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CONSULTANCY SERVICES FOR THE
CONCEPTUAL DESIGN OF A LONG TERM
INTEGRATED DAR ES SALAAM BRT
SYSTEM AND DETAILED DESIGN FOR
THE INITIAL CORRIDOR

ANNEX VOLUME 8
IMPACT ANALYSIS AND MITIGATION
CHAPTER 8.5
Resettlement Action Plan – Phase 1 – Part B

Dar es Salaam
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Rev. 02

LOGIT

i inter-consult ltd.

JGP

ACRONYMS AND ABBREVIATIONS

BRT	Bus Rapid Transit
CBD	Central Business District
CBO	Community Based Organizations
DAI	Direct Area of Influence
DART	Dar Es Salaam Rapid Transit
DCC	Dar Es Salaam City Council
DSM	Dar Es Salaam
ESIA	Environmental and Social Impact Assessment
GOT	Government of Tanzania
PAP	Property Affected Peoples
PIC	Public Information Center
PMU	DART Project Management Unit
RAP	Resettlement Action Plan
ROW	Rights-of-Way
RPF	Resettlement Policy Framework
RPIT	Resettlement Planning and Implementation Team
TANROADS	Tanzania National Roads Agency
ToR	Terms of Reference
TZS:	Tanzanian Shillings
USD:	United States Dollars

**Dar es Salaam BRT System
Phase I – Part B**

Resettlement Action Plan (RAP)

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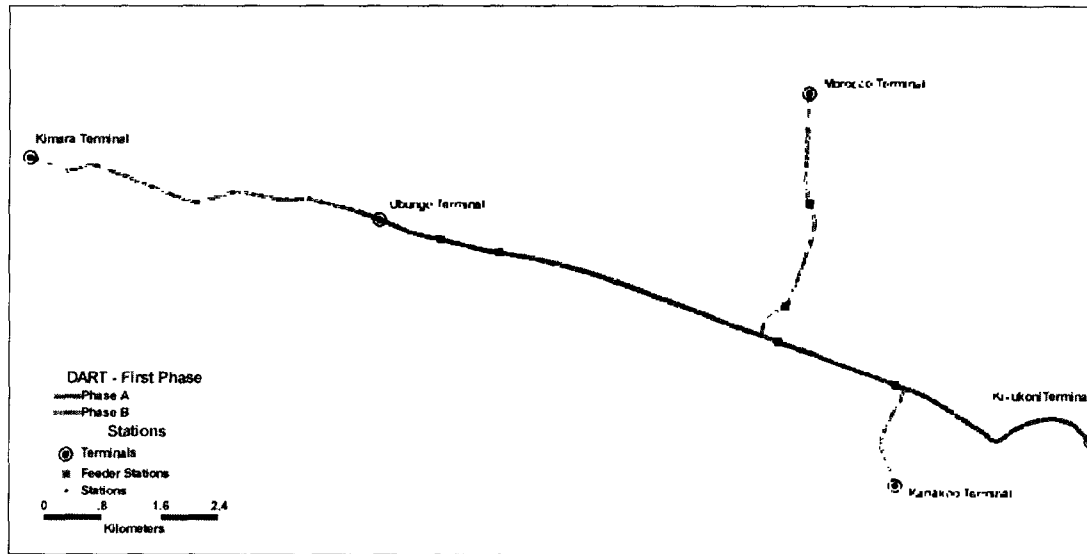
1.0 Introduction

1.1 The Resettlement Action Plan of Phase I – Part B in the Context of the DART Project’s Resettlement Policy Framework

This is the Resettlement Action Plan (RAP) for Phase I – Part B of the DART Project in Dar es Salaam and has been prepared according to the Resettlement Policy Framework (RPF) issued by the Dar es Salaam City Council in February 2007 (Rev. 01). It should be analyzed in conjunction with the RPF.

As established in the RPF, two (02) Resettlement Action Plans will be prepared for Phase I. Part B herein refers to the 9.90 Km. segment between Ubungo Terminal and Kimara along Morogoro Road, and to the branches along Msimbazi Road leading to Kariakoo Terminal, and Kawawa Road leading to Morocco Terminal. **Figure 1.1.a** below shows the location of DART Phase I – Parts A and B.

Figure 1.1.a
Location of DART Phase I – Parts A and B



This RAP closely follows the guidelines established in the RPF. It is based on a Socio-Economic Survey and a Property Survey which were concluded in April 2007. This month has been adopted as the cut-off date for valuation purposes.

The Property Survey was based on application of the Inspection and Valuation Questionnaire included in Annex 01 of the RPF. The Socio-Economic Survey was carried out through application of the Residential Resettlement Questionnaire and Commercial Activities Questionnaire (Annex 02 of the RPF).

A total of 191 properties will be affected for implementation of DART Phase I – Part B. The vast majority of these will be totally affected (172). Thus resettlement impacts resulting from implementation of Part B will be significantly greater than those for Part A.

Project Affected Peoples (PAP) include 459 entities (families, businesses, other). This includes 14 non-occupant owners of buildings, 228 resident families (including 135 owner-occupiers and 93 tenant families occupying a total of 146 properties with residential use, of which 80 exclusively residential and 66 mixed use), 212 businesses (including 63 businesses in owner-occupied premises and 149 businesses in rented premises, occupying 106 properties, 40 of which with exclusively commercial use) and 5 occupants whose activities were not identified, since their properties will be only marginally affected (fences and walls).

Of the 19 partially affected properties, ten (10) will suffer interference limited to fences or boundary walls. Current use in five (05) of these was not identified in the survey.

The 228 resident families include an estimated 1,465 people in total. The 212 businesses employ an estimated 496 persons (besides the business owners).

Most expropriation for Phase I – Part B will take place along Morogoro Road and at station locations along the Msimbazi and Kawawa Road branches. Significant relocation impacts will occur at Kariakoo Terminal, affecting several employee houses in a single property belonging to the TRC railway company.

Expropriation Plans showing project intervention limits and affected properties are included in **ANNEX 04**. Other back-up documentation for this RAP includes the Proposed Compensation Schedule (**ANNEX 01**), and the complete data bank resulting from application of the Property Inspection and Valuation Questionnaire (**ANNEX 02**) and the Socioeconomic Questionnaires (**ANNEX 03**).

The Proposed Compensation Schedule was prepared by the RAP team's certified valuer and will be submitted for approval by the City's Chief Valuer.

Summary tables with main Compensation Schedule total values per Ward and Municipality and per type of allowance are included in this RAP. Similarly, a summary budget for Complementary Compensation and Assistance measures as established in Section 5.2 of the RPF is presented, as well as a consolidated budget for DART Phase I – Part B RAP implementation.

All figures are indicated in Tanzanian schillings (TZS) and main totals and sub-totals are also presented in US dollars. A 1.200 TZS per dollar exchange rate was adopted.

As will be noted in some sections of this RAP, a great deal of effort was spent with verification of data consistency. Not all data required in the questionnaires was completed in all cases. Some questionnaires were only partially considered due to inconsistencies between data in the Property Survey and the Socioeconomic Survey. All these cases are pointed out as pertinent. These shortcomings, however, are minor and do not affect the general results. Furthermore, all PAP with grievances affecting valuation and/or eligibility will have the opportunity to request revisions during this RAP's disