kiosk attached to building	
AL/DART/IL/GRN/008	SOC/DART/IL/GRN/008A	Female		Other retail or services	Tenant	Kiosk attached to building	
/AL/DART/IL/GRN/008	SOC/DART/IL/GRN/(108B	Male	male v na male	Other retail or services	Tenant	Kiosk attached to building	
AL/DART IL GRAMM	DE ELENIER VIELEN deme				Tenant	Not interviewed	
/AL/DART/IL/GRN/008	SOC/DART/IL/GRN/008C	Male		Other retail or services	Tenant	Kiosk attached to building	

ANNEX 3 3 - RAP REPORT Commercial Ressettemnt -----

Annex 03 - Part 3 Data Bank related to Sections 01 and 02 of the Socio-economic Survey - Commercial Activities Questionnarie

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Data Bank related to Sections 01 and Code of Corresponding Property Survey	Code of this Socia-Economic Unit	Family Head	Other:	Commercial Activity Type:	Type of occupancy	Property Type	(Inher.
VAL/DART/IL/GRN/009	SOC/DART/IL/GRN/009	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/009	SOC/DART/IL/GRN/009B	Male		Other retail or services	Tenant	No answer	[
VAL/DART/IL/GRN/009	SOC/DART/IL/GRN/009C	Male		Other retail or services	Tenant	Kiosk attached to building	[
VAL/DART/IL/GRN/009	SOC/DART/IL/GRN/009E	Malc		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/010	SOC/DART/IL/GRN/010	Female		Restaurant/ bar/ food&leisure/ hotel	Property Owner	Kiosk attached to building	
VAL/DART/IL/GRN/011	SOC/DART/IL/GRN/011	Female		Other retail or services	Tenant	Kiesk attached to building	
VAL/DART/IL/GRN/011	SOC/DART/IL/GRN/011A	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/011	SOC/DART/IL/GRN/011B	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/012	SOC/DART/IL/GRN/012	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/012	SOC/DART/IL/GRN/012A	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/012	SOC/DART/IL/GRN/012B	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/013	SOC/DART/IL/GRN/013	Male		Other	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/013	SOC/DART/IL/GRN/013A	Male		Other	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/013	SOC/DART/IL/GRN/013B	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL DART IL CRAMA	The other station of 28 feet dools.			Residential	Tenant	Not interviewed	
VAL/DART/IL/GRN/013	SOC/DART/IL/GRN/013C	Female		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/015	SOC/DART/IL/GRN/015	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/015	The oten of a prince have			Residential	Tenant	Not interviewed	
VAL/DART/IL/GRN/015	SOC/DART/IL/GRN/015A	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/016	SOC/DART/IL/GRN/016	Female		Other retail or services	Tenant	No answer	
VAL/DART/IL/GRN/016	SOC/DART/IL/GRN/016A	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/016	SOC/DART/IL/GRN/016B	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/016	SOC/DART/IL/GRN/016C	Female		Restaurant/ bar/ food&leisure/ hotel	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/017	SOC/DART/IL/GRN/017	Male		Other retail or services	Tenant	Kiosk attached to building	

ANNEX 3.3 - RAP REPORT Commercial Ressetternnt -

Annex 03 - Part 3 <u>Data Bank related to Sections 01 and 02 of the Socio-comonic Survey - Commercial Activities Questionnatic</u>

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Code ai Corresponding Property Survey	Code of this Socia-Economic Unit	Family Head	Other:	Commercial Activity Type:	Type of occupancy	Property Type	Other:
VAL/DART/IL/GRN/017	SOC/DART/IL/GRN/017A	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/017	SOC/DART/IL/GRN/017B	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/017	SOC/DART/IL/GRN/017C	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DAK1 (L.) KN (J.)	По анестоя маяти dime			D L di L	i unt	Not interviewed	
VAL/DART/IL/GRN/018	SOC/DART/IL/GRN/018	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/020	SOC/DART/IL/GRN/020	Female		Other retail or services	Property Owner	Kiosk attached to building	
VAL/DART/IL/GRN/022	SOC/DART/IL/GRN/022	Female		Other retail or services	Property Owner	Stand-Alone Kiosk	
VAL/DART/IL/GRN/025	SOC/DART/IL/GRN/025	Female		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/027	SOC/DART/IL/GRN/027	Male		Other	Property Owner	Stand-Alone Kiosk	
VAL/DART/IL/GRN/031	SOC/DART/IL/GRN/031	Female		Other retail or services	Other	Kiosk attached to building	
VAL/DART/IL/GRN/031	SOC/DART/IL/GRN/031A	Female		Other retail or services	Other	Kiosk attached to building	
VAL/DART/IL/GRN/034	SOC/DART/IL/GRN/034	Female		Other retail or services	Property Owner	Kiosk attached to building	
VAL/DART/IL/GRN/036	SOC/DART/IL/GRN/036	Female		Other retail or services	Property Owner	Kiosk attached to building	
VAL/DART/IL/GRN/038	SOC/DART/IL/GRN/038	Female		Restaurant/ bar/ food&leisure/ hotel	Property Owner	Room in residential building	
VAL/DART/IL/GRN/047	SOC/DART/IL/GRN/047	Female		Vehicle related services	Property Owner	Independent Commercial Building	
VAL/DART/IL/GRN/063	SOC/DART/IL/GRN/063	Female		Other retail or services	Property Owner	Stand-Alone Kiosk	
VAL/DART/IL/GRN/068	SOC/DART/IL/GRN/068	Maic		Other retail or services	Property Owner	Kiosk attached to building	
VAL/DART/IL/GRN/069	SOC/DART/IL/GRN/069	Female		Other retail or services	Property Owner	Kiosk attached to building	
VAL/DART/IL/GRN/074	SOC/DART/IL/GRN/074	Female		Other retail or services	Property Owner	Kiosk attached to building	
VAL/DART/IL/GRN/075	SOC/DART/IL/GRN/075	Male		Other retail or services	Property Owner	Kiosk attached to building	
VAL/DART/IL/GRN/077	SOC/DART/IL/GRN/077	Female		Other retail or services	Property Owner	Krosk attached to building	
VAL/DART/IL/GRN/078	SOC/DART/IL/GRN/078	Male	,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,	Restaurant/ bar/ food&leisure/ hotel	Property Owner	Kiosk attached to building	
VAL/DART/IL/GRN/081	SOC/DART/IL/GRN/081	Male		Other retail or services	Property Owner	Kiosk attached to building	
VAL/DART/IL/GRN/085	SOC/DART/IL/GRN/085	Female		Other retail or services	Property Owner	Stand-Alone Kiosk	

ANNEX 3.3 - RAP REPORT Commercial Ressetternt ____

Annex 03 - Part 3
Data Bank related to Sections 01 and 02 of the Socio-economic Survey - Commercial Activities Questionnarie

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Code of Corresponding Property Survey	Code of this Secon Economic Unit	Family Head	Other:	Commercial Activity Type	Type of occupancy	Property Type	Other
VAL/DART/IL/GRN/086	SOC/DART/IL/GRN/086	Male		Other retail or services	Property Owner	Room in residential building	
VAL/DART/IL/GRN/093	SOC/DART/IL/GRN/093	Female		Other retail or services	Property Owner	Stand-Alone Kiosk	
VAL/DART/IL/GRN/097	SOC/DART/IL/GRN/097	Female		Other retail or services	Other	Kiosk attached to building	
VAL/DART/IL/GRN/098	SOC/DART/IL/GRN/098	Male		Other retail or services	Property Owner	Stand-Alone Krosk	
VAL/DART/IL/GRN/100	SOC/DART/IL/GRN/100	Male		Other retail or services	Property Owner	Kiosk attached to building	
VAL/DART/IL/GRN/103	SOC/DART/IL/GRN/103	Malc		Other retail or services	Property Owner	Kiosk attached to building	
VAL/DART/KN/HNF/001	The interview was not done			Restaurant/ bar/ food&lessure/ hotel	Property Owner	Not interviewed	
VAL/DART/KN/HNF/002	The interview with our date			Other	Property Owner	Not interviewed	
VAL/DART/KN/HNF/003	SOC/DART/KN/HNF/003				Property Owner	No answer	
VAL/DART/KN/HNF/004	The interview was not devic)		Property Owner	Not interviewed	
VAL/DART/KN/HNF/006	The interview was not done			Other retail or services	Tenant	Not interviewed	
VAL/DART/KN/HNF/006	The materials was not done			Other retail or services	Tenant	Not interviewed	
VAL/DART/KN/HNF/007	SOC/DART/KN/HNF/007	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/KN/HNF/007	SOC/DART/KN/HNF/007A	Female		Industrial	Tenant	Stand-Alone Kiosk	
VAL/DART/KN/HNF/007	The interview toge tow dance			Residential	Tenant	Not interviewed	
VAL/DART/KN/HNF/007	SOC/DART/KN/HNF/007B	Male		Restaurant/ bar/ food&lessurc/ hotel	Tenant	Stand-Alone Kiosk	
VAL/DART/KN/HNF/008	SOC/DART/KN/HNF/008	Female		Restaurant/ bar/ food&lessure/ hotel	Property Owner	Kiosk attached to building	
VAL/DART/KN/HNF/009	SOC/DART/KN/HNF/009	Male		Restaurant/ bar/ food&leisure/ hotel	Property Owner	Kiosk attached to building	
VAL/DART/KN/HNF/010	SOC/DART/KN/HNF/010A	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/KN/HNF/010	SOC/DART/KN/HNF/010B	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/KN/HNF/010	SOC/DART/KN/HNF/010C	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/KN/HNF/010	SOC/DART/KN/HNF/010D	Female		Other retail or services	Tenant	Krosk attached to building	
VAL/DART/KN/HNF/011	SOC/DART/KN/HNF/011	Female		Restaurant/ bar/ food&leisure/ hotel	Tenant	Kiosk attached to building	
VAL/DART/KN/HNF/011	SOC/DART/KN/HNF/011A	Female		Other	Tenant	Kiosk attached to building	

ANNEX 3.3 - RAP REPORT Commercial Ressetlemnt -----

Annex 03 - Part 3 Data Bank related to Sections 01 and 02 of the Socio-economic Survey - Commercial Activities Questionnarie

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Code of Corresponding Property Survey	Code of this Socio-E conomic Unit	Family Head	Other	Commercial Activity Type	Турс об оссорався	Property Type	Other.
VAL/DART/KN/HNF/011	SOC/DART/KN/HNF/011B	Male		Other	Tenant	Kiosk attached to building	
VAL/DART/KN/HNF/011	SOC/DART/KN/HNF/011C	Male		Other	Tenant	Kiosk attached to building	
VAL/DART/KN/HNF/011	SOC/DART/KN/HNF/011D	Male		Other	Tenant	Stand-Alone Kiosk	
VAL/DART/KN/HNF/011	SOC/DART/KN/HNF/011E	Male		Other	Tenant	Kiosk attached to building	
VAL/DART/KN/HNF/011	SOC/DART/KN/HNF/011F	Male		Other	Tenant	Kiosk attached to building	
VAL/DART/KN/HNF/012	SOC/DART/KN/HNF/012	Male		Other retail or services	Tenant	Room in residential building	
VAL/DART/KN/HNF/012	SOC/DART/KN/HNF/012A	Fentale		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/KN/KMR/003	SOC/DART/KN/KMR/003	Male		Public/ Institutional buildings	Tenant	No answer	
VAL/DART/KN/KMR/004	SOC/DART/KN/KMR/004	Male		Vehicle related services	Property Owner	No answer	
VAL/DART/KN/KMR/004	SOC/DART/KN/KMR/004A	Male		Vehicle related services	Tenant	No answer	
VAL/DART/KN/KMR/005	SOC/DART/KN/KMR/005	Male		Industrial	Tenant	No answer	
VAL/DART/KN/KMR/005	SOC/DART/KN/KMR/005A	Male		Other retail or services	Tenant	No answer	
VAL/DAKT KN KNR or	The interview was not done			····· · · · · · · · · · · · · · · · ·	Tenant	. Not interviewed	
VAL/DART KN KMR 1815	The interview was not done			····· · · · · ·	Tenant	Not interviewed	
VAL/DART/KN/KMR/005	SOC/DART/KN/KMR/005B	Female		Restaurant/ bar/ food&leisure/ hotel	Tenant	No answer	
VAL/DART/KN/KMR/006	SOC/DART/KN/KMR/006A	Female		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/006	SOC/DART/KN/KMR/006B	Female		Restaurant/ bar/ food&leisure/ hotel	Tenant	No answer	
VAL/DART/KN/KMR/007	SOC/DART/KN/KMR/007A	Male		Other retail or services	Property Owner	No answer	
VAL/DART/KN/KMR/007	SOC/DART/KN/KMR/007C	Female		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/007	SOC/DART/KN/KMR/007D	Female		Restaurant/ bar/ food&leisure/ hotel	Tenant	No answer	
VAL/DART/KN/KMR/008	SOC/DART/KN/KMR/008	Male		Vehicle related services	Tenant	No answer	
VAL/DART/KN/KMR/008	SOC/DART/KN/KMR/008A	Male		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/009	SOC/DART/KN/KMR/009A	Male		Other	Tenant	No answer	
VAL/DART/KN/KMR/009	SOC/DART/KN/KMR/009B	Female		Other	Tenant	No answer	

ANNEX 3 3 - RAP REPORT Commercial Ressetlemnt ----

Annex 03 - Part 3 Data Bank related to Sections 01 and 02 of the Socio-economic Survey - Commercial Activities Questionnarie

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Code of Corresponding Property Survey	Cude of this Socio-Economic Unit	Family Head	Other	Commercial Activity Type:	Type of occupancy	Property Type	Other:
VAL/DART/KN/KMR/009	SOC/DART/KN/KMR/009C	Male		Other	Tenant	No answer	
VAL/DART/KN/KMR/009	SOC/DART/KN/KMR/009D	Female		Restaurant/ bar/ food&leisure/ hotel	Tenant	No answer	
VAL/DART/KN/KMR/009	SOC/DART/KN/KMR/009E	Female		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/009	SOC/DART/KN/KMR/009F	Female		Restaurant/ bar/ food&leisure/ hotel	Tenant	No answer	
VAL/DART/KN/KMR/009	SOC/DART/KN/KMR/009G	Male		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/011	SOC/DART/KN/KMR/011A	Female		Restaurant/ bar/ food&lessure/ hotel	Tenant	No answer	
VAL/DART/KN/KMR/012	SOC/DART/KN/KMR/012	Female		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/012	SOC/DART/KN/KMR/012A	Male		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/012	The anterview was next dom.			······································	1	"Liss i lewed	
VAL/DART/KN/KMR/012	The affertive of the first dataset			- 14 - 14 -	i 5	* + + + icwed	
VAL/DART/KN/KMR/012	SOC/DART/KN/KMR/012B	Female		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/013	SOC/DART/KN/KMR/013	Male		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/013	SOC/DART/KN/KMR/013A	Male		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/013	SOC/DART/KN/KMR/013B	Male		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/013	SOC/DART/KN/KMR/013C	Male		Vehicle related services	Tenant	No answer	
VAL/DART/KN/KMR/013	SOC/DART/KN/KMR/013D	Female		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/014	SOC/DART/KN/KMR/014	Male		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/014	SOC/DART/KN/KMR/014A	Male		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/014	SOC/DART/KN/KMR/014B	Male		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/014	SOC/DART/KN/KMR/014C	Male		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/014	SOC/DART/KN/KMR/014D	Male		Restaurant/ bar/ food&leisure/ hotel	Tenant	No answer	
VAL/DART/KN/KMR/014	SOC/DART/KN/KMR/014E	Female		Other	Property Owner	No answer	
VAL/DART/KN/KMR/015	SOC/DART/KN/KMR/015	Male		Restaurant/ bar/ food&leisure/ hotel	Property Owner	No answer	
VAL/DART/KN/KMR/015	SOC/DART/KN/KMR/015A	Male		Other retail or services	Tenant	No answer	

ANNEX 3 3 - RAP REPORT Commercial Ressettemnt ----

Annex 03 - Part 3 Data Bank related to Sections 01 and 02 of the Socio-economic Survey - Commercial Activities Questionnarie

Code of Corresponding Property Survey	Code of this Sector Economic Unit	Family Head	Other	Commercial Activity Type	Type of occupancy	Property 1xpe	Other.
VAL/DART/KN/KMR/017	SOC/DART/KN/KMR/017	Male		Industrial	Property Owner	No answer	
VAL/DART/KN/KMR/017	SOC/DART/KN/KMR/017B	Male		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/017	SOC/DART/KN/KMR/017C	Male		Restaurant/ bar/ food&leisure/ hotel	- Tenant	No answer	
VAL/DART/KN/KMR/020	SOC/DART/KN/KMR/020	Male		Restaurant/ bar/ food&lessure/ hotel	Tenant	No answer	
VAL/DART/KN/KMR/020	SOC/DART/KN/KMR/020A	Male		Restaurant/ bar/ food&leisure/ hotel	Tenant	No answer	
VAL/DART/KN/UB2/021	The interview was not done			1965 1915 25	Τ,ι	Not interviewed	
VAL/DART/KN/KMR/023	SOC/DART/KN/KMR/023	Female		Other	Property Owner	No answer	
VAL/DART/KN/KMR/025	SOC/DART/KN/KMR/025B	Female		Other retail or services	Property Owner	No answer	
VAL/DART/KN/KMR/026	SOC/DART/KN/KMR/026	Male		Restaurant/ bar/ food&leisure/ hotel	Tenant	No answer	
VAL/ÐART/KN/KMR/026	SOC/DART/KN/KMR/026A	Male		Vehicle related services	Tenant	No answer	
VAL/DART/KN/KMR/026	SOC/DART/KN/KMR/026B	Male		Other retail or services	Property Owner	No answer	
VAL/DART/KN/KMR/027	SOC/DART/KN/KMR/027	Male		Other retail or services	Property Owner	No answer	
VAL/DART/KN/KMR/028	SOC/DART/KN/KMR/028	Male		Other	Property Owner	No answer	
VAL/DART/KN/KMR/029	SOC/DART/KN/KMR/029	Female		Other retail or services	Property Owner	No answer	
VAL/DART/KN/KMR/029	SOC/DART/KN/KMR/029A	Female		Restaurant/ bar/ food&lessure/ hotel	Tenant	No answer	
VAL/DART/KN/KMR/029	SOC/DART/KN/KMR/029B	Female		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/031	SOC/DART/KN/KMR/031	Female		Restaurant/ bar/ food&leisure/ hotel	Property Owner	No answer	
VAL/ JANT KN KMR (03)	The interview was not done			f jund	6.1.9.9.10	Not interviewed	
VAL/DANT KN KNR 193	The otherway was not done			F 111	1	Not interviewed	
VAL/DART/KN/KND/002	SOC/DART/KN/KND/002A	Female		Other retail or services	Property Owner	Kiosk attached to building	
VAL/DART/KN/KND/004	SOC/DART/KN/KND/004			Other	Property Owner	No answer	
VAL/DART/KN/MG2/001	SOC/DART/KN/MG2/001	Female		Industrial	Tenant	No answer	
VAL/DART/KN/MG2/002	SOC/DART/KN/MG2/002	Maie		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/KN/MG2/002	SOC/DART/KN/MG2/002A	Male			Tenant	No answer	

ANNEX 3.3 - RAP REPORT Commercial Ressetternnt _

Annex 03 - Part 3 Data Bank related to Sections 01 and 02 of the Socio-economic Survey - Commercial Activities Questionnarie

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Code of Corresponding Property Survey	Code of this Socio-Economic Unit	Family Head	Other	Commercial Activity Type.	Туре об оссырался	Property Type	Uner:
VAL/DART/KN/MG2/002	SOC/DART/KN/MG2/002B	Male			Tenant	No answer	
VAL/DART/KN/MG2/002	SOC/DART/KN/MG2/002C	Male			Tenant	No answer	
VAL/DART/KN/MG2/002	SOC/DART/KN/MG2/002D	Female		Restaurant/ bar/ food&lessure/ hotel	Tenant	Stand-Alone Kiosk	
VAL/DART/KN/MG2/002	Du interview is as not dune			I trul	Tenant	Not interviewed	
VAL/DART/KN/MG2/002	The interview was not deno			E aj aŭ l	Tenant	Not interviewed	
VAL/DART/KN/MG2/002	SOC/DART/KN/MG2/002E	Malc			Tenant	No answer	
VAL/DART/KN/MG2/003	SOC/DART/KN/MG2/003	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/KN/MG2/003	SOC/DART/KN/MG2/003A	Female		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/KN/MG2/003	SOC/DART/KN/MG2/003B	Female		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/KN/MG2/003	SOC/DART/KN/MG2/003C	Male		Restaurant/ bar/ food&leisure/ hotel	Tenant	Stand-Alone Kiosk	
VAL/DART/KN/MG2/004	SOC/DART/KN/MG2/004A	Male		Restaurant/ bar/ food&lcisure/ hotel	Tenant	Stand-Alone Kiosk	
VAL/DART/KN/MG2/005	SOC/DART/KN/MG2/005	Male		Other retail or services	lenant	Kiosk attached to building	
VAL/DART/KN/MG2/005	SOC/DART/KN/MG2/005A	Female	, , , , , , , , , , , , , , , , , , , 	Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/KN/MG2/005	SOC/DART/KN/MG2/005C	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DAKT KN MIL, : INF	The interview was not done			100 1 1 1 1	1	ka itala	
VAL/DART/KN/MG2/005	SOC/DART/KN/MG2/005D	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/KN/MG2/006	SOC/DART/KN/MG2/006A	Male			Property Owner	No answer	
VAL/DART/KN/MNY/001	SOC/DART/KN/MNY/001	Male		Petrol stations	Property Owner	No answer	
VAL/DART/KN UB: INI	The otherway was not done		······	int i truices	Tenant	Not interviewed	
VAL/DART/KN/UB2/001	SOC/DART/KN/UB2/001	Male		Other retail or services	Tenant	No answer	
VAL PART KNIED IN	The interview was not done			+ 1 n l	Property Owner	Not interviewed	
VAL DART KN I B. mo	The internation was not done.			1 1 1		Not interviewed	
VAL/DART/KN/UB2/003	SOC/DART/KN/UB2/003	Female		Other retail or services	Tenant	No answer	
VAL/DART/KN/UB2/003	SOC/DART/KN/UB2/003A	Female		Other retail or services	Property Owner	No answer	

ANNEX 3.3 - RAP REPORT Commercial Ressetternnt ----

Code of Corresponding Property Survey	Cade al this Socia-Economic Unit	Family Head	Other:	Commercial Activity Type	Туре об оссирация	Property Type	Other.
VAL/DART/KN/UB2/004	SOC/DART/KN/UB2/004	Male		Other retail or services	Tenant	No answer	
VAL/DART/KN/UB2/005	SOC/DART/KN/UB2/005	Female		Other retail or services	Property Owner	No answer	
VAL/DART/KN/UB2/006	SOC/DART/KN/UB2/006	Male		Other retail or services	Tenant	No answer	
VAL/DART/KN/UB2/007	SOC/DART/KN/UB2/007B	Female		Other retail or services	Property Owner	No answer	
VAL/DART/KN/UB2/008	SOC/DART/KN/UB2/008A	Male		Public/ Institutional buildings	Property Owner	No answer	
VAL/DART/KN/UB2/009	SOC/DART/KN/UB2/009	Female		Public/ Institutional buildings	Property Owner	No answer	
VAL/DART/KN/UB2/010	SOC/DART/KN/UB2/010	Female		Other retail or services	Property Owner	No answer	
VAL/DART/KN/UB2/011	SOC/DART/KN/UB2/011	Male		Other retail or services	Property Owner	No answer	
VAL/DART/KN/UB2/012	SOC/DART/KN/UB2/012A	Male		Restaurant/ bar/ food&lessure/ hotel	Property Owner	No answer	
VAL/DART/KN/UB2/013	SOC/DART/KN/UB2/013	Male		Other retail or services	Property Owner	No answer	
VAL/DART/KN, PE, 1914	The effective strate not done			Other retail or services	Tenant	Not interviewed	
VAL/DART/KN/UB2/014	SOC/DART/KN/UB2/014	Male		Other retail or services	Property Owner	No answer	
VAL/DART KN + B. HI	The interview was not a mi			Other retail or services	Property Owner	Not interviewed	
VAL/DART/KN/UB2/016	SOC/DART/KN/UB2/016	Male		Other retail or services	Property Owner	No answer	
VAL/DART/KN/UB2/017	SOC/DART/KN/UB2/017	Female		Other retail or services	Property Owner	No answer	
VAL/DART/KN/UB2/021	SOC/DART/KN/UB2/021	Male		Other	Property Owner	Stand-Alone Kiosk	
VAL/DART IL IGN 001	The tractorers was not door.			Petrol stations	Property Owner	Not interviewed	
VAL/DARTIL / North	Піе шеглем в 18 пля фоле			Other retail or services	Property Owner	Not interviewed	
VAL/"INRTINN MINI MI	The INCOME A REPORT OF THE			Residential	Property Owner	Not interviewed	
VAL/DAKT II KKUUP.	The otterview was not done		<u> </u>	Residential	Tenant	Not interviewed	

Data Bank related to Sections 01 and 02 of the Socio-economic Survey - Commercial Activities Questionnarie

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Annex 03 - Part 3

ANNEX 3.4 - RAP REPORT Commercial Ressetlemnt - Affected Employees -

Annex 03 - Part 4 Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnarie

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Code of this Socio-Economic Unit	N° of Employee	Relation to Owner	Lives within 300m of corridor?	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
SOC/DART/IL/GRN/001	Owner	not applicable		not applicable	Full Time	1	daladala
SOC/DART/IL/GRN/001	Manager	not applicable		not applicable	Full Time	1	daladala
SOC/DART/IL/GRN/001	Other	no relation		Other	Full Time	l	daladala
SOC/DART/IL/GRN/002	Owner	not applicable		not applicable	Full Time	0	daladala
SOC/DART/IL/GRN/002A	Owner	not applicable		not applicable	Full Time	0	daladala
SOC/DART/IL/GRN/002A	Manager	not applicable		not applicable	Full Time	0	daladala
SOC/DART/IL/GRN/002A	Other	no relation		Other	Full Time	ų.	daladala
SOC/DART/IL/GRN/002A	Other	no relation		Other	Full Time	0	daladala
SOC/DART/IL/GRN/003	Owner	not applicable		not applicable	Full Time	0	daladala
SOC/DART/IL/GRN/003	Manager	no relation		Registered employ ee	Full Time	0	daladala
SOC/DART/IL/GRN/003	Other	no relation		Registered employee	Full Time	0	daladala
SOC/DART/IL/GRN/003	Other	no relation		Registered employ ee	Full Time	0	daladala
SOC/DART/IL/GRN/003A	Owner	not applicable		not applicable	Full Time	0	daladala
SOC/DART/IL/GRN/003A	Other	other relative		Registered employ ee	Full Time	0	daladala
SOC/DART/IL/GRN/003A	Other	no relation		Registered employee	Full Time	0	daladala
SOC/DART/IL/GRN/003A	Other	other relative		Registered employee	Full Time	0	daladala
SOC/DART/IL/GRN/003A	Other	other relative		Registered employee	Full Time	0	daladala
SOC/DART/IL/GRN/003B	Owner	not applicable		not applicable	Full Time	0	daladala
SOC/DART/IL/GRN/003B	Manager	other relative		Registered employee	Full Time	U	daladala
SOC/DART/IL/GRN/003C	Owner	not applicable		not applicable	Full Time	0	datadala
SOC/DART/IL/GRN/004	Owner	not applicable		not applicable	Full Time	2	daladala
SOC/DART/IL/GRN/004A	Owner	not applicable		not applicable	Full Time	0	daladala
SOC/DART/IL/GRN/005	Омпея	not applicable		not applicable	Full Time	1	daladala

ANNEX 3.4 - RAP REPORT Commercial Ressetlemnt - Affected Employees ----

Annex 03 - Part 4 Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnarie

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Code of this Nocio-Economic Unit	N° of Employee	Relation to Owner	Lives within WOM of corndor?	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
SOC/DART/IL/GRN/005	Manager	other relative		Informal	Full Time	1	daladala
SOC/DART/IL/GRN/005A	Owner	not applicable		not applicable	Full Time	0	daladala
SOC/DART/IL/GRN/005A	Manager	no relation		Informal	Full Time	0	daladala
SOC/DART/IL/GRN/00%	Owner	not applicable			1 at 1 a.		ne
SOC/DART/IL/GRN/006	Other	no relation		Informal	Full Time	4	daladala
SOC/DART/IL/GRN/006	Other	no relation		Informal	Full Time	I	daladala
SOC/DART/IL/GRN/006A	Other	other relative		Informal	Full Time	2	daladala
SOC/DART/IL/GRN/006A	Other	other relative		Informal	Full Time	2	daladala
SOC/DART/IL/GRN/006A	Owner	not applicable		not applicable	Full Time	2	daladala
SOC/DART/IL/GRN/006A	Manager	other relative		Informal	Full Time	2	car-motorbike
SOC/DART/IL/GRN/006A	Other	other relative		Informal	Full Time	2	daladala
SOC/DART/IL/GRN/006B	Owner	not applicable		not applicable	Full Time	0	daladala
SOC/DART/IL/GRN/006B	Manager	other relative		Informal	Full Time	0	daladala
SOC/DART/IL/GRN/006C	Owner	not applicable		not applicable	Full Time	8	daladala
SOC/DART/IL/GRN/006C	Manager	no relation		Informal	Full Time	8	daladala
SOC/DART/IL/GRN/(#)8A	Owner	not applicable		not applicable	Full Time	3	daladala
SOC/DART/IL/GRN/008B	Owner	not applicable		not applicable	Full Time	5	daladala
SOC/DART/IL/GRN/008B	Other	other relative		Informal	Full Time	5	daladala
SOC/DART/IL/GRN/008B	Other	no relation		Informal	Full Time	5	daladala
SOC/DART/IL/GRN/008B	Other	no relation		Informal	Full Time	5	daladala
SOC/DART/IL/GRN/008C	Owner	not applicable		not applicable	Full Time	1	daladala
SOC/DART/IL/GRN/008C	Other	other relative		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/009	Owner	not apple at the		not applicable	Full Time	2	car-motorbike

ANNEX 3.4 - RAP REPORT Commercial Ressettemnt - Affected Employees -

Annex 03 - Part 4 Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnarie

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Code of this Socio-Economic Unit	N° of Employee	Relation to Owner	Lives within 370m of corridor*	Type of Work Contract	Working Period	How long does the perion have this occupation?	Means of Train/port to Work
SOC/DART/IL/GRN/009	Manager	other relative		Registered employee	Full Time	2	daladala
SOC/DART/IL/GRN/009	Other	no relation		Registered employee	Full Time	l	daladala
SOC/DART/IL/GRN/009	Other	no relation		Registered employee	Full Time	l	daladala
SOC/DART/IL/GRN/009	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/IL/GRN/009B	Owner	not applicable		not applicable	Full Time	1	daladala
SOC/DART/IL/GRN/009B	Manager	no relation		Registered employ ee	Full Time	1	datadala
SOC/DART/IL/GRN/009C	Owner	not applicable		not applicable	Full Time	1	daladata
SOC/DART/IL/GRN/009C	Manager	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/009C	Other	no relation		Registered employ ee	Full Time	1	daladala
SOC/DART/IL/GRN/009E	Owner	not applicable		not applicable	Full Time	3	car-motorbike
SOC/DART/IL/GRN/009E	الم ي ال	ils a z ra		É a tred a pl	F.JUT.com		walking
SOC/DART/IL/GRN/009E		digenship.		1	₹01 T		walking
SOC/DART/IL/GRN/009E	· ut	r Lite is		ti ti miji	Full for t	1	walking
SOC/DART/IL/GRN/010		0 C (1) C (1)		с тары, П	ent tun	11	none
SOC/DART/IL/GRN/010	Other	no relation		Registered employee	Full Time	6	daladala
SOC/DART/IL/GRN/010	Other	no relation		Registered employ ee	Full Time	3	daladala
SOC/DART/IL/GRN/010	Other	no relation		Registered employee	Full Time	t	daladala
SOC/DART/IL/GRN/010	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/IL/GRN/010	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/010	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/010	Other	no relation		Registered employee	Full Time	11	daladala
SOC/DART/IL/GRN/011	Owner	not applicable		not applicable	Full Time	0	daladala
SOC/DART/IL/GRN/011	Other	other relative		Informal	Full Time	Û	daladala

ANNEX 3 4 - RAP REPORT Commercial Ressetlemnt - Affected Employees =

Annex 03 - Part 4 Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnarie

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, Code of this Socio-Economic Linit	N° of Employ ee	Relation to Owner	Lives within 300m of corridor?	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
SOC/DART/IL/GRN/011A	Owner	not applicable		not applicable	Full Time	0	daladala
SOC/DART/IL/GRN/011A	Other	no relation		Other	Full Time	0	daladala
SOC/DART/IL/GRN/011A	Other	no relation		Other	Full Time	0	daladala
SOC/DART/IL/GRN/011A	Other	no relation		Other	Full Time	0	daladala
SOC/DART/IL/GRN/011B	Owner	not applicable		not applicable	Full Time	1	daladala
SOC/DART/IL/GRN/011B	Other	no relation		Informal	Full Time	1	daladala
SOC/DART/IL/GRN/012	Owner	not applicable		not applicable	Full Time	1	daladala
SOC/DART/IL/GRN/012	Manager	other relative		Other	Full Time	1	daladala
SOC/DART/IL/GRN/012A	Owner	not applicable		not applicable	Full Time	I	car-motorbike
SOC/DART/IL/GRN/012A	Other	no relation		Registered employee	Full Time	1	car-motorbike
SOC/DART/IL/GRN/012B	Owner	not applicable		not applicable	Partial Time	0	daladala
SOC/DART/IL/GRN/012B	Manager	no relation		Other	Full Time	0	daladala
SOC/DART/IL/GRN/013	Owner	not applicable		not applicable	Full Time	2	daladala
SOC/DART/IL/GRN/013	Manager	other relative		Other	Partial Time	2	daladala
SOC/DART/IL/GRN/013A	Owner	not applicable		not applicable	Full Time	2	daladala
SOC/DART/IL/GRN/013A	Manager	other relative		Other	Full Time	2	daladala
SOC/DART/IL/GRN/013B	Owner	not applicable		not applicable	Full Time	1	daladala
SOC/DART/IL/GRN/013B	Other	no relation		Registered employ ee	Full Time	1	daladala
SOC/DART/IL/GRN/013C	Owner	not applicable		not applicable	Full Time		daladala
SOC/I······	Q. as			or e aj plo, al le	FUT	1	daladala
SOC/I····	the new	theory for		E to fingt a	Lott	I	daladala
SOC/I ''''''''''''''''''''''''''''''''''''	11.0	an é a		E. Britand	Full Im	0	daladala
SOC/DART/IL/GRN/016A	Owner	not applicable		not applicable	Partial Time	4	daladala

Legend Yes No Not Informed

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Annex 03 - Part 4

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Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnarie

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Code of this Socio-Economic Limit	N° of Employee	Relation 10 Owner	Lives within 36m of corridor?	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
SOC/DART/IL/GRN/016A	Manager	not applicable			Partial Time	4	daladala
SOC/DART/IL/GRN/016A	Other	son/daughter		Informal	Full Time	4	daladala
SOC/DART/IL/GRN/016B	Owner	not applicable		· · · · · · · · · · · · · · · · · · ·	Full Time	1	daladala
SOC/DART/IL/GRN/016B	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/016B	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/016B	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/016B	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/016C	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/IL/GRN/016C	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/IL/GRN/016C	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/IL/GRN/016C	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/016C	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/IL/GRN/016C	Other	no relation		Registered employee	Full Time	5	daladala
SOC/DART/IL/GRN/016C	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/016C	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/IL/GRN/016C	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/016C	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/016C	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/016C	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/016C	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/017	Owner	not applicable		not applicable	Full Time	2	daladala
SOC/DART/IL/GRN/017	Manager	other relative		Informal	Fult Time	2	daladala
SOC/DART/IL/GRN/017A	Owner	not applicable		not applicable	Full Time	1	daladaia

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Annex 03 - Part 4 Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnarie

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Code of this Socio-Economic L'mit	N' of Employee	Relation to Owner	Lives within 500m of corridor*	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
SOC/DART/IL/GRN/017A	Other	agreggates		Informal	Full Time	1	daladala
SOC/DART/IL/GRN/017B	Owner	not applicable		not applicable	Fuli Time	0	daladala
SOC/DART/IL/GRN/017C	Owner	not applicable		not applicable	Full Time	2	daladala
SOC/DART/IL/GRN/017C	Manager	other relative		Registered employ ee	Full Time	2	daladala
SOC/DART/IL/GRN/018	Owner	not applicable		not applicable	Full Time	1	daladala
SOC/DART/IL/GRN/020	1.1. Tere	مال طريرا ت		n nggh 10	Partial Time	0	none
SOC/DART/IL/GRN/020	Other	no relation		Not informed	Full Time	0	daladala
SOC/DART/IL/GRN/022		يالار المرادين		الد اين ب	Full Time	3	car-motorbike
SOC/DART/IL/GRN/022	entr _e ,			Elizable d'arte el	Full Time	3	none
SOC/DART/IL/GRN/022	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/IL/GRN/025	41	الاستاني بالتر	-		Full Time	12	none
SOC/DART/IL/GRN/025	Other	no relation		Registered employee	Full Time	10	daladala
SOC/DART/IL/GRN/025	Other	no relation		Registered employee	Full Time	4	daladala
SOC/DART/IL/GRN/025	Other	no relation		Registered employee	Full Time	5	daladala
SOC/DART/IL/GRN/027	Ú teo	الانهاريد الا		فان غرو الم	Partial Time	8	none
SOC/DART/IL/GRN/027	Other	no relation		Registered employee	Full Time	8	daladala
SOC/DART/IL/GRN/027	Other	no relation		Registered employee	Full Time	8	daladala
SOC/DART/IL/GRN/027	Other	no relation		Registered employee	Full Time	6	daladala
SOC/DART/IL/GRN/031		e najte ni		0 (1 1).	t migt time	7	daladala
SOC/DART/IL/GRN/031	Other	no relation		Registered employee	Full Time	7	daladala
SOC/DART/IL/GRN/031	Other	no relation		Registered employee	Full Time	7	daladala
SOC/DART/IL/GRN/031A		a 1 juli 10		not applicable	Partial Time	7	walking
SOC/DART/IL/GRN/031A	Other	no relation		Registered employee	Full Time	7	daladala

Annex 03 - Part 4 Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnarie

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Code of this Socio-Economic Unit	N ⁺ of Employee	Relation to Owner	Lives within Ström of corridor"	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Tran-port to Work
SOC/DART/IL/GRN/031A	Other	no relation		Registered employee	Full Time	7	daladala
SOC/DART/IL/GRN/034	Owner	الد بارو د د		hicable ، با ب	Partial Time	8	none
SOC/DART/IL/GRN/034	Manager	at i tai		· · · rmal	Full Time	8	none
SOC/DART/IL/GRN/034	Other	no relation		Informal	Full Time	3	daladala
SOC/DART/IL/GRN/034	Other	no relation		Informal	Full Time	3	daladala
SOC/DART/IL/GRN/034	Other	no relation		Informal	Full Time	3	daladala
SOC/DART/IL/GRN/034	Other	no relation		Informal	Full Time	3	daladala
SOC/DART/IL/GRN/034	Other	no relation		Informal	Full Time	3	daladala
SOC/DART/IL/GRN/034	Other	no relation		Informal	Full Time	3	daladala
SOC/DART/IL/GRN/036	Owner	not applicable		Δ.1. ₀₁₁ 6. Μ.	Full Time	10	none
SOC/DART/IL/GRN/036	Other	no relation		Informal	Full Time	10	daladala
SOC/DART/IL/GRN/036	Other	no relation		Informal	Full Time	10	daladala
SOC/DART/IL/GRN/036	Other	no relation		Informal	Full Time	10	daladala
SOC/DART/IL/GRN/038	0 7 s	تلفيل ويتبع		بالاستان ورايين	Full Time	5	none
SOC/DART/IL/GRN/038	1	6 J. 12 bris			Full Time	5	daladala
SOC/DART/IL/GRN/047	Owner	not applicable		not applicable	Full Time	3	none
SOC/DART/IL/GRN/047	'1 _{'''+1}				Full Time	3	none
SOC/DART/IL/GRN/047	e qL 50	0 - 1 10 - 1		P i i i≂mi ee	Full Time	3	лопе
SOC/DART/IL/GRN/047	ing the second sec	n 1 10		F _ 1 1 1 1 1 1 30	Full Time	3	none
SOC/DART/IL/GRN/047		the could		Feliti 1 mil 20	Full Time	3	none
SOC/DART/IL/GRN/047	1.11.	r selate a		i juliant mol se	Full Time	3	none
SOC/DART/IL/GRN/063				ներին էն	Full Time	17	none
SOC/DART/IL/GRN/063	Cales.	ilse lo		Fight Joy Fige	Full Time	0	none

Annex 03 - Part 4 Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnarie

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Code of this Socio-Economic Unit	N" of Employre	Relation to Owner	Lives within 340m of corridor?	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
SOC/IIIIIII		- 1 g (b, 1)		o 1 aj 16, 11	يتنبأ الإيلان ا		
SOC/DART/IL/GRN/068	Other	no relation		Registered employee	Full Time	2	daladala
SOC/D +++++++	11 fee	$X \in \{f(t_i, t)\}$		ու գլե մե	Partial Time	5	none
SOC/D \$FT II + FF II	rute a	بار اخت جنان		Jul stard	Full Time	5	none
SOC/D NET TO LET 1		- الريانيو ال		ւ դղն մե	Full Time	13	none
SOC/D # F TU + F + + +		1,0 x 0		11I	Full Time	1	none
SOC/D ++++++++++	11 h.j.	يە بىر يە		ուղլե ու	Full Time	5	none
SOC/DART/IL/GRN/075	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/IL/GRN/075	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/IL/GRN/077	Owner	بالبيطر ويل		not applicable	Full Time	1	none
SOC/DART/IL/GRN/077	Other	at contra		Informal	Full Time	1	none
SOC/DART/IL/GRN/077	Other	no relation		Informal	Full Time	1	daladala
SOC/DART/IL/GRN/077	Other	no relation		Informal	Full Time	l	daladala
SOC/DART/IL/GRN/077	Other	no relation		Informal	Fuli Time	1	daladala
SOC/DART/IL/GRN/077				1.1.464	Full Time	1	none
SOC/DART/IL/GRN/077	rat "	or the father		hat or 1	Full Time	1	none
SOC/DART/IL/GRN/078	ų 5 r			الانتان الت	Full Time	24	none
SOC/DART/IL/GRN/078	•1,	الاستارية الم			Full Time	24	none
SOC/DART/IL/GRN/078	·	they control of		Eligen of Example in	Full Time	10	none
SOC/DART/IL/GRN/078	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/078	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/078	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/078	Other	no relation		Registered employee	Full Time	1	daladala

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Annex 03 - Part 4 Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnarie

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Code of this Socio-Economic Unit	∿° of Employee	Relation to Owner	Laver within 300m of corridor*	Type of Work Contract	Working Period	How long does the person have the occupation?	Means of Transport to Work
SOC/DART/IL/GRN/078	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/081	Owner	not applicable		not applicable	Partial Time	17	daladala
SOC/DART/IL/GRN/081	Manager	not applicable		Informal	Partial Time	17	daladala
arm frøk kun dige, so	Ú	ينه بارويت	· ·	ուցլի մե	Partial Time	7	none
21 IOFFILTE 01	*1e	a selata a		1 1d	Full Time	4	none
SOC/DART/IL/GRN/085	Other	no relation		Informal	Full Time	્ય	daladala
SOC/DART/IL/GRN/086	Owt.			ուղքի 11	F.all 1	17	none
SOC/DART/IL/GRN/093	Ow1 +	اليباروا ب			Land Land	3	none
SOC/DART/IL/GRN/093	Other	agreggates		Registered employee	Full Time	3	daladala
SOC/DART/IL/GRN/093	Other	agreggates		Registered employee	Full Time	3	daladala
SOC/DART/IL/GRN/093	Other	father/mother		Registered employee	Full Time	6	daladala
SOC/DART/IL/GRN/093	Other	father/mother		Registered employee	Full Time	2	daładala
SOC/DART/IL/GRN/093	Other	father/mother		Registered employee	Full Time	3	daladala
SOC/DART/IL/GRN/093	Other	grandson/granddaughter		Registered employee	Full Time	2	daladala
in particular.	11.7.0	ال طور ب		եղլե մե	Full Time	5	none
and from the constant of the c	·····	مالا مارو د م		o 1 q16, 11	Partial Time	5	none
SOC/DART/IL/GRN/100	Owner	not applicable		not applicable	Partial Time	2	none
SOC/DART/IL/GRN/100	Other	grandson/granddaughter		Informal	Full Time	2	none
SOC/DART/IL/GRN/103		8 Capter M.			Full Time	13	none
SOC/DART/IL/GRN/103	1	or tay bit o			Full Time	13	none
SOC/DART/KN/HNF/007	Owner	not applicable		not applicable	Full Time	4	daladala
SOC/DART/KN/HNF/007	Manager	not applicable			Full Time	4	daladala
SOC/DART/KN/HNF/007	Other	no relation		Other	Full Time	3	datadala

Annex 03 - Part 4 Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnarie

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; Code of this Socio-Economic Unit	N' of Employee	Relation to Owner	Lives within 36%m of corridor?	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
SOC/DART/KN/HNF/007A	Manager	not applicable			Full Time	0	daladala
SOC/DART/KN/HNF/007A	Other	no relation		Other	Full Time	0	daladala
SOC/DART/KN/HNF/007A	Other	no relation		Other	Full Time	Q	daladala
SOC/DART/KN/HNF/007A	Other	no relation		Other	Full Time	0	daladala
SOC/DART/KN/HNF/007A	Other	no relation		Other	Full Time	0	daladala
SOC/DART/KN/HNF/007B	Owner	not applicable		not applicable	Full Time	7	daladala
SOC/DART/KN/HNF/007B	Manager	not applicable			Fuli Time	7	daladala
SOC/DART/KN/HNF/007B	Other	other relative		Other	Full Time	5	daladala
SOC/DART/KN/HNF/007B	Other	no relation		Other	Full Time	4	daladala
SOC/DART/KN/HNF/008	Owner	not applicable		not applicable	Full Time	10	daladala
SOC/DART/KN/HNF/009	Owner	not applicable		not applicable	Full Time	12	datadala
SOC/DART/KN/HNF/009	Manager	other relative		Not informed	Full Time	12	daladala
SOC/DART/KN/HNF/009	Other	no relation		Not informed		12	daladala
SOC/DART/KN/HNF/009	Other	no relation		Not informed		12	daladala
SOC/DART/KN/HNF/009	Other	no relation		Not informed		12	daladala
SOC/DART/KN/HNF/009	Other	no relation		Not informed		12	daladala
SOC/DART/KN/HNF/009	Other	no relation		Not informed		12	daladala
SOC/DART/KN/HNF/009	Other	no relation		Not informed		8	daladala
SOC/DART/KN/HNF/009	Other	no relation		Not informed		8	daladala
SOC/DART/KN/HNF/009	Other	no relation		Not informed		8	daladala
SOC/DART/KN/HNF/009	Other	no relation		Not informed		6	daladala
SOC/DART/KN/HNF/009	Other	no relation		Not informed		4	daladala
SOC/DART/KN/HNF/009	Other	no relation		Not informed		5	daladala

Annex 03 - Part 4 Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnarie

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Code of this Socio-Economic L'ini	N° of Employee	Relation to Owner	Lives within 300m of corridor?	Type of Work Contract	Working Period	Hew long does the person have this occupation?	Means of Transport to Work
SOC/DART/KN/HNF/009	Other	no relation		Not informed		4	daladala
SOC/DART/KN/HNF/010A	Owner	not applicable		not applicable	Full Time	17	daladala
SOC/DART/KN/HNF/010A	Other	no relation		Registered employee	Full Time	5	daladala
SOC/DART/KN/HNF/010B	Owner	not applicable		not applicable	Partial Time	16	car-motorbike
SOC/DART/KN/HNF/010B	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/KN/HNF/010B	Other	no relation		Registered employee	Full Time	12	daladala
SOC/DART/KN/HNF/010C	Owner	not applicable		not applicable	Partial Time	2	car-motorbike
SOC/DART/KN/HNF/010C	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/KN/HNF/010D	Owner	not applicable		not applicable	Full Time	2	daladala
SOC/DART/KN/HNF/010D	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/KN/HNF/010D	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/KN/HNF/010D	Other	no relation		Registered employ ee	Full Time	2	daladala
SOC/DART/KN/HNF/011		الد این ب		e i glicati	111.1	5	walking
SOC/DART/KN/HNF/011	198.	· · · · · · · · · · · · · · · · · · ·		a sector a sector a sector a sector a sector a sector a sector a sector a sector a sector a sector a sector a s	F T	4	walking
SOC/DART/KN/HNF/011				المتعادية المتعادية	€ do Torio,	4	walking
SOC/DART/KN/HNF/011	так ,	0 1-1-10 0		F and the plane.	1.111	4	watking
SOC/DART/KN/HNF/011	rul			Especie Alimpton.	Full Lo	2	walking
SOC/DART/KN/HNF/011	ent .	i relati i		Éspetie Frigt	F I	2	walking
SOC/DART/KN/HNF/011A	Owner	not applicable		not applicable	Full Time	12	car-motorbike
SOC/DART/KN/HNF/011A	Manager	no relation		Registered employee	Full Time	12	car-motorbike
SOC/DART/KN/HNF/011A	Other	no relation		Registered employee	Full Time	12	daladala
SOC/DART/KN/HNF/011A	Other	no relation		Registered employee	Full Time	12	daladala
SOC/DART/KN/HNF/011A	Other	no relation		Registered employee	Full Time	12	daladala

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Annex 03 - Part 4 Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnarie

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Code of this Socio-Economic L'init	N' of Employre	Relation 19 Owner	Lives within KAM of corridor?	Type of Work Contract	Working Period	How long does the person have this occupation"	Means of Transport to Work
SOC/DART/KN/HNF/011A	Other	no relation		Registered employ ee	Full Time	12	daladala
SOC/DART/KN/HNF/011A	Other	no relation		Registered employee	Full Time	12	daladala
SOC/DART/KN/HNF/011A	Other	no relation		Registered employee	Full Time	12	daladala
SOC/DART/KN/HNF/011A	Other	no relation		Registered employ ee	Full Time	12	car-motorbike
SOC/DART/KN/HNF/011A	Other	no relation		Registered employ ee	Full Time	12	car-motorbike
SOC/DART/KN/HNF/011A	Other	no relation		Registered employ ee	Full Time	12	daladala
SOC/DART/KN/HNF/011B	Owner	not applicable		not applicable	Partial Time	12	car-motorbike
SOC/DART/KN/HNF/011B	Other	no relation		Registered employ ee	Full Time	7	daladala
SOC/DART/KN/HNF/011B	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/KN/HNF/011C	Owner	not applicable		not applicable	Partial Time	6	car-motorbike
SOC/DART/KN/HNF/011C	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/KN/HNF/011C	Other	no relation		Registered employ ee	Full Time	2	daladala
SOC/DART/KN/HNF/011D	Owner	not applicable		not applicable	Partial Time	6	car-motorbike
SOC/DART/KN/HNF/011D	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/KN/HNF/011D	Other	no relation		Registered employee	Full Time	4	daladala
SOC/DART/KN/HNF/011D	Other	no relation		Registered employ ee	Full Time	4	daladala
SOC/DART/KN/HNF/011D	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/KN/HNF/011D	110.5	A selara		F _ tec 1 dit es	É IL É ur		daladala
SOC/DART/KN/HNF/011D	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/KN/HNF/011D				t in the second	1.01 T	2	daladala
SOC/DART/KN/HNF/011D	in.			يرم ل مدين ف	Lattine.	2	daladala
SOC/DART/KN/HNF/011E	Owner	not applicable		not applicable	Full Time	8	daladala
SOC/DART/KN/HNF/011E	Other	no relation		Registered employee	Full Time	5	daiadala

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Annex 03 - Part 4			
Data Bank related to Section 03 of the S	ocio-economic Survev - Comm	ercial Activities Questionnarie	

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Code of this Socio-Economic L'init	N° of Employee	Relation to Owner	Liver within 300m of corridor?	Type of Work Contract	₩arking Period	How long does the person have this occupation?	Means of Transport to Work
SOC/DART/KN/HNF/011F	Owner	not applicable		not applicable	Full Time	12	daladala
SOC/DART/KN/HNF/011F	Other	no relation		Informal	Full Time	3	daladala
See 1 11 1 K 111 1 111		6 . Lu		hat a cal	Full Time	1	walking
Stu 1146 î.K.t. M ^a Fut,		الار باروا ب			Full Time	2	car-motorbike
Sun (Entring)	1	ilise a light o		F (1), []	Full Time	2	daladala
Sent (c. FEK') H. F.O.(.)	4 u b .			a constrait	Full Time	2	daladala
See forming relation:				 Equipation (1) and 	Full Time	2	daladala
Statute the statute status	i ul			f generational as	Full Time	2	daladala
Stor Texts Is to the data and				E principal de la companya de la company	Full Time	2	daladala
SOC/DART/KN/HNF/012A	Owner	not applicable		not applicable	Partial Time	1	car-motorbike
SOC/DART/KN/HNF/012A	Manager	no relation		Registered employee	Full Time	l	daladala
SOC/DART/KN/KMR/003	Manager	no relation		Other	Full Time	3	none
SOC/DART/KN/KMR/003	Other	no relation		Other	Full Time	3	none
SOC/DART/KN/KMR/003	Other	no relation		Other	Full Time	3	none
SOC/DART/KN/KMR/003	Other	no relation		Other	Full Time	3	none
SOC/DART/KN/KMR/003	Other	no relation		Other	Full Time	3	none
SOC/DART/KN/KMR/003	Other	no relation		Other	Full Time	3	none
SOC/DART/KN/KMR/003	Other	no relation		Other	Full Time	3	none
SOC/DART	1.01			E print and the	Full Time	1	none
SOC/DAR++' K'IE +++		0.1 april 201.		الد ارو آ	Full Time	25	none
SOC/DART PERFORMA		de contige			Full Time	10	none
SOC/DART/KN/KMR/004	Other	son/daughter		Other	Full Time	4	daladala
SOC/DART/KN/KMR/004	Other	no relation		Other	Full Time	5	daladala

Legend Yes Nortot Informed

Annex 03 - Part 4

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Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnarie

Code of this Socie-Economic Linii	N° of Employee	Relation 10 Owner	Lives within 300m of corridor*	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
on generations	. 11	ili e Elio	· · · · · · · · · · · · · · · · · · ·		Full Time	12	none
SOC/DART/KN/KMR/004A	Owner	not applicable		not applicable	Full Time	11	car-motorbike
SOC/DART/KN/KMR/004A	Other	other relative		Other	Full Time	6	daladala
SOC/DART/KN/KMR/004A	Other	, no relation		Other	Full Time	6	daladala
SOC/DART/KN/KMR/004A	Other	no relation		Other	Full Time	6	daladala
SOC/DART/KN/KMR/004A	Other	no relation		Other	Full Time	6	walking
SOC/DART/KN/KMR/004A	Other	no relation		Other	Full Time	3	daladala
SOC/DART/KN/KMR/004A	Other	no relation		Other	Full Time	4	daladala
SOC/DART/KN/KMR/004A	Other	no relation		Other	Full Time	3	daladala
SOC/DART/KN/KMR/004A	Other	no relation		Other	Full Time	2	daladala
SOC/DART/KN/KMR/004A	Other	no relation		Other	Full Time	7	daladala
SOC/DART/KN/KMR/005	Manager	no relation			Full Time	2	walking
SOC/DART/KN/KMR/005	Other	no relation		Other	Full Time	1	daladala
SOC/DART/KN/KMR/005	Other	no relation		Other	Full Time	υ	daladala
SOC/DART/KN/KMR/005A	Owner	not applicable		not applicable	Full Time	3	daladala
SOC/DART/KN/KMR/005A	Manager	not applicable		not applicable	Full Time	3	daladala
SOC/DART/KN/KMR/005A	Other	no relation		Other	Full Time	1	daladala
SOC/DART/KN/KMR/005B	Owner	not applicable		not applicable	Full Time	4	daladala
SOC/DART/KN/KMR/005B	Manager	no relation			Full Time	4	datadala
SOC/DART/KN/KMR/005B	Other	no relation		Other	Full Time	4	daladala
SOC/DART/KN/KMR/006A	Owner	not applicable		not applicable	Full Time	5	car-motorbike
SOC/Erspire Plan N	egalises			Lat could	Full Time	5	wałking
SOC/I+++ T F, '+ K + II + ····	rule e	· · · · · · · · · · · · · · · · · · ·		hat or d	Full Time	1	walking

Annex 03 - Part 4 Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnarie

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Code of this Socio-Economic Lau	N' of Employee	Relation to Owner	Laves within 300m of corridor?	Lype of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
SOC	Úth i	n relation	11	tel and	فمرا المرا	1	، د ا ار
SO(11381 8, 5 8 18 191 4	t ut	ي مارا م		tot or d	1U T., e	· ·	,
SO(UNFIN' N'IF !!! 5				tar o d	I dl Tarre	1	d1
SOC LIGET F N'TE IN A	ente,			b I co d	F U Î		، ادا واول
SOC/DART/KN/KMR/006A	Other	no relation		Informal	Full Time	5	daladala
SOC/DART/KN/KMR/006A	Other	0 - 14 bit		Int and	Full Time	5	daladala
SOC/DART/KN/KMR/006A	Other	n i liter		tet is d	Full Time	5	walking
SOC/DART/KN/KMR/006A	Other	no relation		Informal	Full Time	5	daladala
SOC/DART/KN/KMR/006A	Other	no relation		Informal	Full Time	5	walking
SOC/DART/KN/KMR/006B	Owner	not applicable		not applicable	Full Time	21	car-motorbike
SOC/DART/KN/KMR/006B	Other	no relation			Full Time	4	daladala
SOC/DART/KN/# ' 16		e petato o			Full Time	13	попе
SOC/DART/KN/KMR/006B	Other	no relation			Full Time	2	daladala
Sign to the Tare to Addie concrete					Full Time	14	none
See telkingt kilkere e	• 11 .				Full Time	20	none
SOC/DART/KN/KMR/006B	Other	no relation		· · · · · · · · · · · · · · · · · · ·	Full Time	7	daladala
SOC/DART/KN/KMR/006B	Other	no relation		· · · · · · · · · · · · · · · · · · ·	Full Time	4	daladala
SOC/DART E' E' IF U' U		ە بايلەت ئ			Full Time	21	none
SOC/DART/KN/KMR/006B	Other	no relation			Full Time	18	daladala
SOC/DART/KN/KMR/006B	Other	no relation			Full Time	17	daladala
SOC/DART/KN/KMR/007A	Owner	not applicable		not applicable	Full Time	22	car-motorbike
SOC/DART/KN/KMR/007C	Owner	not applicable		not applicable	Full Time	3	none
SOC/DART/KN/KMR/007C	Other	no relation		Registered employee	Full Time	3	none

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Annex 03 - Part 4 Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnarie

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Code of this Socio-Economic Limit	N' of Employee	Relation to Owner	Laves within 360m of corridor*	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
SOC/DART/KN/KMR/007C	Other	no relation	++	Registered employee	Full Time	3	none
SOC IL TE TIK'S ETTE ONLY	12 m ·			به باری ده	Partial Time	18	car-motorbike
SOC UNKIN NIK MIT	1.a	A + 1.00 m		Republic damations	Full Time	8	daladala
SOC 1146 T 8,1 8 116 10111		1.0		E generation for	Full Time	3	daladala
SOC ETTEL NO. N'IE HICLE				المرتبع المرتبع المرتبع المرتبع المرتبع المرتبع المرتبع المرتبع المرتبع المرتبع المرتبع المرتبع المرتبع المرتبع	Full Time	2	daladala
SOC UTINEN' NºTE INCLE	1 Marc			k junitary)	Full Time	1	daladala
SOC UNITINE PHENOTY				E construction for the	Full Time	2	daladala
SOC INTERIO TENTE	• ut; .	a s Cars		Regela Isógi	Full Time	2	daladala
SOC INTERT AT MOTOR TO	Únter -			المسرف معادر أ	Full Time	6	daladala
SOC IT IF THE ATTENDED	·			the contract of the t	Full Time	12	daladala
SOCI FIFT File of (Full Time	2	daladala
SOC FOR ENTIFICATION		11		ارت 1 مراجع ا	Full Time	2	daladala
SOC FOLD A NUMBER OF THE OWNER	Único.			É prove d'arget a	Full Time	2	daladala
SOC/DART/KN/KMR/008	Owner	not applicable		not applicable	Full Time	1	daladala
SOC/DART/KN/KMR/008	Manager	agreggates		Other	Partial Time	1	car-motorbike
SOC/DART/KN/KMR/008	Other	no relation		Registered employee	Full Time	1	daladala
SOC UTET N' N' IF	196.	a tata		Frank troub	Full Time	1	daladala
SOC/DART/KN/KMR/008	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/KN/KMR/008	1.01.55			• • • • • • • employee	Full Time	1	daladala
SOC/DART/KN/KMR/008				⊢ ⊨ employee	Full Time	1	daladala
SOC/DART/KN/KMR/008	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/KN/KMR/008	• nb.	- 1 µ - A		t pringtingt -	Full Time	1	daladais
SOC/DART/KN/KMR/008A	Owner	not applicable		not applicable	Full Time	1	car-motorbike

Annex 03 - Part 4 Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnarie

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Code of this Socio-Economic Unit	N° of Employee	Relation to Owner	Laver within 500m of corridor?	Type of Work Contract	Working Period	How long does the person have this occupation"	Means of Transport to Work
SOC/DART/KN/KMR/008A	Other	no relation		Registered employ ee	Full Time	I	daladala
SOC/DART/KN/KMR/008A	Other	no relation		Registered employ ee	Full Time	1	daladała
SOC/DART/KN/KMR/009A	Owner	not applicable		on on plicable	Partial Time	2	daladala
SOC/DART/KN/KMR/009A	Manager	no relation		Registered employ ee	Partial Time	1	walking
SOC/DART/KN/KMR/009A				Electric Program.	Partial Time	1	walking
SOC/DART/KN/KMR/009A		e e Lació		Example 1. Stars	Partial Time	1	walking
SOC/DART/KN/KMR/009A	1. Ulas	n a data a		Kaya ta gita ay t	Partial Time	1	walking
SOC/DART/KN/KMR/009B		م د زرادها		o rapicali.	Full Time	4	daladala
SOC/DART/KN/KMR/009B	21 m. pr.	ils e chie		€., to £.ogt	Fuli Time	1	daladala
SOC/DART/KN/KMR/009C	Owner	not applicable		not applicable	Full Time	4	daladala
SOC/DART/KN/KMR/009D	Owner	يال ليهانه		a 1946 d15	Full Time	2	walking
SOC/DART/KN/KMR/009D	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/KN/KMR/009D	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/KN/KMR/009D	Other	no relation		Registered employee	Full Time	1	dalədalə
SOC/DART/KN/KMR/009D	Other	no relation		Registered employ ee	Full Time	1	daladala
SOC/DART/KN/KMR/009E	Owner			not applicable	Full Time	1	walking
SOC/DART/KN/KMR/009E	Other			Registered employee	Full Time	1	none
SOC/DART/KN/KMR/009F	Owner	not applicable		not applicable	Full Time	6	daladala
SOC/DART/KN/KMR/009F	Other	no relati			Full Time	2	none
SOC/DART/KN/KMR/009G	Owner	not applicable		not applicable	Full Time	2	datadala
SOC/DART/KN/KMR/009G	Other	no relation		й _г . г. т. _г . vee	Full Time	1	daladala
SOC/DART/KN/KMR/011A	Owner	not applicable		not applicable	Full Time	1	daladala
SOC/DART/KN/KMR/011A	Other	no relation		Registered employee	Full Time	t	daladala

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Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnarie

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Code of this Socio-Economic Unit	N ^o of Employee	Relation to Owner	Lives within 366m of corridor?	I spe of Work Cuntraci	Working Period	How long dues the person have this occupation?	Means of Transport to Work
SOC/DART/KN/KMR/012	Owner	not applicable		not applicable	Full Time	3	walking
SOC/DART/KN/KMR/012A	Owner	not applicable		not applicable	Partial Time	1	walking
SOC/DART/KN/KMR/012A	Other	agreggates		Other	Full Time	1	walking
SOC/DART/KN/KMR/012B	Owner	not applicable		not applicable	Partial Time	6	walking
SOC/DART/KN/KMR/012B	Other	agreggates		Other	Partial Time	3	walking
SOC/DART/KN/KMR/013	Owner	not applicable		not applicable	Partial Time	3	daladala
SOC/DART/KN/KMR/0]3A		الد اين ب			E JULIE THE	: -	walking
SOC/DART/KN/KMR/013B	Owner	not applicable		not applicable	Partial Time	2	daladala
SOC/DART/KN/KMR/013C		التعليونية		مالر بلري 1 م	al Time، رو	8	walking
SOC/DART/KN/KMR/013D		- الحارو ب		n raph die	•al Time	7	walking
SOC/DART/KN/KMR/013D	Other	no relation		Registered employee	Partial Time	3	daladala
SOC/DART/KN/KMR/014	Owner	not applicable		not applicable	Full Time	16	car-motorbike
SOC/DART/KN/KMR/014	Manager	no relation		Registered employee	Full Time	16	daladala
SOC/DART/KN/KMR/014	Other	other relative		Registered employee	Full Time	16	daladala
SOC/DART/KN/KMR/014	Other	no relation		Registered employee	Full Time	7	daladala
SOC/DART/KN/KMR/014	Other	no relation		Registered employee	Full Time	4	none
SOC/DART/KN/KMR/014	Other	no relation		Registered employ ee	Full Time	3	daladala
SOC/DART/KN/KN1+1111	0 to			البيارينية.	1.0110	8	walking
SOC/DART/KN/KMR/014B	Owner	not applicable		not applicable	Full Time	6	car-motorbike
SOC/DART/KN/KMR/014B	Other	other relative		Registered employee	Full Time	4	daladala
SOC/DART/KN/KMR/014C		t gjb atle		· · · · · · · · cable	Full Time	5	walking
SOC/DART/KN/KMR/014C	Other	other relative		Registered employee	Full Time	2	walking
SOC/DART/KN/KMR/014C	Other	no relation		Registered employee	Full Time	2	walking

Annex 03 - Part 4 Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnarie

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Code of this Socio-Economic Unit	N° of Employee	Relation to Owner	Lives within States of corridor?	Type of Work Contract	Working Period	How long dues the person have this occupation?	Means of Transport to Work
боло добила К ^а ло било сало				- Հայիսի	t dt i	3	walking
5-0 1-1 1 5 2 8 11 - 140	Luter	1. i.e.l.ji		E print og Lore	Fat 1.	3	walking
ξεπ. το κ.) κ.'τκτηύ στημ	Quites.			Product 1	F 10 to .	3	none
SOC/DART/KN/KMR/014E	Owner	not applicable		not applicable	Full Time	7	daladala
SOC/DART/KN/KMR/015	Øwner	not applicable		not applicable	Full Time	25	car-motorbike
SOC/DART/KN/KMR/015	Manager	other relative		Registered employee	Full Time	20	walking
SOC/DART/KN/KMR/015	Other	no relation		Registered employee	Full Time	16	daladala
SOC/DART/KN/KMR/015	Other	no relation		Registered employee	Full Time	4	walking
SOC/DART/KN/KMR/015	Other	no relation		Registered employee	Full Time	6	walking
SOC/DART/KN/KMR/015	Other	no relation		Registered employee	Full Time	10	walking
SOC/DART/KN/KMR/015	Other	no relation		Registered employee	Full Time	3	walking
SOC/DART/KN/KMR/015A	Owner	not applicable		not applicable	Full Time	6	car-motorbike
SOC/DART/KN/KMR/015A	Manager	not applicable			Full Time	6	car-motorbike
SOC/DART/KN/KMR/015A	Other	no relation		Registered employee	Full Time	6	daladala
SOC/DART/KN/KMR/015A	Other	no relation		Registered employee	Full Time	6	daladala
SOC/DART/KN/KMR/015A	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/KN/KMR/017	Owner	not applicable		not applicable	Full Time	22	car-motorbike
SOC/DART/KN/KMR/017	Manager			Not informed	Full Time	22	car-motorbike
SOC/DART/KN/KMR/017	Other	no relation		Other	Full Time	18	walking
SOC/DART/KN/KMR/017	Other	no relation		Other	Full Time	10	walkmg
SOC/DART/KN/KMR/017	Other	no relation		Other	Full Time	10	walking
SOC/DART/KN/KMR/017	Other	other relative		Other	Full Time	5	walking
SOC/DART/KN/KMR/017B	Owner	not applicable		not applicable	Full Time	8	car-motorbike

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Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnarie

¹ Code of this Socio-Economic Unit	N° of Employee	Relation to Owner	Lives within 300m of corridar?	Type of Work Contract	Working Period	How long does the person have this occupation"	Means of Transport to Work
SOC/DART/KN/KMR/017B	Manager			Not informed	Full Time	8	car-motorbike
SOC/DART/KN/KMR/017B	Other	other relative		Other	Full Time	7	car-motorbike
SOC/DART/KN/KMR/017B	Other	no relation		Other	Full Time	3	daladala
SOC/DART/KN/KMR/017C	Owner	not applicable		not applicable	Full Time	10	daladala
SOC/DART/KN/KMR/017C	Other	other relative		Other	Full Time	4	daladala
SOC/DART/KN/KMR/017C	Other	agreggates		Other	Full Time	1	daladala
SOC/DART/KN/KMR/017C	Other	agreggates		Other	Full Time	1	daladala
SOC/DART/KN/KMR/020	Owner	not applicable		not applicable	Full Time	υ	none
SOC/DART/KN/KMR/020	ttor	يت بارايت ،		Int nd	C.B. Los		daladala
SOC/DART/KN/KMR/020A	Owner	not applicable		not applicable	Full Time	3	none
SOC/DART/KN/KMR/020A	Manager	no relation		Other	Full Time	3	none
SOC/DART/KN/KMR/020A	Other	no relation		Other	Full Time	2	none
SOC/DART/KN/KMR/020A	Other	no relation		Other	Fuli Time	1	none
SOC/DART/KN/KMR/023	Owner	not applicable		not applicable	Full Time	28	daladala
SOC/DART/KN/KMR/023	Manager	no relation		Registered employee	Full Time		daladala
SOC/DART/KN/KMR/025B	Owner	not applicable		not applicable	Full Time	3	none
SOC/DART/KN/KMR/025B	Manager	no relation		Other	Full Time	3	none
SOC/DART/KN/KMR/025B	Other	no relation		Other	Full Time	3	none
SOC/DART/KN/KMR/026	Owner	not applicable		not applicable	Full Time	0	walking
SOC/DART/KN/KMR/026	Other	no relation		Registered employee	Full Time	()	walking
SOC/DART/KN/KMR/026	Other	no relation		Registered employee	Full Time	0	walking
SOC/DART/KN/KMR/026	Other	no relation		Registered employee	Full Time	()	walking
SOC/DART/KN/KMR/026B		علل غارو ت ،		· · · · · · · · · · · · · · · · · · ·	fall taus	17	none



Annex 03 - Part 4 Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnarie

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Code of this Socio-F conomic Unit	N' of Employee	Relation to Owner	Lives within 360m of corridor?	Type of Work Contract	Werking Period	How long does the person have this occupation?	Means of Tran-port to Work
10 1028 T 6.5 6.18 0.1		e taptent		ւդլև վե	f f	13	none
on other of Kineses	11.05	الياري ا		ما في ما يو يا ي]	none
SOC/DART/KN/KMR/029	Owner	not applicable		not applicable	Full Time	3	none
SOC/DART/KN/KMR/029A	Owner	not applicable		not applicable	Partial Time	2	daladala
SOC/DART/KN/KMR/029A	Other	other relative		Informal	Full Time	2	daladala
SOC/DART/KN/KMR/029B	Owner	not applicable		not applicable	Full Time	10	daladala
SOC/DART/KN/KMR/029B	. ut	den el la		1	Const Loos	4	none
SOC/DART/KN/KMR/031	Owner	not applicable		not applicable	Full Time	y y	daladala
SOC/DART/KN/KMR/031	Other	no relation		Other	Fuli Time	9	daladala
SOC/DART/KN/KMR/031	Other	no relation		Other	Full Time	4	daladala
SOC/DART/KN/KND/002A	Owner	not applicable		not applicable	Full Time	3	car-motorbike
SOC/DART/KN/KND/002A	Other	no relation		Registered employ ee	Full Time	t	car-motorbike
SOC/DART/KN/MG2/(a naph an			ĒIJī	6	none
SOC/DART/KN/MG2/IIII	11	e e produce			F. (U. 1	6	none
SOC/DART/KN/MG2/001	Other	no relation		Other	Full Time	5	daladala
SOC/DART/KN/MG2/001	Other	no relation		Other	Full Time	2	daladala
SOC/DART/KN/MG2/002	Owner	not applicable		not applicable	Partial Time	U	walking
SOC/DART/KN/MG2/002	Manager	other relative		Other	Partial Time	0	walking
SOC/DART/KN/MG2/002D	Plan 2 1	the conflicts		€ ₁₂ in etropt	Latifa .	0	daladala
SOC/DART/KN/MG2/003	Owner	not applicable		not applicable	Full Time	6	daladala
SOC/DART/KN/MG2/003	Manager	other relative		Informal	Partial Time	3	daladala
SOC/DART/KN/MG2/003A	Owner	not applicable		not applicable	Full Time	7	car-motorbike
SOC/DART/KN/MG2/003A	Manager	no relation		Registered employee	Full Time	1	daladala

Annex 03 - Part 4 <u>Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnarie</u>

Code of this Socio-Economic Limit	N [.] of Employee	Relation to Owner	Lives within Journ of corridor?	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
SOC/DART/KN/MG2/003B	Owner	not applicable		not applicable	Full Time	5	daladala
SOC/DART/KN/MG2/003B	Manager	no relation			Full Time	5	daladala
SOC/DART/KN/MG2/003C	11 8.5	م بورد ما			Full Time	7	daladala
SOC/DART/KN/MG2/003C	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/KN/MG2/003C	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/KN/MG2/003C	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/KN/MG2/003C	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/KN/MG2/003C	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/KN/MG2/004A	<u>О. в.</u> ,	بنارو ا		5 L 41 6 - 11	Fall to a	5	daladala
SOC/DART/KN/MG2/004A	11	A . L 0		E. S. Marken and States	1.01.1	5	daladala
SOC/DART/KN/MG2/004A					1 .,II T	5	daladala
SOC/DART/KN/NIG2/004A	····.			E principalita	Lutt Lute	2	daladala
SOC/DART/KN/MG2/(#)4A	1 theo			Ester Brout at	1.01.7	3	daladala
SOC/DART/KN/MG2/(#)4A				E generation to a	1.01.T	5	daladala
SOC/DART/KN/MG2/004A	• • • •	2.516		A CONTRACTOR OF A CONTRACTOR OF A CONTRACTOR OF A CONTRACTOR A CONTRAC	1.01.1.0	2	daladala
SOC/DART/KN/MG2/004A				10 Jan 10 Jan	Lottin.	2	daladala
SOC/DART/KN/MG2/004A	ente a			Figure 1 right	Full Turn	3	daladala
SOC/DART/KN/MG2/004A	Other	no relation		Registered employee	Full Time	3	daladala
SOU HAFT FILTE LINES		tar.a		L pringet south a	, i 16,4	5	daladala
SOC/DART/KN/MG2/005	Other	other relative		Other	Full Time	17	daladala
SOC/DART/KN/MG2/005	Other	other relative		Other	Full Time	18	daladala
SOC/DART/KN/MG2/005	Other	other relative		Other	Full Time	1	daladala
SOC/DART/KN/MG2/005	Other	other relative		Other	Full Time	14	daladala

Annex 03 - Part 4

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Code of this Socio-Economic Unit	N° of Employee	Relation to Owner	Lives within 360m of corridor"	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
SOC/DART/KN/MG2/005	Other	other relative		Other	Full Time	18	daladala
SOC/DART/KN/MG2/005A	Manager	other relative		Other	Full Time	1	daladala
SOC/DART/KN/MG2/005A	Other	no relation		Other	Full Time	0	daladala
SOC/DART/KN/MG2/005C	Owner	not applicable		not applicable	Full Time		none
SOC/DART/KN/MG2/005C	Other	no relation		Other	Full Time	4	daladala
SOC/DART/KN/MG2/005C	Other	no relation		Other	Full Time	24	daladala
SOC/DART/KN/MG2/005D	Manager	other relative		Other	Full Time	1	daladala
SOC/DART/KN/MNY/001	Owner	not applicable		not applicable	Full Time	26	car-motorbike
SOC/DART/KN/MNY/001	Manager	not applicable		Registered employee	Full Time	9	car-motorbike
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	18	daladala
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	0	daladala
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	n	daladala
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	7	daladala
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	5	daladata
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	5	daladala
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	0	daladala
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	5	daladala
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	5	daladala

Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnarie

Legend Yes Nortiot Informed

Annex 03 - Part 4 Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnarie

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Data Dana Politica to Section 05 03 (inc.)							
Code of this Socio-Economic Unit	N' of Employee	Relation to Owner	Lives within 300m of corridor"	Type of Work Contract	Working Period	How long does the person have this occupation"	Means of Transport to Work
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	7	daladala
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	6	daladala
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	6	daladala
SOC/DART/KN/MNY/001	Other	no relation		Registered employ ee	Full Time	6	daladala
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/KN/UB2/001	Owner	not applicable		not applicable	Full Time	7	daladala
SOC/DART/KN/UB2/001	Manager	no relation			Full Time	1	daladala
SOC/DART/KN/UB2/001	Other	no relation		Registered employee	Full Time	8	daladala
SOC/DART/KN/UB2/001	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/KN/UB2/003	Owner	not applicable		not applicable	Full Time	17	car-motorbike
SOC/DART/KN/UB2/003	Other	no relation		Registered employee	Full Time	15	daladala
SOC/DART/KN/UB2/003	Other	no relation		Informal	Full Time	5	daladala
SOC/DART/KN/UB2/003	Other	no relation		Registered employee	Full Time	10	daladala
SOC/DART/KN/UB2/003	Other	no relation		Registered employee	Full Time	15	car-motorbike
SOC/DART/KN/UB2/003	Other	no relation		Registered employee	Full Time	15	car-motorbike
SOC/DART/KN/UB2/003A	Owner	not applicable		not applicable	Full Time	20	car-motorbike
SOC/DART/KN/UB2/003A	Other	son/daughter		Informal	Partial Time	6	car-motorbike
SOC/DART/KN/UB2/004	Owner	not applicable		not applicable	Full Time	17	car-motorbike
SOC/DART/KN/UB2/004	Other	no relation		Registered employee	Full Time	15	daladala
SOC/DART/KN/UB2/004	Other	no relation		Registered employee	Full Time	5	daladala
SOC/DART/KN/UB2/004	Other	son/daughter		Informal	Partial Time	7	daladala
SOC/DART/KN/UB2/004	Other	son/daughter		Registered employee	Partial Time	10	daladala

Legend Yes NotNotInformed

Annex 03 - Part 4

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Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnarie

Code of this Socio-Economic Unit	N° of Employee	Relation 1/ Owner	Lives within 300m of corridor*	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
SOC/DART/KN/LL. III		ال عام و د د.		متى الم ال			,11 <u>.</u>
SOC/DART/KN/UB2/005	Other	other relative		Registered employee	Full Time	2	daladala
SOC/DART/KN/UB2/006	Owner	not applicable		not applicable	Full Time	3	daladala
SOC/DART/KN/UB2/006	Other	no relation		Registered employ ee	Full Time	3	daladala
SOC/DART/KN/UB2/007B	Owner	not applicable		not applicable	Full Time	14	daladala
SOC/DART/KN/UB2/008A	Owner	not applicable		not applicable	Partial Time	25	daladala
SOC/DART/KN/UB2/009	Owner	not applicable		not applicable		1	daladala
SOC/DART/KN/UB2/010	Owner	not applicable		not applicable	Full Time	5	daladala
SOC/DART/KN/UB2/010	Other	agreggates			Full Time	4	daladala
SOC/DART/KN/UB2/011	Owner	not applicable		not applicable	Full Time	13	daladala
SOC/DART/KN/UB2/012A	Owner	not applicable		not applicable	Full Time	23	daladala
SOC/DART/KN/UB2/012A	Other	other relative		Registered employee	Full Time	12	daladala
SOC/DART/KN/UB2/013	Owner	not applicable		not applicable	Full Time	2	daladala
SOC/DART/KN/UB2/014	Owner	not applicable		not applicable	Full Time	9	daladala
SOC/DART/KN/UB2/014	Other	no relation		Informal	Full Time	3	daladala
SOC/DART/KN/UB2/014	Other	no relation		Informal	Full Time	5	daladala
SOC/DART/KN/UB2/016	Owner	not applicable		not applicable	Full Time	10	daladala
SOC/DART/KN/UB2/016	Manager	other relative		Registered employee	Full Time	8	daladala
SOC/DART/KN/				ياليول ب	Full Time	12	walking
SOC/DART/KN/UB2/021	Owner	not applicable		not applicable	Full Time	4	car-motorbike
SOC/DART/KN/UB2/021	Manager	no relation		Registered employee	Full Time	4	car-motorbike
SOC/DART/KN/UB2/021	Other	no relation		Registered employee	Full Time	0	daladala
SOC/DART/KN/UB2/021	Other	no relation		Registered employee	Full Time	4	daladala

Annex 03 - Part 4 Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnarie

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Code of this Socio-Economic Unit	N° of Employee	Relation to Owner	Lives within 300m of corridor?	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport in Work
SOC/DART/KN/UB2/021	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/KN/UB2/021	Other	no relation		Registered employee	Full Time	4	daladala
SOC/DART/KN/UB2/021	Other	no relation		Registered employee	Full Time	4	daladala
SOC/DART/KN/UB2/021	Other	no relation		Registered employee	Full Time	4	car-motorbike

Legend
 Yes
No Not Informed

ANNEX 3.5 - RAP REPORT Commercial Ressettemnt

Annex 03 - Part 5

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 Data Bank related to Section: 04 and 05 of the S 	ecte-conemic '	Survey - Commercial	Activities Que	stionnarie

Code of Corresponding Property Survey	Code of this Socio-Economic Unit	Average monthly sales	Average nei monthly profit	Av süsble Documentation	Bank Loans	Mörigage Inform Value	Residential area within the property
VAL/DART II GRN 1001	The interview was not done						
VAL/DART/IL/GRN/001	SOC/DART/IL/GRN/001	21 000 000	500 000				
VAL/DART/IL/GRN/002	SOC/DART/IL/GRN/002	4 500 000	900 000				
VAL/DART/IL/GRN/002	The intersien was not done						
VAL/DART/IL/GRN/0+1	1.00 Trail T II 1.188, 100 (8		: No 100				
VAL/DART/IL/GRN/0+3	ling for the first second	ý confiction	1.,01000				
VAL/DART/IL/GRN/0">	en (esci ger) oues	4 111000	19 (00)				
VAL/DART/IL/GRN/003	SOC/DART/IL/GRN/003B						
VAL/DART/IL/GRN/003	SOC/DART/IL/GRN/003C	75 000 000	600 000				
VAL/DART/IL/GRN/004	SOC/DART/IL/GRN/004	1 800 000					
VAL/DART/IL/GRN 1014	The interview was not derive						
VAL/DART/IL/GRN/004	SOC/DART/IL/GRN/004A	1 500 000	450 000				
VAL/DART/IL/GRN/005	SOC/DART/IL/GRN/005	900 000	400 000				
VAL/DART/IL/GRN/005	The interview was not done						
VAL/DART/IL/GRN/005	SOC/DART/IL/GRN/005A	90 000	30 000				
VAL/DART/IL/GRN mir	ราย มีประที่มีปี ปี หรือการ	L outcoud					
VAL/DART/IL/GRN Dur	τη, Έτακ (1 <u>1</u> τ. κ. ¹ . 1000		4 11100				
VAL/DART/IL/GRN/006	SOC/DART/IL/GRN/006B	400 000	150 000				
VAL/DART/IL/GRN unir	απ îr £11 <u>1</u> + ε' απα	1 (61/00)	1911081				
VAL/DART/IL/GRN/008	SOC/DART/IL/GRN/008	30 000 000					
VAL/DART/IL/GRN/008	SOC/DART/IL/GRN/008A	10 500 000	1 200 000				
VAL/DART/IL/GRN/008	SOC/DAR T/IL/GRN/008B	6 000 000	1 200 000				

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Data Bank related to Sections 04 and 05 of the	Socio-economic Survey - Commercial Activities (Juestionnarie					
Cade of Corresponding Property Survey	Code of this Socia-Economic Unit	Average monthly sales	Average net monthly profit	Available Documentation	Bank Loans	Mortgage Inform value	Residential area within the property
VAL DART II. GKN 1008	The interview was not done						
VAL/DART/IL/GRN/008	SOC/DART/IL/GRN/008C	75 000 000	750 000				
VAL DART IL GRN 002	an Disky golf on		1				
VAL DART IL GRN 000	ra, intéri population (s		.10 10.01				
VAL/DART/IL/GRN/009	SOC/DART/IL/GRN/009C	900 000	450 000				
VAL/DART/IL/GRN/009	SOC/DART/IL/GRN/009E						
VAL/DART/IL/GRN/010	SOC/DART/IL/GRN/010	1 800 000	600 000				
VAL/DART/IL/GRN/011	star 10 VE 1 II station		4000-000				
VAL/DART/IL/GRN/011	SOC/DART/IL/GRN/011A						
VAL/DART/IL/GRN/011	SOC/DART/IL/GRN/011B						
VAL/DART/IL/GRN/012	SOC/DART/IL/GRN/012	450 000	300 000				
VAL/DART/IL/GRN/012	SOC/DART/IL/GRN/012A	2 300 000	1 450 000				
VAL/DART/IL/GRN/012	SOC/DART/IL/GRN/012B						
VAL/DART/IL/GRN/013	SOC/DART/IL/GRN/013	900 000	180 000				
VAL/DART/IL/GRN/013	SOC/DART/IL/GRN/013A	1 200 000	360 000				
VAL/DART/IL/GRN/013	SOC/DART/IL/GRN/013B	2 700 000	150 000				
VAL/DART IL GRN 013	The interview was not done						
VAL/DART/IL/GRN/013	SOC/DART/IL/GRN/013C	1 200 000	180 000			L	
VAL/DART/IL/GRN/015	SOC/DART/IL/GRN/015	360 000	80 000				
VAL/DART/IL/GRN "1"	The interview was not dong						
VAL/DART/IL/GRN/015	SOC/DART/IL/GRN/015A	450 000	150 000				
VAL/DART/IL/GRN/016	SOC/DART/IL/GRN/016						

Data Bank related to Sections 04 and 05 of the Socio-economic Survey - Commercial Activities Questionnarie

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Annex 03 - Part 5

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Annex 03 - Part 5

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Data Bank related to Section: 04 and 05 of the Section commits Survey (Commercial Activities Questionnarie)

Code of Corresponding Property Survey	Code of this Socio-Economic Unit:	Average monthly sales	Average net monthly profit	A v atlable Documentation	Bank Loans	Morigage Inform value	Residential area within the property
VAL/DART/IL/GRN/016	SOC/DART/IL/GRN/016A	1 000 000	500 000				
VAL/DART/IL/GRN/016	SOC/DART/IL/GRN/016B	600 000	250 000				
VAL/DART/IL/GRN/016	SOC/DART/IL/GRN/016C	250 000 000	80 000				
VAL/DART/IL/GRN/017	SOC/DART/IL/GRN/017	12 000 000	750 000				
VAL DART IL GRN 01."	ton it \Kii to,K' ot".	1					
VAL DART IL GRN 017	10 TE ET [] 1 E 10 TE	1,100	1.000				
VALDART IL GRN 91"	on Dolling of Stories		111.101				
VAL DART IL GRN 008	The interview was not done						
VAL/DART/IL/GRN IIIN	contration (Contration)	1					
VAL/DART/IL/GRN/020	SOC/DART/IL/GRN/020						
VAL/DART/IL/GRN "!!	ма фоксцок од		diorrou).				
VAL/DART/IL/GRN/025	SOC/DART/IL/GRN/025	400 000	. 100 000				
VALDART IL GRN 021	200 TEFT 10 + 1017	1 (61)(61)					
VAL DART IL GRN #31	10. DAKT II. (1893) 1	1 1011011					
VALDART B. GRN 001	авісернов'ат.	1.111.1.1111	_ [++++++++++++++++++++++++++++++++++++				
VAL/DART/IL/GRN/034	SOC/DART/IL/GRN/034	6 000 000	450 000				
VAL/DART/IL/GRN/036	SOC/DART/IL/GRN/036	4 500 000	3 000 000				
VAL/DART/IL/GRN/038	SOC/DART/IL/GRN/038	600 000	100 000				
VAL/DART/IL/GRN/047	SOC/DART/IL/GRN/047		1.00000				
VAL/DART/IL/GRN/063	SOC/DART/IL/GRN/063	1.01101.1					
VAL/DART/IL/GRN/068	SOC/DART/IL/GRN/068	a mumu	introduct				
VAL/DART/IL/GRN/069	SOC/DART/IL/GRN/069	9.0110.01	1.00.0				

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Annex 03 - Part 5 Data Bank related

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	Data Bank related to Sections 04 and 05 of the So	cio-economic Survey - Commercial Activities Questionnarie
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Cade of Corresponding Property Survey	Code of this Socio-Economic Unit:	Average monthly sales	Average net monthly profit	۹۱ ailable Documentation	Bank Loans	Morigage Inform value	Residential area within the property
VAL/DART/IL/GRN/074	SOC/DART/IL/GRN/074	690 000	360 000				
VAL/DART/IL/GRN/075	SOC/DART/IL/GRN/075	1 in fuir					
VAL/DART/IL/GRN/077	SOC/DART/IL/GRN/077	1	111001		····		
VAL/DART/IL/GRN/078	SOC/DART/IL/GRN/078	4 11 01 1 01	4 10 004				
VAL/DART/IL/GRN/081	SOC/DART/IL/GRN/081	150 000					
VAL/DART/IL/GRN/085	SOC/DART/IL/GRN/085	450 000	90.000				
VALDARI II. GRN um	τα Τι Ει <u>Π</u> ι Β', οι	1					
VAL/DART/IL/GRN/093	SOC/DART/IL/GRN/093	10 800 000	2 450 000				
VAL/DART/IL/GRN/097	SOC/DART/IL/GRN/097	300 000	150 000	<u>-</u>			
VAL/DART/IL/GRN/098	SOC/DART/IL/GRN/098	100.000					
VAL/DART/IL/GRN/100	SOC/DART/IL/GRN/100	- <u>1</u> 0000					
VAL/DART/IL/GRN/103	SOC/DART/IL/GRN/103	[11101				-	
VAL/DART/KN/HNF/4001	The interview was not done						
VAL/DART/KN/HNF/001	The interview was not done						
VAL/DART/KN/HNF/003	SOC/DART/KN/HNF/003						
VAL/DART/KN/HNF/0004	The interview was not done						
VAL/DART/KN/HNF/ um	The interview was not done						
VAL/DART/KN/HNF/0005	The interview was not done						
VAL/DART/KN/HNF/ 10"	Star LEAFT K' H' Food	4 1011011					
VAL/DART/KN/HNF/·····	00 [046] K2:027.007.0						
VAL/DART/KN/HNF/007	The interview was not dure						
VAL/DART/KN/HNF/007	100 10 8 J 1 7 8 9 1 0 TB	[• COLLOI	1111000				



ANNEX 3.5 - RAP REPORT Commercial Ressettemnt

Annex 03 - Part 5 Data Bank related to Sections 04 and 05 of the Socio-exonomic Survey Commercial Activities Questionnaries

Code of Corresponding Property Survey	Cude of this Sacia-Economic Viair.	Average monthly value	Average net monthly profit	Avadable Documentation	Bank Logns	Morizage Inform value	Revidential area within the property
VAL DART KY HYF my	THE DARLE SHELL OF						
VAL PART KA HALI Dura	CONCRACTION HEREIN						
VAL PART KA HAL IIM	THE DARK NEEDED AND A DARK	: 111001	-				
VAL DARE KN HVE ma	Son TOAPTIK'S HELTOIDAK		1441-441				
VAL DART KY HAF ofo	Son IDARTER (PSE 0100	-	_ 111 (KM)				
VAL DART KA HAF and	See TOAPTEN HISTORIA	-					
VAL DART KY DAT OTT	THE PART PRIME	. 11111100	- 1				
VALDART KN HVF om	A TONE I PARTE AND A TONE AND A TONE I PARTE AND A TONE I PARTE AND A TONE A TONE AND A	1 ANY 10	I DIME				-
VAL DART KY HALMI	And TO AND THE ANAL OF DEPARTMENT	teur car	interinte A				
A AL DART KA HAF III	Yer ENET I. V HIGT OFF	. (BA) 1 (BA)	1 1012011				
V VE DVRT KN HVF mit	so trach and other	, Terrenal	. (10) (17) (10)				
V VI 19481 KN HNI 101	You [MPTI. SHALMAL	likali dili 🔓	, chi chatt				
A AL TART MA HAY MA	Sen Francisco Hadronn	1449344	land they				
V VE DARE KN HNE HE	AN INPERTING OF	frier riter	1 K F 1 F				
A VE DART PA HALMS	A THE PART PARAMETER	* 10811807	11101101				
1 VE DARE KY KYR met	WHE DALLERS KARE OUT						
V VL [FVK] KV KNIK mut	THE TAMP I TO PARTY WATE AND	2 Journal	1.000				
VAL DART KA KAR und	Som LATERNEAD and A	- 141 (191)	11071010				
V VE DVRT KV KVIR mes	or TAFLENKAR on	1.11.64161	strant				
V VE DVKE KV KVIR mix	and IMP I RANKING A	1 - ear man	1011111				
V VE DVRT KV KVIR me	ראב נחורראייע אבר חיזו לאוור						
VAL/DART/KN/KMR/005	רוב יאורי ייזע שזג חיז ליזור						

Page 5 of 10

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Data Bank related to sections 04 and 05 of the	Socio-economic Survey - Commercial Activities		1		<u> </u>	Γ	
Code of Corresponding Property Survey	Code of this Sucio-Economic Linit:	Average monthly sales	Average net monthly profit	Available Documentation	Bank Loans	Morigage Inform value	Residential area within the property
VAL DART KN KMR mis	LUCTURE AT AT MADE OF D	1 contrained	hormon				
VAL DART KN KMR uur	111 TISET & KIE 111 - 1	- 10 DOD1	Lincon				
VAL DART KN KMR uur	NUE DAREKT KITE DE	1 141 144	<u>.</u>				
VAL DARI KN KMK mi	CH LIAFTER KATE OF						
VAL/DART/KN/KMR/007	one to je tik tik tit oo tij		Lorinor				
VAL/DART/KN/KMR/007	OF TRAFTER NOT	1.111	47.1.1				
VAL/DART/KN/KMR/008	OF THEFT FLACTION	11 11 10 1 1					
VAL/DART/KN/KMR/008	10 1150 1 6° K 16 00 4	40041401	1.1.0000				
VAL/DART/KN/KMR/009	SOC/DART/KN/KMR/009A	160 000	120 000				
VAL/DART/KN/KMR/009	SOC/DART/KN/KMR/009B	300 000	200 000				
VAL/DART/KN/KMR/009	SOC/DART/KN/KMR/009C	350 000	220 000				
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Annex 03 - Part 5 Data Bank related to Sections 04 and 05 of the Socio-economic Survey - Commercial Activities Questionnarie

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Annex 05 - Part 5 Data Bank related to Section: 64 and 65 of the Socio-economic Survey - Commercial Activities Questionnarie

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ANNEX 3.5 - RAP REPORT Commercial Ressettemnt -----

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Annex 03 - Part 5

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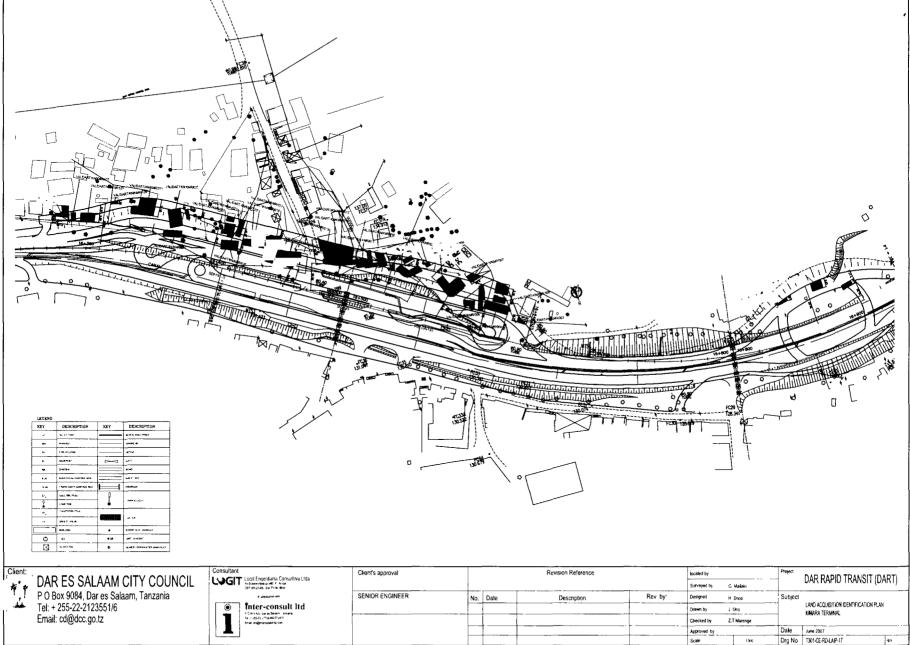
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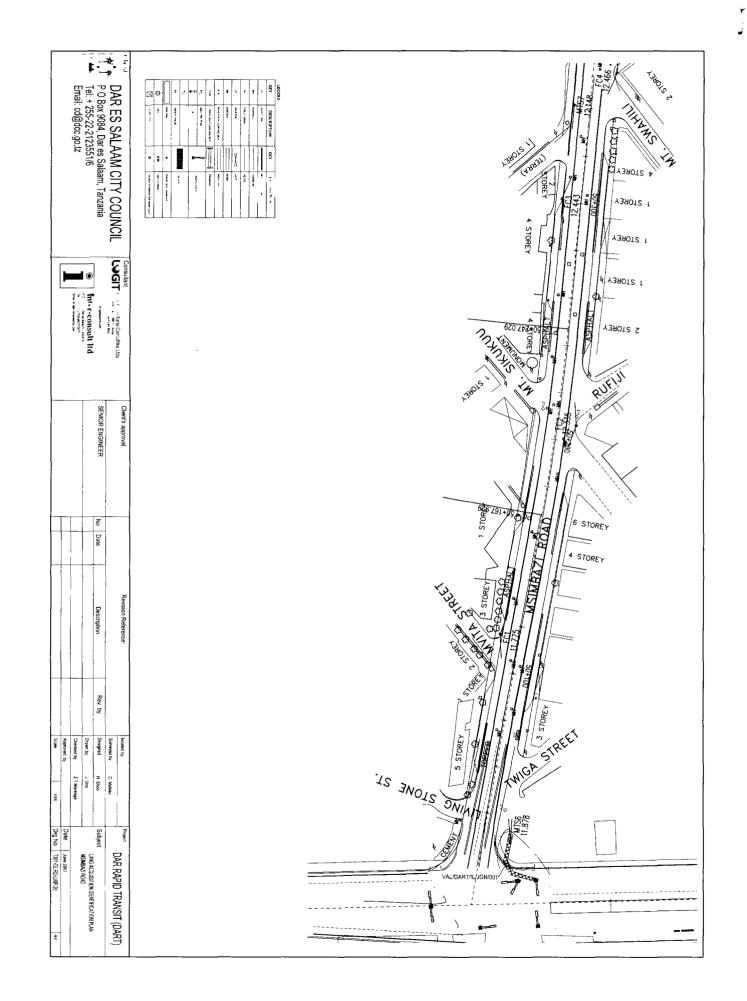
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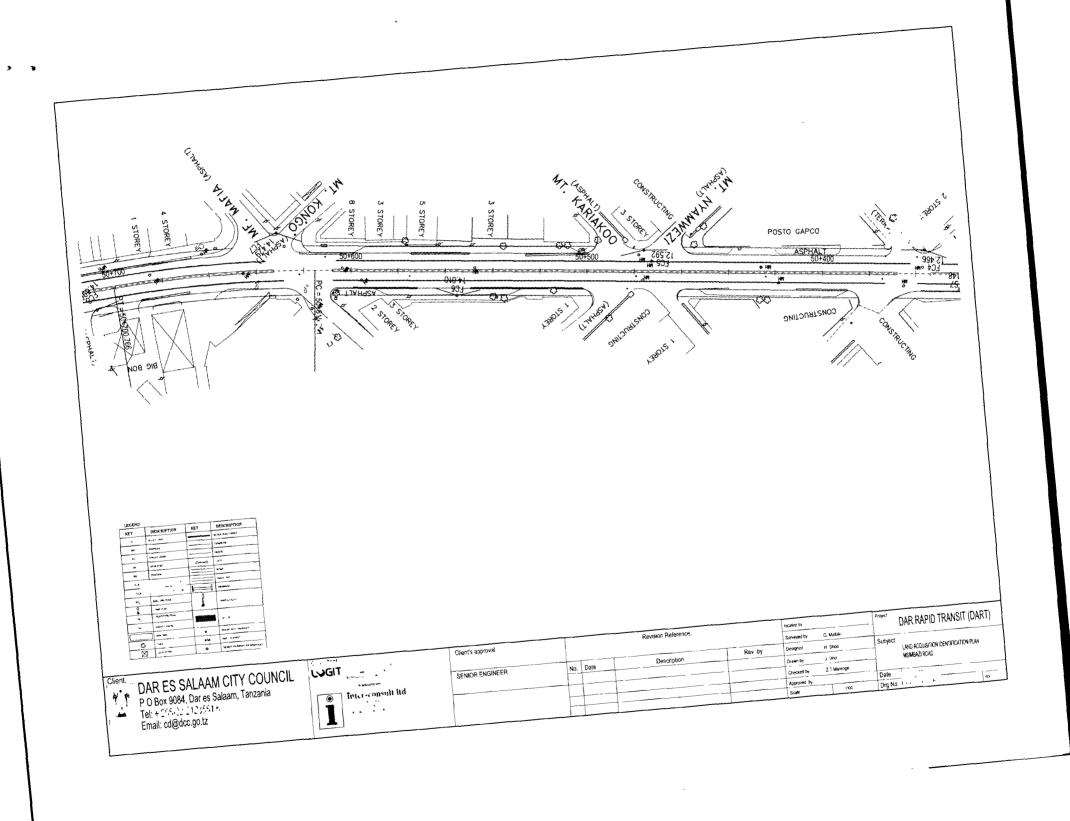
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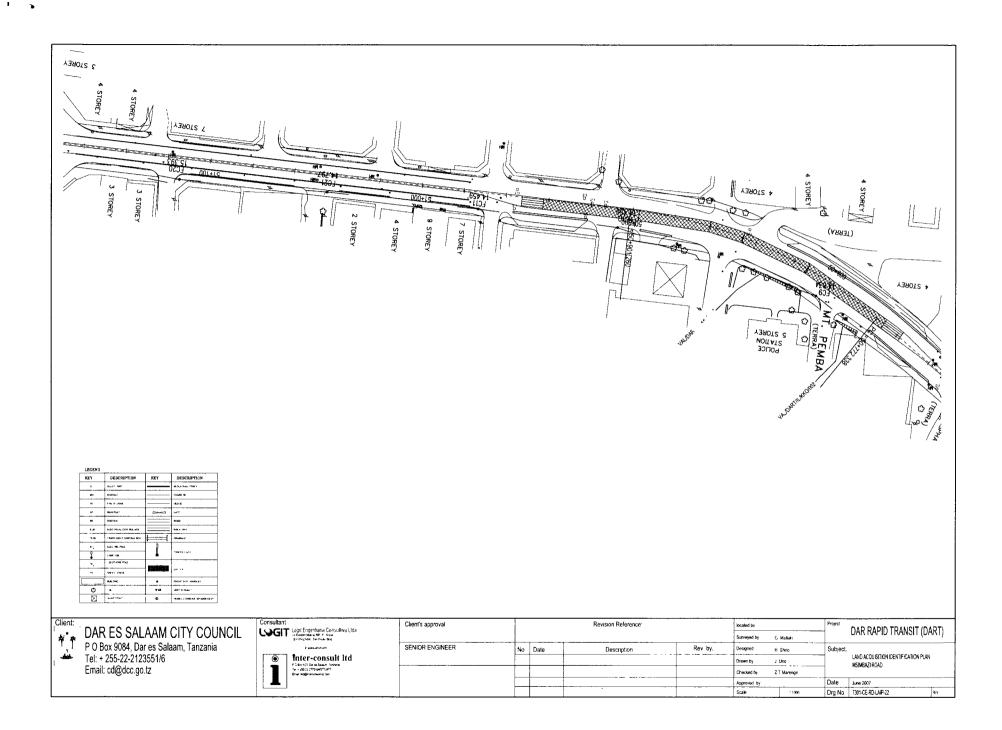
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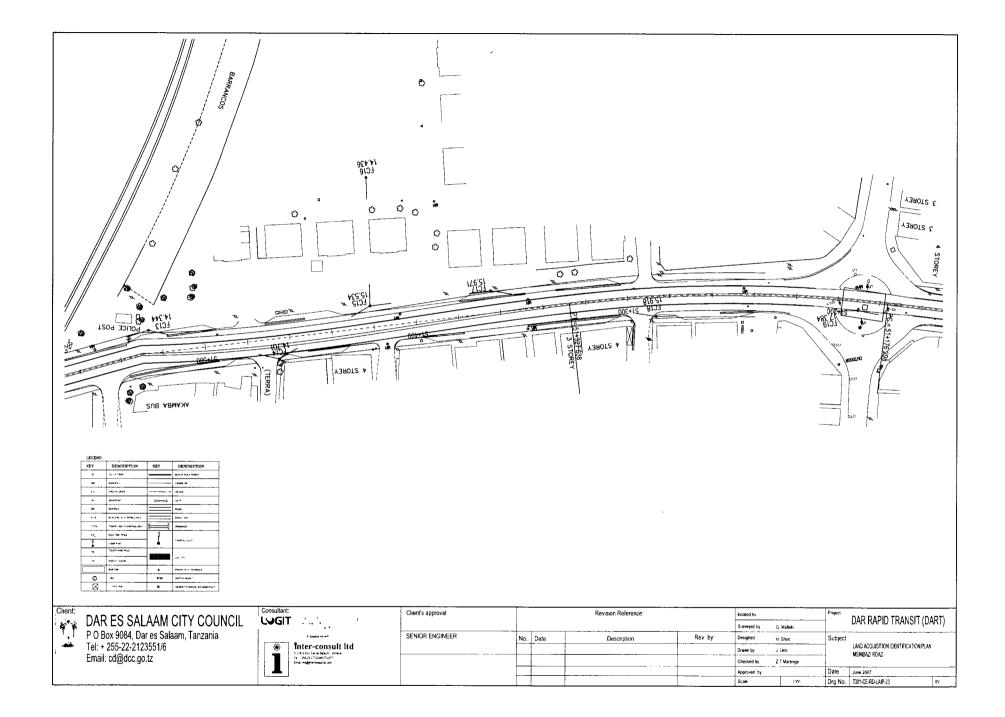






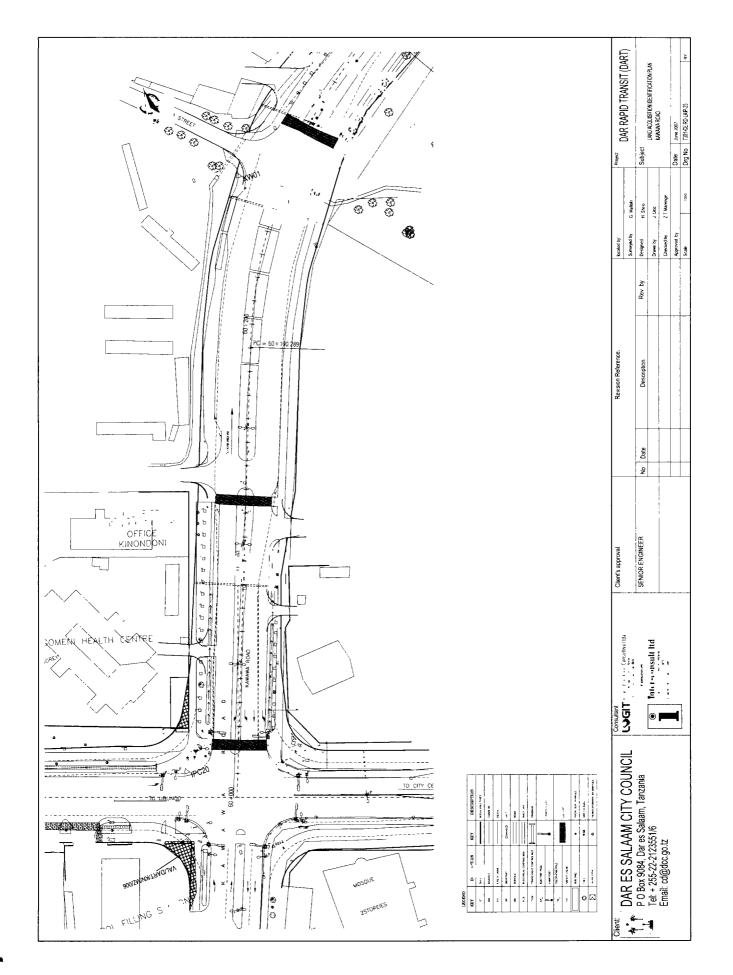


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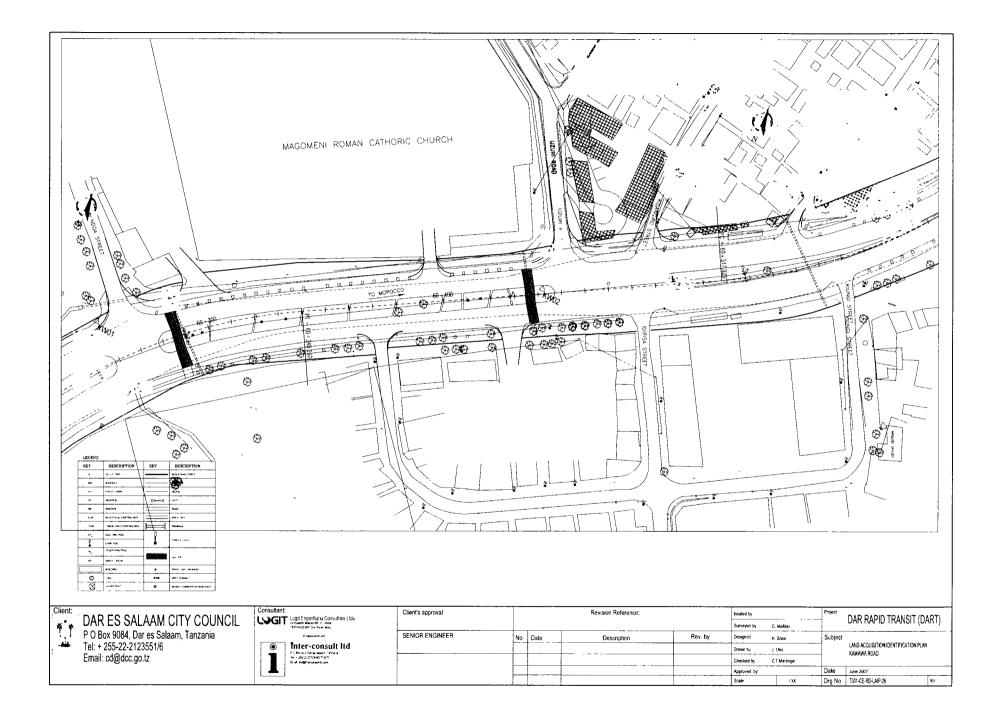


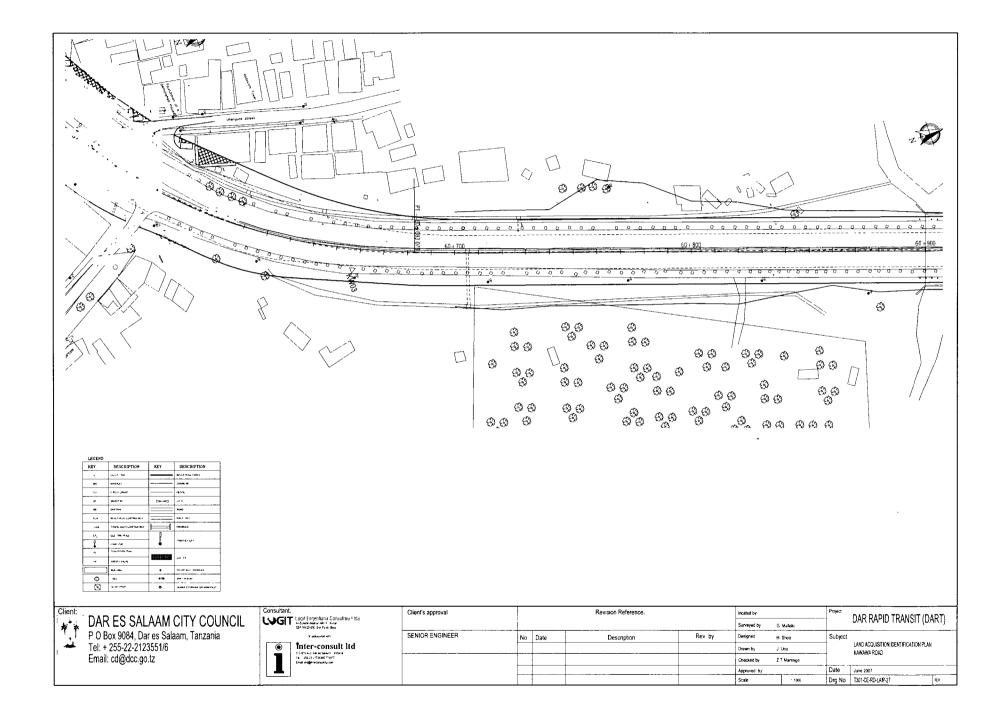
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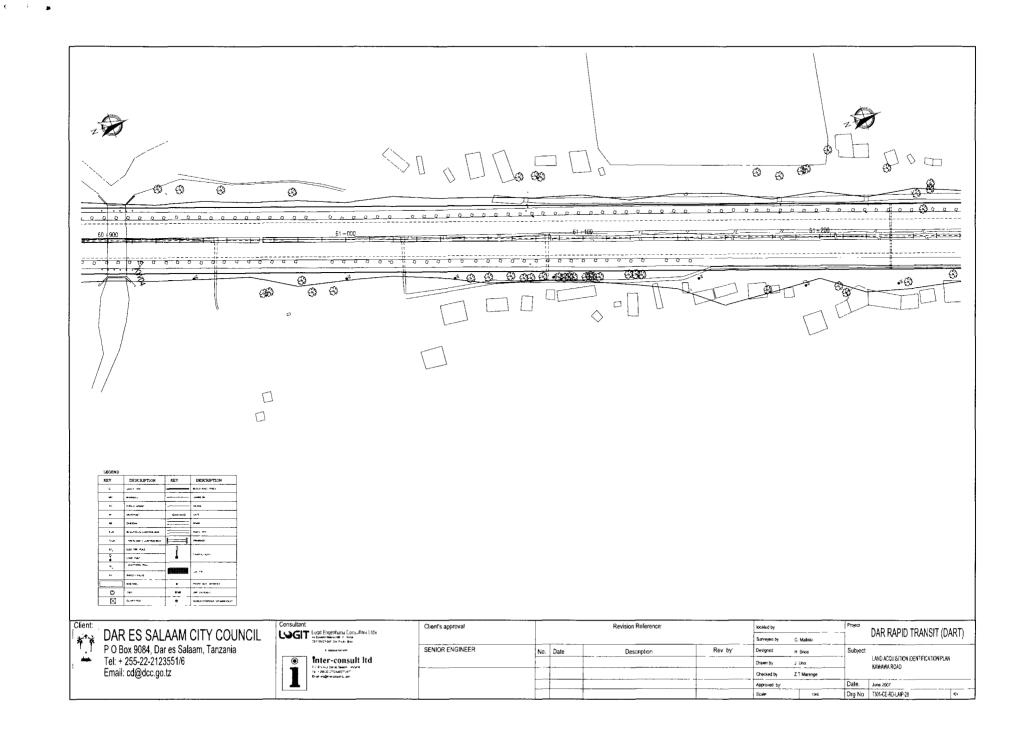
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ANNEX 05

Social Impact Assessment included in the ESIA

ANNEX 05

Socio-Economic Impacts Identified in the Environmental and Social Impact Assessment

Potential Impacts During Construction:

I.7 Potential impacts on road infrastructure and traffic:

7.01 General increase in traffic congestions during construction

All of Phase 1 of the BRT system will be implemented simultaneously, within a two year construction schedule. This will imply in significant limitation of capacity along the alignment, requiring provisional rearrangement of traffic and associated disturbances to pedestrians and adjacent land use. Due to limited extension of Dar Es Salaam's road network, provisional detours are not feasible. Thus, traffic management procedures during construction will have to be restrained to the project's Directly Affected Area (DAA).

Given the importance of the selected corridors, it is likely that impacts on traffic congestion will be widespread and will affect a significant part of Dar Es Salaam's main avenues during most of the construction period.

Though intense, this is a provisional impact that will cease once the project is in operation.

7.02 Temporary alterations in the local patterns of traffic circulation

The implementation process of DART (Phase 1) will require a series of provisional changes in the local standard of traffic distribution.

The change and/or strengthening of pavements will imply a partial interruption of traffic on the roads directly affected, compelling the temporary relocation of traffic within the DAA limits. In addition to the activities of demolition and paving on the BRT roads, the cross street traffic will be affected by the adjustments specified for the intersections. The project implementation will also compel traffic relocation on the Central Business District (CBD) road network, as per described in the "Programme for Traffic Management During Construction" (Section 6.5, P.05) and detailed in Annex 10.

As with the previous modifications, despite the intensity of changes in the patterns of road traffic, these changes shall be temporary, ceasing after the end of works. The exception is the CBD, where a new permanent scheme of traffic will be implemented at the operational phase (see Section 2.6).

7.03 Use of local street network by construction vehicles

During construction, there will be heavy duty vehicle traffic along the directly affected roads and in the rest of the network. It will be more intense on DART trunk roads because the local network lacks sufficient roads or expressways. Consequently, the intensity of impact 7.01 (increase in traffic congestions) will tend to rise.

This will also affect minor roads which connect the working faces to the support areas (send-off, borrow pit areas) and to the asphalt and concrete mixing plants. Nonetheless, in such cases, the impact should be less intense due to the lower traffic volume on those roads.

The usage of main roads and secondary routes by heavy duty vehicles will tend to decrease as demolition and paving works come to an end.

7.04 Local street pavement deterioration due to construction related traffic

The traffic of heavy vehicles on duty at the construction site will cause the wear and tear of the road pavements, especially those assigned to the implementation of DART Phase 1.

Such impact, also of a temporary nature, will be reverted after the end of works, given that new concrete pavements will be built on the segregated bus lanes, and the asphalt pavements will be rehabilitated / resurfaced. Nevertheless, the traffic of heavy vehicles on local roads may increase the deficiencies currently observed, since most part of the local road network, with the exception of the CBD roads and the major city links, is not paved and presents no drainage system.

7.05 Temporary disturbance of pedestrian circulation patterns

During the construction phase, there will be temporary interferences in the pedestrian circulation patterns, mostly on the roads directly affected and at the crossroads with the main local roads. Both the cross and parallel routes will be altered due to lane interruption, relocation of bus stops and the implementation of physical barriers (boarding, etc), as well as the implementation of provisional footbridges.

I.8 Potential impacts on population's quality of life:

8.01 Interference with local traffic during construction

During the construction works it will not be possible to divert the traffic on the avenues to other road sections, given the scarcity of alternative routes. Hence, in order to enable the occupation of part of the right-of-way by the construction teams without transferring the traffic to other sections, it will be necessary to stagger the relocation of traffic lanes.

This process, needless to say, shall cause a disruption to the users of the affected avenues, including freight vehicles, passenger vehicles and daladalas.

In the case of the Msimbazi Street branch, the disruption shall be particularly strong as a function of the relatively narrow right-of-way and of the continuous commercial activity along the link. Alternative roads will be inevitably used, affecting the conditions of traffic and noise therein.

A similar situation is expected along the Morogoro Road section within the CBD. In contrast, the problems expected on Kivukoni Road and Sokoine Drive, also in the central area, will be smaller, due to less intense traffic along these links.

Along the remaining parts of Morogoro Road the right-of-way is wider and the relocation should be possible with reduced impact on the road capacity during construction. On the other hand, on the Kawawa Road branch the available spaces are a little smaller.

8.02 Project induced social unrest during planning and construction

During the pre-construction stage the project disclosure may generate a substantial mobilization of the population employed or living in the areas directly affected by the roadworks.

Indeed, there will be concern and enquiries by the owners of properties to be expropriated, people living in zones prone to direct effect, owners and employees of shops and stores located along the corridor and other people whose activities and/or daily trips will be directly affected.

The mobilization of all these stakeholders, both for and against, shall extend until the construction stages, particularly the period of negotiation of indemnities and other complementary measures included in the Resettlement Action Plan for the present project.

8.03 Direct and indirect employment generation

It is expected that the works will last for twenty-four months with a workforce of roughly 450 during the peak season. This peak period is estimated to last approximately eight months. Out of the total staff, 70% will be direct and 30% indirect workforce. It is also estimated that up to 20% of the job positions may be filled by expatriate workers, coming from the country of origin of the company that wins the bid for the works.

In spite of the fact that the positive impact will cease after the end of works, it will provide a direct benefit to the families of all engaged workers.

8.04 Loss of employment in adjacent commercial activity during construction

The businesses facing the corridor shall be negatively affected during the construction, especially when the roadworks impose access difficulties.

If the reduction in their income level is significant, it may produce the dismissal of some workers. In the case interferences produced by the roadworks compel the temporary closure of a business, the dismissal of all workers will be a possibility.

The Socio-economic Census developed during the elaboration of the Resettlement Action Plan (RAP) Part A has identified a total of 70 to 75 workers in the business organizations to be partially or fully expropriated. It is estimated, based on the current status of the Socioeconomic Census, Part B, that the number of employees in the businesses directly affected in this section of the alignment will be around 40 to 50 (preliminary research).

It must be emphasized that this number does not include workers of businesses provisionally affected during the construction and therefore not subject to expropriation (that is, not covered by the Socio-economic Census). In fact, the number of workers that may be affected by this impact may be considerably higher.

8.05 Risk of construction related accidents

At the construction phase the activities that represent accident risks for workers at the contractors' service (occupational risks) are as follows:

- Work at high traffic volume areas (risk of pedestrian casualty);
- Vehicle driving (collision risk);
- Transportation, movement and handling of materials and inputs;

- Passenger transport;
- Transportation, loading and unloading of explosives;
- Transportation, loading and unloading of dangerous products (fuel, oil, etc.);
- Warehousing and handling of fuel and flammable materials;
- Operation of heavy machines and equipment;
- Demolition of pavements and structures;
- Excavation works;
- Concreting Works (shaping, reinforcement, concreting, form stripping);
- Works at high places (relocation of air utilities, construction of bridges);
- Tree cutting;

• Works with the risk of electric shock (operation of electrical appliances and equipment, relocation of power distribution posts).

Apart from the occupational risks, the construction will also bring about accident risks for third parties, namely drivers and pedestrians on the move at DAA, such as: pedestrian casualties, vehicle collisions, falling into pavement holes, electric shock and other associated risks.

8.06 Increased noise and vibration levels along construction fronts and routes to support areas

The population who lives or works in places next to areas directly affected by the project will experience an increase in the levels of noise and vibration during the construction.

This impact will be stronger during the initial stages of works, when the activities of demolition of pavements and structures, relocation of interferences and transportation of rubble as far as the send-off area will require the use of heavy equipment and vehicles. It must be noted, though, that the roads subject to the implementation of DART at phase 1 already present high traffic volume, so that the current levels of noise and vibration at DAA are already significant.

The intensity of this impact shall vary according to the degree of severity or sensitivity of those affected. In general terms, the most sensitive recipients to noise are schools, hospitals and residential areas.

Along Kawawa Road the existing schools are not located next to the road alignment, so that the impact shall not be significant. Otherwise, in the Morogoro Road section between Kawawa Road and Kagera Street there are around half a dozen schools and one hospital bordering on the road which may be affected by such activity. There is also a hospital very close to the area planned for the location of Ubungo Terminal.

The increase in the noise and vibration levels also tends to occur in the case of sections far from the urban area, which will be used by trucks for the transportation of materials between the support areas and the working faces. On the other hand, the amount of critical recipients will be, in such cases, substantially smaller.

8.07 Public utility service interruptions during construction

During the construction phase, some disturbances to the supply of some public services may occur, sometimes due to the need to completely relocate the services, and in other cases because of pavement improvement work or similar direct intervention where the services are located.

The companies whose utilities supply may be affected are: TTCL (public telephone), TANESCO (power supply), DAWASCO (water supply) and DAWASA (sewage).

Though services interruption cannot be avoided in some cases, the duration of interruptions is expected to be minimal since any relocation work will be completed before removal of the existing utility lines.

8.08 Expropriation

The necessities of expropriation along the corridor and complementary roadworks are distributed unevenly along the alignment. On the whole, the geometry of the corridor design requires expropriation, mainly at the stations, where the cross section needs to be wider.

The following table synthesizes the information on the need of expropriating for Parts A and B of DART Phase 1, specifying the cases of partial and full expropriation.

Table 6.4.1.aNumber of Affected Properties

Types of Property	Phase I – Part A (Final)	Phase I – Part B (Estimate)
Residences to be totally affected	9	69
Residences to be partially affected	5	0
Commercial establishments affected within residences	11	21
Totally affected independent commercial establishments	7	21
Partially affected independent commercial establishments	68	2
Other affected buildings	12	1
TOTAL AFFECTED PROPERTIES	112	114

It is expected that a total of 226 properties will be affected, out of which 75 will be partially affected. Nevertheless, the most affected types of use will be the businesses which develop in front of dwellings, facing the right-of-way

Further details on the necessary expropriations are found in the Resettlement Action Plan, Parts A and B.

8.09 Resettlement of families

As described in the previous impact (8.09), from the 226 properties to be expropriated, 138 will be thoroughly affected. Out of these, 110 present residential use.

In the case of the residential properties to be partially affected, it will be possible to relocate them within their same building lots, through indemnities for the construction of additional rooms in compensation for the areas to be demolished.

Concerning the incident legislation (Section 5.2), it is inferred that the dwellers to be resettled shall fit basically into the following types of situation:

• Owners-dwellers who occupy the land regularly, and who will receive a compensation which amounts to the market value of the lot and to the restitution cost of the construction, with the possibility to use it in order to acquire an alternative property.

• Dwellers on land irregularly occupied, whose compensation will be limited to the reconstruction costs only, subject to special assistance in the respective Resettlement Action Plan.

• Tenant-dwellers who shall be resettled and compensated for the loss of accommodation as provided by the Resettlement Action Plan.

I.9 Potential impacts on economical activities:

9.01 Displacement of economic activities along the corridor

The project implementation shall result in the displacement of some commercial activities to alternative places. The activities to be displaced may be divided in two major groups:

- Activities displaced owing to expropriation
- Activities displaced as a function of the indirect effects of the project

In the first case, based on the Census of properties for Part A (concluded) and Part B (in progress) it is estimated that the number of businesses to be fully expropriated will be around 60, with 18 businesses in Part A and 42 in Part B.

It should be considered, nonetheless, that some business owners whose useful area is reduced by the expropriation are likely to choose the relocation to more spacious places.

The displacement of economic activities by the indirect effects of the corridor implementation will happen in many different ways.

As observed in the land use survey of the Directly Affected Area (Section 4.4.1), many businesses bordering the right-of-way use the inner space of this latter. Among these, pubs and restaurants place tables and chairs on the pavement or in lateral streets, especially in shaded areas, as well as other establishments that use public space for merchandise exhibition or even as a product stock area. In this group are included furniture stores, carpentry workshops, building material stores, used car (on-street parking) shops, amongst others.

With corridor implementation, which will occupy most of the right of way, eliminating the possibility of informal occupation, it is probable that most of these establishment owners choose to move out to other road links.

Other economic activities that use the right of way include businesses that rely on kiosks or other permanent facilities which remain on the streets at night.

In this group are mostly sellers of tree seedlings and ornamental plants, which are concentrated in Kimara and in the surroundings of Ubungo Terminal. In addition to these, there are many street vendors scattered around commercial concentration spots along the corridor alignment. Their number is presently smaller since the municipality has decided to relocate this kind of seller from the streets of Dar es Salaam to specially designated markets.

Moreover, along the corridor there area many taxi ranks and daladala parking spaces spread over the empty spaces in the right of way. There are also some parking spaces for construction lorries where the population goes to hire transport services.

All these economic activities shall be displaced as a consequence of the physical occupation of the project right of way.

Lastly, it must be considered that, in some sections of the corridor, where the available road capacity for general traffic will be reduced and/or the on-street parking will be removed, the commercial establishments shall experience a reduction in the business turnover, and this may, in some cases, bring about the decision to move out. It is estimated that this risk is concentrated mainly along the Msimbazi Street branch.

9.02 Reduction of the intensity of commercial activity along the DART during construction

Owing to traffic interruption on DART network, the shops and stores facing the alignments may be affected by the project. In the most critical situations, difficulties in the accessibility may compromise the turnover of clients, implying a reduction of sales volumes and / or services, including a reduction in the staff of such business organizations.

Despite being a temporary impact, it will demand monitoring and specific programmes aimed at the support for the local business, during the construction phase.

I.10 Potential impacts on physical and social infrastructure:

10.01 Disturbance and/or relocation of public utilities

As outlined in the description of impact 8.08, there may be interferences on air and underground utility networks during the construction phase.

All interferences shall be identified and listed in a physical inventory during the executive project phase. The entrepreneur will contact the concessionaire companies responsible for planning the technical procedures related to the relocation and the interruption of supply to public services. The relocation should be adjusted to the construction schedule.

I.11 Potential impacts on public finances:

11.01 Appropriation of part of the public sector's investment capacity

The DART implementation will be funded as follows: 70% of the total investment will be financed by the World Bank, in the form of grants, and 30% by the government of the United Republic of Tanzania.

As a result of such sums of investment, the government will pledge part of its revenue to the public transport sector, reducing, therefore, the availability of resources for investment in other sectors, as for example, health, education, etc.

Although significant, the amount to be invested by the government will be small as compared to the total resources necessary for the implementation of Phase 1. It should be considered, still, the direct and indirect fiscal benefits which will derive, respectively, from the construction and operation of the BRT system.

11.02 Increase in tax revenue during corridor construction

The implementation of DART Phase 1 will demand an investment in the order of U\$ 110 million. Tax revenues will be generated from the construction contracts won by the building contractors.

Complementary tax revenues will come out of the local acquisition of building materials, goods and services.

I.12 Potential impacts on architectural monuments and cultural property:

12.01 Disturbance of architectonic landmarks and other cultural property

The construction phase will directly affect the access to some important buildings and cultural landmarks of Dar Es Salaam. The main buildings to be affected are the many religious buildings located along the entire corridor, but some institutional buildings that will also be directly affected during this period.

The significant areas and/or buildings to be affected during the construction phase are listed in the Section 6.3 (Impact Receptors - Component C.3.08).

Potential Impacts during Operation:

I.16 Potential impacts on road infrastructure and traffic:

16.01 Permanent alteration of street capacities and local patterns of traffic distribution

With the implementation of exclusive rapid bus lanes along the DART corridors and the rationalization of the public transport system, there will be changes in the street capacities and local patterns of traffic distribution in the DAI.

The ideal standard cross sections as per defined in the project result in total width of 46.5 m along the corridor and 55 m at bus stations. However, as mentioned before in Section 2.1.1, this space is not always available and several design compromises have been adopted along the corridor alignment.

At present, it is observed that in the section of Morogoro Road from Kawawa Road to Ubungo, where the implementation of the ideal standard section of DART is forecasted, the carriageway lacks no physical capacity. The main cause of congestion on this road are the traffic lights at the crossroads. As they operate with four phases, they work as bottlenecks, compromising the traffic flow. In order to solve this problem, the project recommends the readjustment of intersections through the replacement with two-phase traffic lights and the ban on right turns. In addition, it will be made geometrical adjustments at the intersections, and some barriers will be placed at the end of a few local streets, in a way to drive the traffic flow to U-turns schemes. The segregation of bus lanes will also benefit the traffic reorganization as a whole along this road.

On the other hand, the central section of Morogoro Road, between City Council Station and Bibititi Road, presents a very restricted right-of-way. For this reason, the traffic will be limited exclusively to buses, cyclists and pedestrians. In Section 2 of Morogoro Road, between Bibititi and the United Nations, despite being wider than in Section 1, the right-of-way is still narrow, resulting in a less generous standard-section.

The most critical situation in terms of road capacity is found on Msimbazi Street, a narrow road downtown, with intense business activity. In this case, parking along the kerb will be forbidden, with a view to freeing one lane for the BRT and another for the general traffic.

In other important roads, currently used by many daladala lines, the substitution for microbus feeder services and the readjustment of intersections, among other road improvements, will provide capacity gains.

Specifically at the CBD, there is a plan for the reorganization of traffic by means of the assignment of one-way traffic throughout the road network, presently used in both directions.

16.02 Reduction in the availability of external parking spaces

Implementation of the corridor will displace various types of informal uses of the selected rights-of-way. One of the most significant uses to be displaced are parking spaces.

On more densely occupied segments of the Central Business District, implementation of the BRT will reduce already scarce street parking spaces. This will be particularly critical along Msimbazi Street branch where the customers of many of the existing commercial establishments make intensive use of available front-of-shop parking spots.

Along Morogoro and Kawawa Road where the right-of-way is significantly wider, left-over spaces are frequently occupied by taxi spots, daladala resting areas or construction truck parking spots which are known by the population that hires these trucks for construction, moving or other services (see Section 4.4.1). These uses of the right-of-way will no longer be feasible and new parking locations will need to be found.

This impact will affect mostly commercial establishments that depend on availability of parking, taxi drivers, daladala operators and truck owners to be displaced, and customers / users of the services provided by them or by the affected commercial establishments. However, only in the case of affected commercial establishments can this impact be considered significant and long lasting. In the other cases it is mostly temporary and will cease as soon as adequate alternative locations are defined.

16.03 Permanent restriction to transversal flows of pedestrians

The implementation of the corridor will entail some changes to the pedestrians' transversal flow, mainly because of the streets cross-sections reconfiguration.

The construction of the segregated lanes, plus large stations and terminals along the road can create a visual and physical block, restricting pedestrian crossing in some areas. This can produce impacts mainly in areas of intense circulation of people such as Msimbazi Street, and some parts of the city center.

Along the Morogoro Road, this impact will not be major, since along the first stretch, from City Council to Bibititi Rd, the road will be one-level, and there will be no mixed traffic, and along the second stretch, from Bibititi Rd up to Kimara, the corridor already has the characteristics of a wide road.

Along Kawawa Rd, the existing pedestrian crossing will be reorganized, according to the new signalling and intersections design, and these impacts will occur mainly at stations and the terminal.

Another feature of pedestrian flow that will change is that, today, in many points of the Morogoro Road, the drainage channels are located so as to discipline street crossing. After the implementation of the project, many of these channels will be closed, in order to accommodate the cycling paths and larger sidewalks, and, although crossing will not be allowed, the possibility of crossing out of the recommended areas might increase.

At the main point of circulation, the project proposes the construction of three pedestrian overpasses, providing access to the terminals and feeder stations and also allowing crossing of the road. These overpasses will be accessed by ramps, complying with all the requisites of universal accessibility. The proposed overpasses are:

Ubungo – at the proposed Ubungo Terminal Kimara – at the proposed Kimara Terminal Morocco - at the proposed Morocco Terminal

At the other Terminals and Stations, the project calls for bumps, signalling and zebra crossings in order to discipline pedestrian flows.

16.04 Changes in traffic level of streets affected by "daladala" route rationalization

With the beginning of the BRT system operation, many of the existing daladala lines will become extinct, while others will be maintained or relocated, as per the proposed scheme for the rationalization of public transport. Such initiative will promote changes in the traffic volumes on the affected roads.

On the roads where the daladalas operate with considerable demand for passengers, the lines will be replaced by micro-bus feeders, vehicles whose capacity is larger than that of the former. On these roads, the impact on the traffic volumes will be positive, because there will be a reduction in the number of vehicles, and subsequently, less demand for the respective road capacity.

At the Central Business District there is a plan for a complete relocation of daladala lines presently in operation, with the extinction of some and route changes in others. In this case, some roads may experience an increase in the traffic volume due to the concentration of lines. Yet, this will happen only on roads whose capacity can hold such increase, without any damage to traffic flow.

On the remaining road network in town where daladala lines move around with low passenger demand, these shall be preserved.

16.05 Changes in traffic level of streets affected by permanent re-routing of passenger vehicles

The reorganization of general traffic in the CBD streets shall affect the levels of road capacity. In this context, the major impact will happen on Morogoro Road between Sokoine Drive and Bibititi Road, where the traffic of passenger vehicles will be removed.

Notwithstanding, in most of the downtown road network there is a plan for the conversion of lanes into one-way traffic, completely changing the patterns of access and exit. Whilst this shall bring about an increase in the flow of some roads and decrease in others, it is a minor impact, given that the traffic in the central region is already quite heavy and this condition shall not be changed in any case.

16.06 Increase in the level of bicycle use

One of the objectives of the DART system project is to promote non-motorized transport (bicycles and pedestrians).

Almost all standard sections specified in the project will have exclusive cycle lanes, excepting Msimbazi Street and the section of Morogoro Road between the City Council Station and Bibititi Road, where cyclists will be able to move freely on bus lanes.

The promotion of NMTs will constitute a positive impact of the Project, bringing indirect benefits to the general traffic.

16.07 Increase of the barrier effect along the corridor

With the implementation of DART, drivers and pedestrians will face difficulties in crossing the roads where segregated bus lanes are fitted.

The main crossroads will be subject to geometric and traffic light adjustments, in order to eliminate bottlenecks. Some barriers will be put at the end of a few local streets parallel to the corridors, in a way to drive the traffic flow to U-turns and enable crossings only at signalized intersections.

From the viewpoint of pedestrian safety on casualties, the restrictions on crossing and accessibility to BRT from some specific stops with high demand represent a positive impact in relation to the current situation. Alternatively, from the point of view of car accessibility they represent a negative impact, whose intensity, nevertheless, tends to decrease as the motorized population familiarizes itself with the new traffic schemes.

I.17 Potential impacts on public transportation:

17.01 Improvement in the quality and capacity of public mass transport serving the IAI population

This is, concurrently, the main objective and main positive impact of the DART operation.

The implementation of a special physical infrastructure, formed by corridors, stations and terminals aimed at the segregation of many different types of vehicles and to transport hierarchy, together with the concession of the DART operation, which includes the replacement of the current "daladalas" by a brand new circulating fleet (articulated buses on trunk lines and micro-buses in feeder services), will result in system rationalization and will increase the capacity of passenger transport.

The final results expected with the implementation and operation of such a system are the increase in users' mobility, comfort and safety.

17.02 Re-routing and rationalization of "daladala" lines

The rationalization and the change of the remaining daladala routes will not imply a smaller coverage of the public transport service. As analysed in Section 2.4, the level of service of public transport will not be reduced in any of the city's regions; much to the contrary, it will be expanded in many cases.

Without any service loss, the rationalization of routes shall imply an increase in some users' trip to the nearest line. Likewise, in many cases the rationalization will result in a reduction in the users' walking distance.

Another effect of the rationalization of the remaining daladala lines will be the improvement in the reliability and quality of services, given that, with a smaller amount of lines, frequencies can be reduced.

17.03 Reduction of total bus / kilometers in the IAI

The implementation of the first phase of DART Project will result in a significant reduction in the total number of buses / kilometre covered in the city of Dar es Salaam. On average, the articulated buses on the corridor will cover 44,532 km / day, with a passenger-kilometre index (PKI) of 9,4. Moreover, the micro-buses of the feeder service will cover 51,211 km / day on average, with a PKI of 5,5.

The articulated buses will travel at an average speed of 23 km/h, and the micro-buses in feeder lines at 17 km/h, which make up a total of 4,750 operating hours / day for the whole fleet of the concessionaire operator.

On the other hand, 33 daladala lines will be shut down. In these lines, nearly 460,000 passengers are transported per day at an average speed of 17 km / hour with a passenger / km index (PKI) of 1.2 Therefore, the elimination of these 33 lines will result in a reduction of 26,600hours / day of daladalas in operation.

Apart from that, in the remaining daladala lines, the total number of passengers / day to be transported will drop to 1,170,000. Assuming that the current average speeds will maintain at 17 km/h with an improvement near of 28% in the PKI as a consequence of the rationalization of such service, it may be estimated that the operation of these lines will result in 56,300 hours / day of daladalas in operation.

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From the above it is possible to gather that with the operation of DART Phase 1, the total number of bus hours / day in operation (including articulated buses, feeder micro-buses, daladalas and the remaining modes) will be of 61,100 hours. Comparatively, it is estimated that the present fleet of 5,800 daladalas operates on average 82,900 hours / day.

Consequently, there will be a significant reduction, of nearly 22,000 in the total amount of hours / bus / day in operation in Dar es Salaam.

Similarly, the total amount of buses / km covered per day shall drop from 1,408,000 km/day (exclusively daladalas) to 1,133,000 km / day (considering articulated buses, feeder micro-buses and daladalas, altogether).

The reductions in time and kilometers covered by buses in town will bring about benefits proportional to the traffic flow.

17.04 Reduction of operational costs per passenger / kilometer in public transport along the DART corridor

The overall reorganization of Dar es Salaam's public transport system will provide a substantial increase in the passenger transported / kilometer index (PKI), in addition to the use of modern vehicles with lower operating costs and the rationalization of routes and itineraries.

Such improvements will result in a large reduction in the costs of public transport services per passenger / kilometer in town, assuring, in the medium and long terms, more economical fares to users.

I.18 Potential impacts on urban structure:

18.01 Indution to changes in land use patterns within DAI

In general terms, the mass transport system offers potential for increasing the population density in the area within the influence range of stations (from 300 to 500 m). The increase in accessibility may cause changes in the patterns of land use and occupation in the areas bordering on or next to the corridors.

However, based on the analysis of the urban structure along the DART corridors at Phase1, the areas at DAÍ where the project would have the greatest potential for promoting changes in the patterns of land use and occupation are those next to the stations planned for the Morogoro Road section along Ubungo e Kimara, where the use, predominantly residential, sprawls in a disperse pattern, among empty spaces. In this section the DART operation may

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cause the reduction of empty spaces for residential purposes and an increase in the density of built-up areas.

The section between Ubungo and Urafiki is defined by the existence of industries and popular dwellings. It is unlikely that the BRT implementation will cause changes in these already consolidated occupation patterns.

In the section between Morogoro Road between Magomeni and Urafiki (entrance to Mabibo), the project implementation may cause the eviction of people that use the ROW for business and services. The shops and stores existing along this section also use Row for the display of their products, and shall lose this space. There is a potential for vertical development in this section, since the buildings lack the structures or slabs that enable easy building of one or more floors. Quite likely the business owners will promote improvements in the buildings, or possibly sell their outlets. At any rate, there would be no significant impacts on the urban structure.

In the Morogoro Road section between the Msimbazi River and Magomeni there is a steep grade separation between the road and the nearby residential areas, located at a higher level. Even if the carriageway is widened, this configuration will not be changed by the project, so that the gain of accessibility in this section will be most unlikely to cause changes in land use and occupation of the neighboring areas. In addition to the barrier effect, these areas are quite close to the city centre, with consolidated residential use and medium standard constructions in planned plots, different from the residential occupation in the section between Magomeni and Urafiki (entrance to Mabibo), with spontaneous and irregular plots and low standard construction dwellings.

On Kawawa Road between River Sinza and the area neighbouring Kinondoni Road, the border use is predominantly commercial, and the residential occupation along the DAÍ is similar to that in the section of Morogoro Road between River Msimbazi and Magomeni. Between Kinondoni and the Ali Hassam Road neighbourhood there is a vague definition as to the bordering uses, owing to the fact that in recent times Kawawa Road was subject to improvements which resulted in some "open wounds" in the urban fabric, causing land occupation with some institutional uses. It is possible that the DART implementation may promote the consolidation of business and institutional uses, without changing; nonetheless, the residential pattern of DAÍ's remaining areas.

In the central area it is unlikely that the project will affect the patterns of land use and occupation, since DART will pass through consolidated, chiefly mixed use areas with vertical development. At most there might be some marginal changes in Kaukauna Front area, with the removal of small outlets along the waterfront. The adjacent areas, featuring institutional, commercial and port uses (ferry-boat), will remain as they are. Notwithstanding, it must be observed that the project implementation may serve as an inducement factor in the urban and landscaping improvements to this section.

18.02 Increase in real estate values

The most important contributing factor in the definition of real estate values in cities with market economy is location.

Well located areas are those near tertiary sector centers or sub-centers, where jobs and urban facilities are concentrated (infrastructure and services in general). These areas are occupied by upper class districts. On the other hand, those areas far from these attractive sites and which are badly served by urban infrastructure are the least valued. As a rule, the further and less supplied an area is, the smaller the property prices are, and therefore, the smaller that resident population's income is.

The implementation of road improvements or the creation of mass transport lines tends, in general, to induce an increase in the property value in the adjacent or nearby areas, owing to an increase in accessibility. This may happen not only in nearby areas but also far from the attractive sites. Road projects that favor individual transport tend to encourage elitism or gentrification, that is, the substitution of families traditionally resident in the areas adjacent to the road works for families with higher incomes or automobile users.

However, in the case of mass transport projects based on buses, the possibility of two different types of increase in property value must be considered:

• The increase in the value of properties immediately adjacent to bus ways, for nonsophisticated, tertiary sector uses (traditional street trading); the shops, stores and services which depend on the turnover of customers, are likewise, less sensitive to the environmental impacts resulting from the changes implemented.

• The increase in the value of properties located in the area directly influenced by the corridor, for residential uses of low and medium standards, as a result of the availability of easy access of users to the transport system.

It should be highlighted that the implementation of bus ways takes place generally on roads that already have commercial corridor features. In such cases, the impact on the value will always be limited, in comparison to the creation of a new bus line for a distant, low-value area, where unused land still exists. Such event may cause a real "leap" in the property values of the affected area.

In the case of the current project, an increase in the property values along all the road links, as per the described trends, is expected. However, in the central area the potential for an increase shall be rather limited, owing to the consolidated occupation. The DART section where the increase in value will be more substantial is on Morogoro Road between Ubungo and Kimara, further from the city center, and presently less accessible.

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18.03 Risk of localized real estate depreciation

This potential impact may be local, in those areas the environmental impact of the BRT system operation are noticed with greater intensity.

The increase in noise levels and the emission of atmospheric pollutants at the terminal and station areas, where a concentration of vehicles and people is expected, may cause a depreciation of some adjacent residential properties.

Aside from that, the implementation of road and landscape improvements and new architectural structures (terminals, stations) along the corridors will tend to minimize or even reverse property depreciation.

18.04 Potential for increased urban density within the DAI

Among the likely results of an increase in population density due to the operation of DART Phase 1 is the occupation of fields and empty lots, on the one hand, and vertical development, on the other. These are both forms of intensification of land use value; nonetheless, whereas the first process implies a concentration of the constructed area, whereas, the second implies the intensification of land occupation potential.

In theory, the increase in density, either through the concentration of the constructed area or through vertical development will have a positive impact. Both provide maximum utilization or social usufruct of the infrastructure and available services along the transport links, contributing to the economy of public resources. The reverse of this process is the centrifugal urban sprawl which promotes an expansion of the urbanized area and subsequently, the necessity to expand the network of public services (see impact 18.04).

In the specific case of the current project, the areas with a stronger probability of an increase in vertical development density are the business and residential areas bordering on Morogoro (as far as Ubungo Terminal) and Kawawa (as far as Ali Hassam Road) Roads. The currently empty areas along Morogoro Road between Kawawa Road and Kimara may experience an increase in land value and land fragmentation.

18.05 Increased potential for radial expansion of the urbanized area

Another likely effect of roadworks and the implementation of the BRT systems is an increase in the potential for centrifugal urban sprawl. This inducement potential is a natural consequence of an increase in accessibility to certain areas in town. With the reduction of travel times in the downtown-uptown radial trips, the relative distances decrease, in a way that new areas, formerly relegated to a second plan by the market, become part of processes such as land fragmentation and increase in property value.

In large cities of third world countries, where historical urban structures have taken on an essentially radial organizational configuration of the downtown-outskirts type, the implementation of the BRT systems has no power to change the urban reality defined by market rules. This is determined by the fact that the poorest population lives, as a rule, in districts where real estate and rents are cheaper, far from the central business district. The so-called 'infinite growth of urbanized area' model, although capable of providing the poor with conditions of access to housing (regardless of the quality of human settlements), has proven to be costly to public funds, because it requires the continuous expansion of networks and public services and demands high maintenance costs.

Differential increases in accessibility may worsen this trend, encouraging a rise in property prices and rents in some of the districts now served by more efficient transport systems. This could result in the eviction of those families who cannot afford the new housing costs, to places even farther or more peripheral to the city centre.

As already mentioned, with the implementation of DART Phase 1, such impact along Morogoro Road link, between Kawawa and Kimara, where there are still many empty spaces may occur.

Such effects can only be compensated by social and urban policies complementary to transport sector policies which allow families to stay put, or else policies that offer low cost dwelling units in locations, fairly served by transport, services and jobs.

In this context, the development of new tertiary sector sub-centers is a policy recommended to reduce the population's functional dependence on the jobs offered in the traditional CBD. Such a measure, though, cannot be exempt from the previously mentioned policies, since the development of sub-centers can also trigger an increase in property values and the eviction of lower income families.

18.06 Development of new urban sub-centers or expansion of existing ones

The corridor implementation can encourage significant urban and economic development along Morogoro and Kawawa Roads and Downtown, creating new hubs and triggering the development of other areas. The urban landscape can be improved by the application of other concepts of urban space use, since the corridor will promote changes predominantly in its area of influence.

The implementation of Phase I of the corridor makes possible the development of certain areas, inducing the creation of leisure and social activity facilities. Seven (07) important urban hubs along the proposed First Phase DART System that can be enhanced by its implementation can be identified:

Hub 1 - Kivukoni Front, including the Fish Market and the Water Front. The impact of the implementation of the Kivukoni Terminal, near the Fish Market, and the new landscape and reconfiguration of sidewalks along the waterfront, can induce new investments along this area, which has great potential for leisure and tourist activities, due to the existence of major hotels and the proximity of the port.

Hub 2 – Central Business District, including City Council, Samora Avenue, and Libya Square – The implementation of DART can bring new investments to these areas and the reorganization of its business characteristics will require also the reorganization of public space.

Hub 3 - Kariakoo - Kariakoo area is a market activity area. The Terminal will be located on the extreme south of this area and can serve to organize public areas, integrating the activities that were displaced by the removal of the street vendors that were occupying the area.

Hub 4 – Magomeni - The intersection of Kawawa and Morogoro is one of the major road network nodes in the City. With the implementation and expansion of the System, this importance will increase, and the area will become a significant hub for the whole city. This is also true of Magomeni Market, which can be enhanced thanks to its proximity to the Magomeni Mapipa Station.

Hub 5 – Morocco - The Morocco Terminal is an major transfer point of the DART System. For this reason, it may become an attractive location for businesses, especially for services and small shops.

Hub 6 – Ubungo – This is an important point for the transportation system in Dar Es Salaam. It is strategically located near the junction with Nelson Mandela/Sam Nujoma Rd and it encompasses the present Upcountry Terminal. This is a potential area for the development of mixed activities, and can be converted into a focal point of the city.

Hub 7 -Kimara – Due to the implementation of a Terminal, this area can be enhanced by the creation of new businesses and the development of existing ones.

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I.19 Potential quality of life impacts:

19.01 Reduced travel time for corridor users

One of the main benefits of the implementation of DART system will be the reduction in travel times. This will arise mostly from:

Implementation of a suitable road infrastructure, which will enable the segregation of different types of traffic (including cycle lanes), and intersection adjustments, mostly for the reduction in waiting time at traffic lights;

Construction of terminals and stations for transfers, and the implementation of a integrated fare system;

The valorization of a public transport system, with the use of high capacity buses in the trunk lines and medium capacity micro-buses in the feeder services (feeders), as well as the organization of the daladala lines;

An incentive to non-motorized transport systems (NMTs), such as bicycles.

The first two improvements will enable an increase in the mean speed of buses on the project roads to an average of 23 km/h. The second two aim at facilitating / speeding up transfers in the trips between different transport modes (walking, bicycles, daladalas, micro-buses and articulated buses).

As compared to the present situation, characterized by the low of capacity of the daladala systems to respond to the rising demand, the implementation and operation of the DART system will provide a significant reduction in the downtown-uptown travel times, benefiting not only public transport users, but also the motorized population due to traffic segregation and the bus stops.

Simulations show that the current daladala travel time amounts to 4,269,917 minutes. The DART system operating at the morning peak hour, by articulated bus will be 3,910,685 minutes, which is equivalent to time savings of 2.6 minutes per trip, or 98,840,119 minutes per month. As to the overall time, which includes in-vehicle, fare payment, walking to the station and waiting time, the average economy per trip will amount to 1.3 minutes per trip, equivalent to 49,242,000 minutes per month, for the whole system.

19.02 Alteration of total cost of travel for corridor and feeder line users

The management of the public transport system in Dar es Salaam will be granted as a concession to a private company, whose revenue shall derive from the collection of fares.

It is therefore expected that the operation of a modern and more efficient transport system will necessitate ticket prices higher than the fares currently charged by the daladala operators. Nevertheless, this price will have to be based on the average users' income possibilities, so that the running of the service becomes economically feasible.

Moreover, as described above, there is plan for the adoption of an integrated system between the trunk lines and feeder services in order to establish a unified system of fares at terminals and stations. The purpose is to provide a reduction passenger time at transfers and an overall economy of total travel time.

The beginning, the DART operation shall gradually introduce the population to the gains in the cost-benefit relationship, reducing the likely impact of the initial rise in transport fares. The overall consideration being that the increase in users' mobility represents, in theory, more available time for the practice of other profitable activities.

19.03 Reduction of total direct employment necessary for operation of the public transportation system of the IAI

The modernization and reorganization of the public transport system in Dar es Salaam will constitute a social negative impact in the reduction of operational people employed.

The current demand data shows that the daladala fleet in operation amounts to 5,800 vehicles, with a total of 11,600 employees (drivers and conductors) per shift. Taking into account a two-shift operation, with one driver and one conductor per shift, the total number of employees amounts to 23,200.

After the elimination and relocation of lines, the remaining daladala fleet will drop to 4,380 (including the spare fleet). Considering a two-shift operation, with one driver and one conductor per shift, the following shall prevail:

- 1,420 vehicles will be taken out of operation;
- 5,680 drivers and conductors will loose their jobs.

According to data shown in Section 2.7, it is estimated that DART Phase I operation will generate a total of approximately 2.100 jobs. A significant part of these jobs may be filled by daladala drivers and fare collectors. However, many will need to seek other employment as a result of DART Phase I implementation. A specific program will seek to mitigate this impact ("Programme for the Compensation of People Involved in Daladala Operation" - Section 6.5, P.13) by assisting affected drivers and fare collectors with training and other income restoration strategies.

19.04 Increase in the potential for employment generation in the DAI as a result of increased density

The increase of land occupation density, caused by the operation of the DART system, may bring about a rise in the potential for job generation. This is a potential indirect impact arising out of impact 8.04, as the population increase at DAI tends to boost business and services, benefiting the urban economy as a whole. The simple fact that there are more people living or moving around DAI represents a potential economic advantage and rise in salaries, which can contribute, accordingly, to the improvement in the quality of life of the population directly affected.

19.05 Disturbances to local population as a result of commercial activity displacement

Local consumption patterns will have to be altered as a result of the displacement of a significant amount of commercial activities, particularly along certain segments of the DART Phase I alignments. Consumers who will have to alter their consumptions patterns will find new alternative suppliers of goods and services and will need to adapt to a significantly altered distribution of commercial activity within some neighborhoods.

This impact will be more intense during construction and initial phases of project operation. However, it can be considered an impact of lesser importance in the global context, since alternative suppliers of goods and services will be rapidly selected by consumers in most cases.

19.06 Local alteration of noise level during operation

During the operation of the DART system there will be changes in noise levels along the corridors and on DAÍ local roads.

On the corridors and at the intersections the traffic-light adjustments (decrease in the number of phases) will reduce congestions, so that vehicles will travel at mean speeds higher than the current ones. As motors turn for longer periods at higher rotation bands, the levels of noise emission will tend to decrease.

In some sections of DART, such as on Msimbazi Street, there will be no significant change in the noise levels, since there are several daladala lines circulating there. Despite the substitution of such services by high capacity buses, with less frequency and fewer stops, the latter will continue to produce higher noise levels.

On the remaining DAI roads changes in the noise levels will result from the relocation of general traffic and daladala lines. Local residential streets, presently with little traffic, may be affected by an increase in vehicles, and in turn, the noise levels. On the other hand, roads with currently intense daladala traffic may benefit from the reduction or extinction of lines, as well as from the introduction of micro-bus lines (feeder services).

Broadly speaking, the most sensitive recipients to vehicle noise are schools, hospitals and residential areas quoted in the description of impact 8.07. As the ROW along Morogoro Road is wide, the landscape project shall include green areas aimed at reducing the level of noise. A similar measure may be applied to the area surrounding the Ubungo Terminal and the existing hospital.

19.07 Landscape alteration and urban requalification of areas adjacent to the corridor

After the end of the execution of DART Phase 1 and the implementation of the landscape project, the configuration of road links directly affected will have their landscape positively changed to the betterment of the general urban aspect of the area.

Such modifications will include the restoration of pavements, fitting of new structures (stations, terminals), cycle lanes and new urban fixtures, as well as an increase in trees and urban gardens. These improvements tend, in turn, to encourage the revalorization of public areas and buildings, currently run-down or degraded by the intense vehicle and pedestrian traffic.

On the other hand, it is possible that some open spaces planted with trees and currently used by the population as leisure areas may be eliminated or partially affected by the widening of the carriage sections.

A project for a high capacity transport system cannot be seen as a sector project. Indeed, urban plans and schemes shall be ordered by the City Council so as to intensify the project's positive aspects and to avoid negative ones.

19.08 Alteration in the number of traffic acidents

Some previous experiences of corridor implementation have shown that there is the possibility of an increase in the number of accidents involving pedestrians, buses and mixed traffic, at least in the first months after implementation. This is due to the fact that both pedestrians and drivers have to get used to the new configuration of traffic speed and signalling, especially in the cases of buses, which will operate in segregated lanes.

Such an impact, however, can be considered to be provisional and it is expected that after an initial adjustment period the level of accidents will decrease below the present rate.

I.20 Potential impacts on economic activities:

20.01 Induced change in commercial and service activities along the corridor

The start of the operation of the DART system tends, in theory, to increase the value of the areas bordering on the roads affected, for tertiary sector uses. The infrastructure, road and landscape improvements may constitute incentives, for the owners of business facilities along the corridor, to renovate and/or expand their buildings, or to transfer their outlets to other parties interested in the investment.

20.02 Increased potential for retail shops and service providers in the areas surrounding stations and terminals

Due to the concentration of people, the areas within the range of accessibility to stations and terminals (from 300 to 500 m) may become particularly interesting for tertiary sector uses.

Notwithstanding, the stations and terminals specified in the project are already located in places meant for public transport users. They contain daladalas stops and concentration of people, business and service activities. Furthermore, as the project specifies the construction of business and service outlets within the terminals, part of the potential for the development of new business facilities will be absorbed by this supply.

As a result of these aspects, major changes, such as significant changes to the commercial pattern or changes to the types of activities existing in the surroundings of stations and terminals are not expected. The possibility of substitution of residential uses by commercial uses in some real estate units should not, however be excluded.

20.03 Potential income loss for daladala operators

The implementation of the DART system is concerned with the rationalization of the whole public transport system currently under operation, including the elimination of several daladala lines. One potential negative impact of the project is the loss of jobs and sources of income for drivers and conductors.

Presently the daladalas perform 41,100,000 trips per month. Once the DART system begins to operate, the articulated buses and feeders will perform 11,200,000 trips, reducing the amount of daladala trips to 30,000,000 per month (reduction around 28%). This reduction will cause the elimination of approximately 5,680 jobs, as per described in Impact 19.03 description.

I.21 Potential impacts on physical and social infrastructure:

21.01 Increase in the serviced area of key components of local social infrastructure

The operation of the BRT system will increase the users' mobility and there will be an indirect expansion of the area covered by some social services existing at DAÍ. In other words, it will be easier and faster for people living in areas previously very far from social services to now have easy access to such facilities.

Most of the social services of interest for the system users (schools, religious buildings and hospitals) are located in the central area, in the surroundings of Kivukoni Front and in two sections of Morogoro Road: between Bibititi Road and River Msimbazi, and between Kawawa Road and Kagera Street. There are also many schools and some religious buildings located at DAI along Kawawa Road.

I.22 Potential impacts on public finances:

22.01 Indirect increases in property tax revenue as a result of real estate value increases and intensification of economic activity along the corridor

An indirect effect of the implementation of the DART system, is the expected increase in property values and the boost to economic activities at DAÍ, mostly in the areas surrounding the terminals and stations. This process tends to promote a gradual increase in the tax revenues on real estate, supported by the Dar es Salaam City Council.

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ANNEX 06

Social Impact Assessment included in the ESIA

ANNEX 06

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Rap Disclosure and Consultation Meetings

Date	Event
31/08/2007	 RAP disclosure in News papers Compensation Tables have been displayed at the DART Office and the two Municipalities (Ilala and Kinondoni)
	 RAP Reports are accessible to the general public in the DART Office, TANROADS and the two Municipalities.
18/10/2007	 A meeting was held with the Authorized Land Officers, Land Valuers, Engineers, Social Workers from the three Municipalities and Ward Executive Officers (WEO) from 17 Wards with affected properties (12 wards - Kinondoni and 5 Wards - Ilala) to discuss emerging issues after disclosure. Although there are no affected properties in the Municipality, Temeke Municipal officials were invited for awareness and capacity building.
	 It was reported that affected persons with an exception of the Kariakoo Terminal site are very cooperative but requests the government to pay their compensation early so that they can start establishment in new areas.
	 Meeting deliberations necessitated the formation of two task forces to address pressing issues and report back within seven days. The two task forces are: Gerezani resettlement procedure Procedures for paying compensation
24/10/2007	 Authorized Land Officers, Land Valuers, Engineers, Social Workers from the three Municipalities and Ward Executive Officers (WEO) from 17 Wards with affected properties (12 wards - Kinondoni and 5 Wards - Ilala) met to receive "Task Force Reports". Temeke Municipal officials were invited again for capacity building.
	 The two Task Forces formed on October 18th, 2007 reported on the findings. Payment procedures were presented and adopted.

Event

• For security reasons payment will be based on bank transfers, an approach which is also favored by the majority of the affected.

Date

- With regard to Kariakoo it was confirmed that all houses at Kariakoo Terminal site are built on government land which changed use from Residential to Terminal in 2002 after a consultative process of preparing "The Kariakoo New Redevelopment Scheme".
- The Title Deed holder is Tanzania Building Agency (TBA) which inherited the properties from Tanzania railways authority and Tanzania Harbors Authority which have changed mode of operation (privatized).
- Procedures are advanced to transfer the title deed from TBA to the DART Agency. DART Agency will pay compensation of properties and all other benefits as per World Bank Guidelines to TBA who will thereafter pay to their tenants who purchased the house after deducting the less the amount paid through tenant purchase scheme.
- TBA pledged cooperation on the timely removal of the properties in a smooth operation.
- After the meeting, a Grievance Committee was composed of the following core team members: Authorized Land Officer, Municipal Valuer, Municipal Engineer, Municipal Social Worker and DART Resettlement Specialist. Engineers from DART, Ward Executive Officers and other relevant officials will be co-opted depending on the issues raised by the affected persons.
 - Taking into consideration that DART is a project with indirect benefits compared to "CIUP", Grievances Committee are formed at Municipal level in the two Municipalities for efficiency and effectiveness.
- 29/10/2007 Under the leadership of the Ilala District Commissioner (a presidential appointee in charge of the Ilala District) a meeting was held to discuss how the TBA will administer compensations in a fair and transparent way. The TBA representative explained clearly on how property rights will be accorded to the occupiers. The meeting was well represented, including the Councilor Mayor, Authorized Land Officer, Municipal Valuer and an assistant representing the Municipal Director.

Date	Event
30/10/2007	 A Meeting with the Kariakoo terminal site residents chaired by the District Commissioner was held. Occupiers of TBA houses were informed on the mode of payment of resettlement values.
	 The DART agency will transfer compensation money to TBA to enable them to pay the occupiers immediately after the transfer of certificates is concluded.
03/11/2007	 A Meeting to be attended by all affected persons to get clarification on compensation procedures and payment procedures. The Kinondoni District Commissioner will head the deliberations. The Municipal Mayor, Councilors from the 12 affected wards and Ward executive Officers will attend.
	 The MEDIA people have also been invited to attend. Through the meeting and media coverage we expect people will understand their rights and procures.
10/11/2007	 A similar meeting to the above meeting of 03/11 will be held on Saturday 10th November.

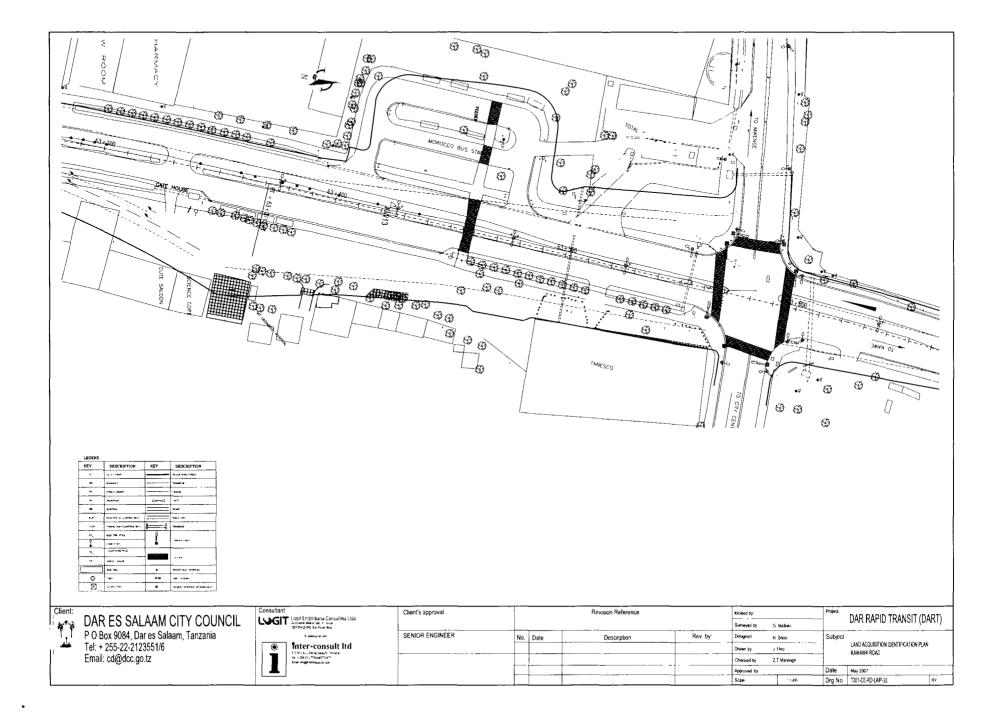
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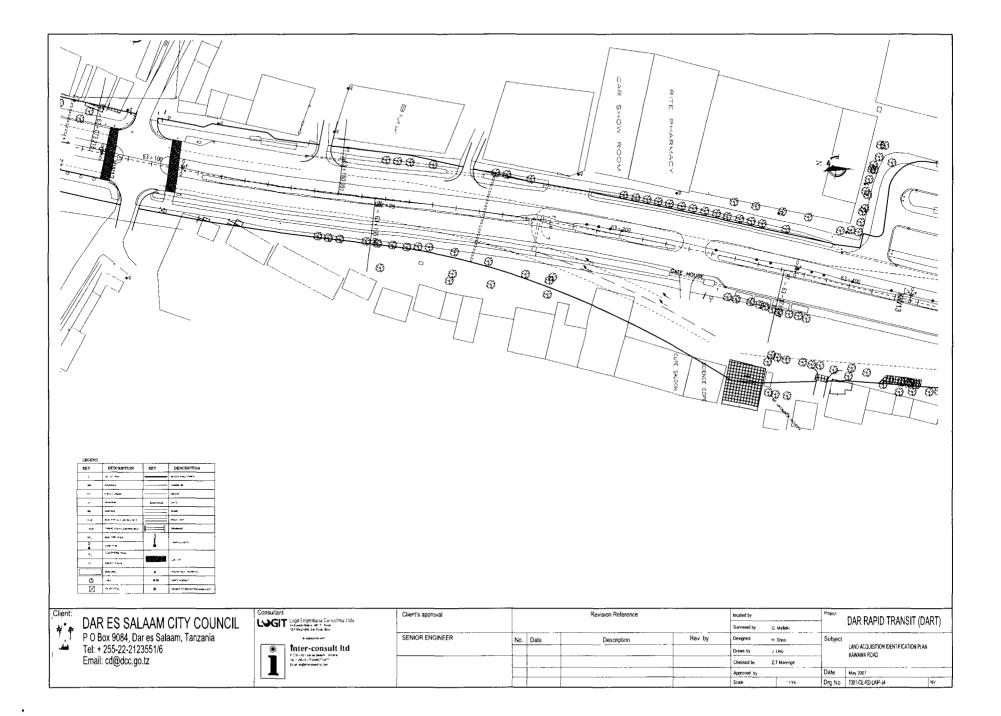
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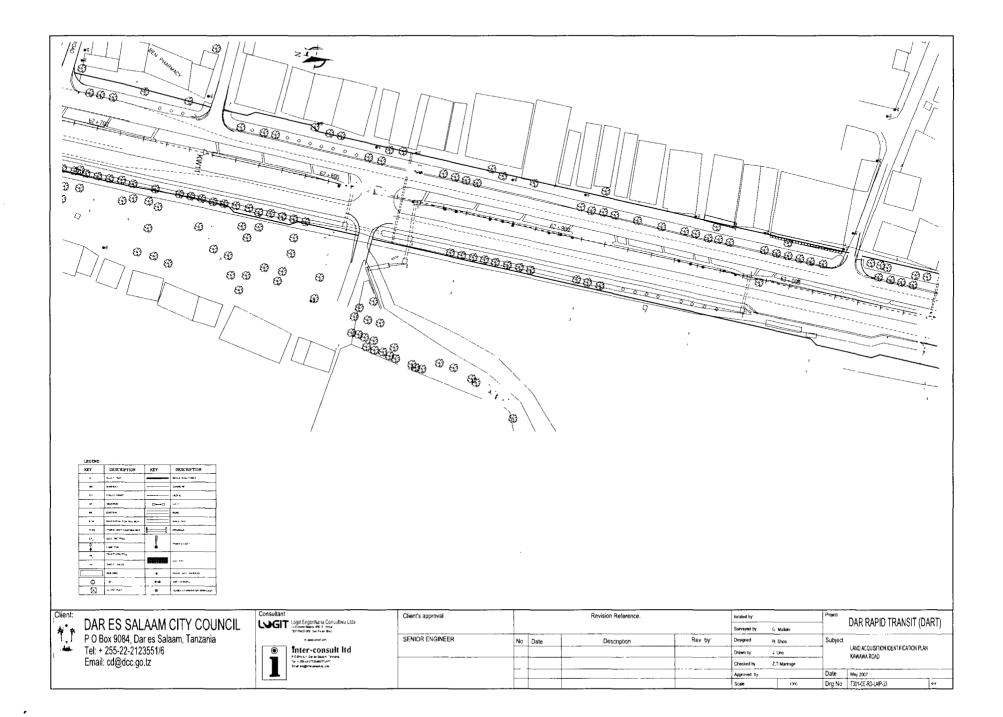
1. The last two meetings intends to explain the procedures but also prepare the affected persons to cope with the changes.

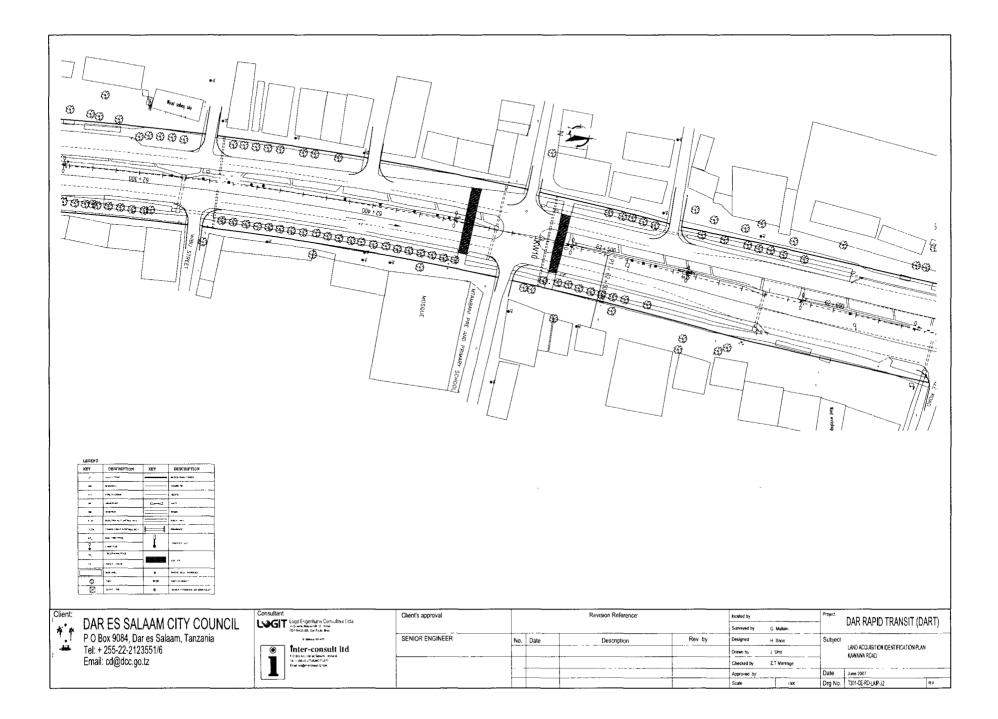
2. After the two meetings MEDIA communication will continue to clear doubts. If necessary another meeting will be held if it is very necessary to do so.

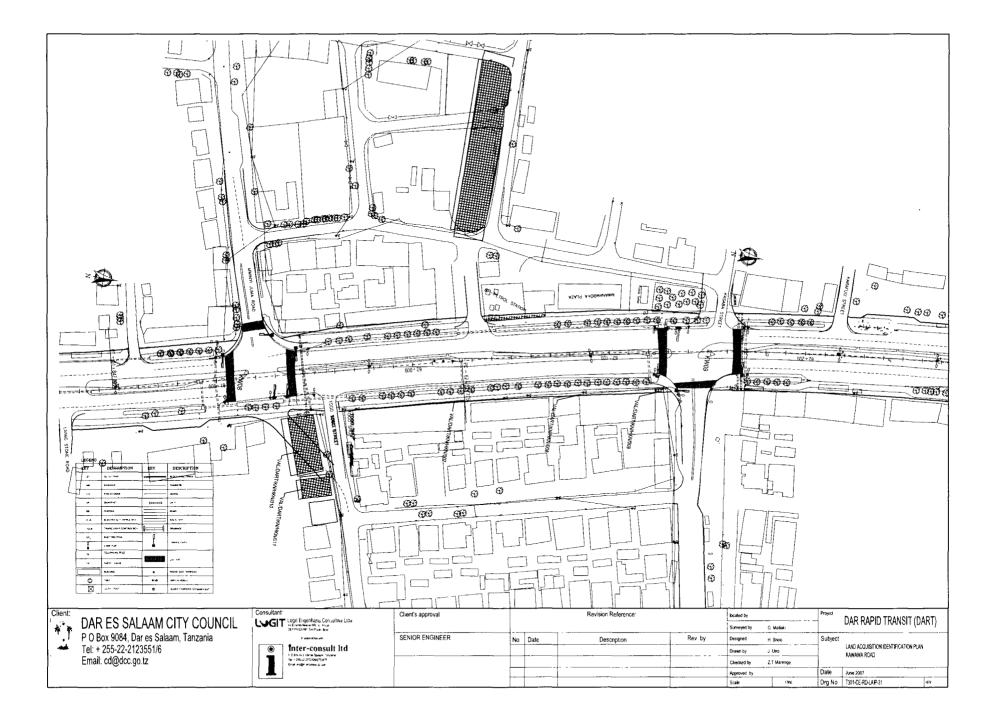
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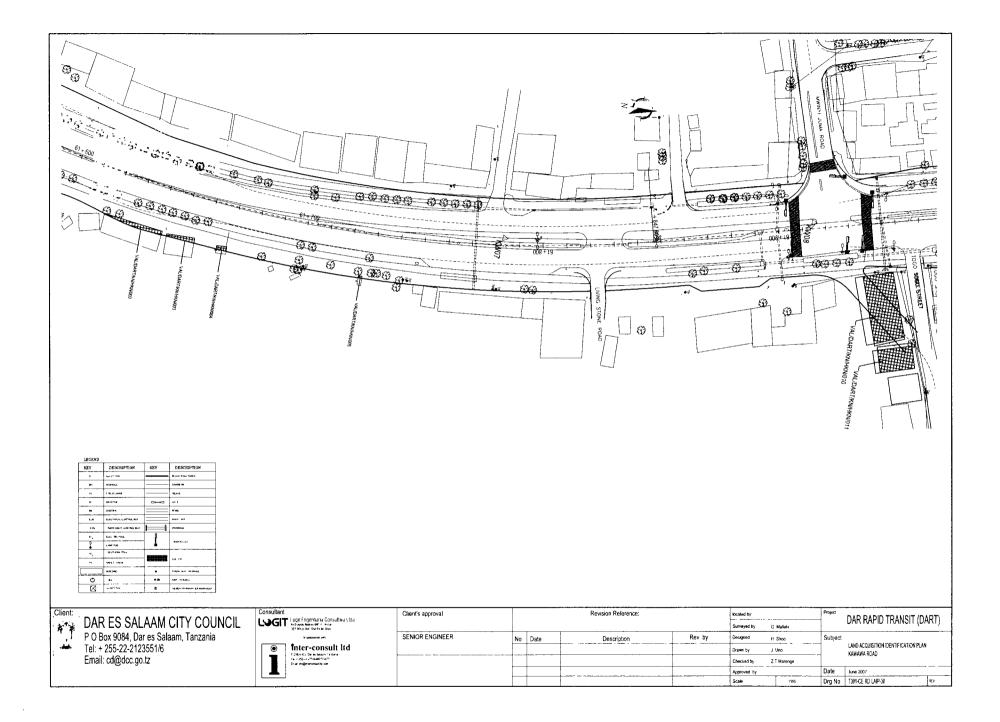


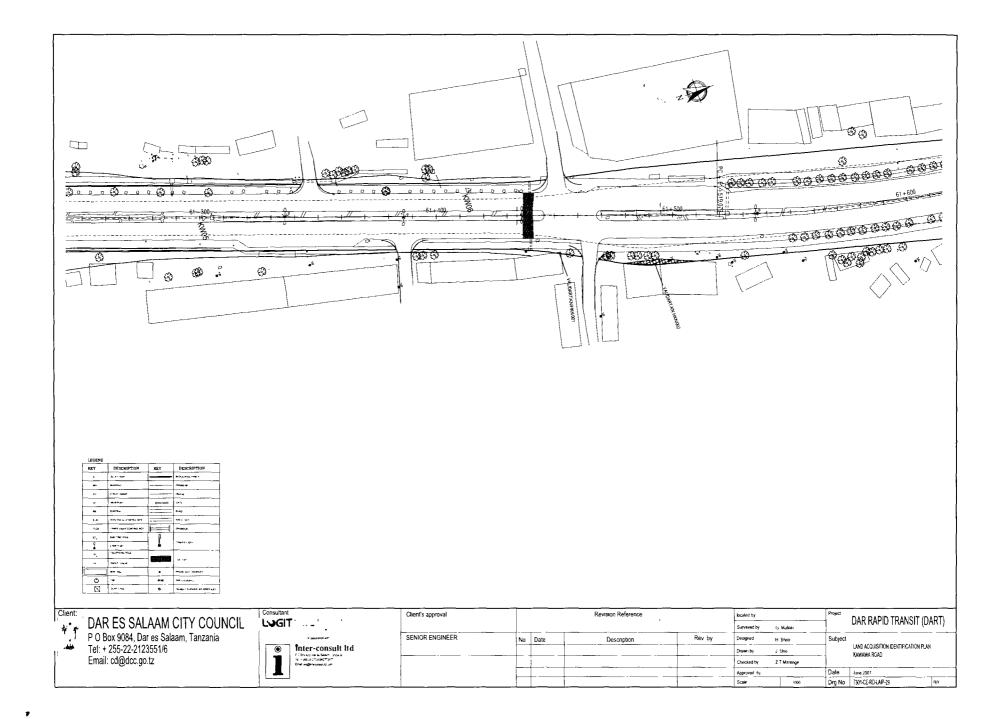












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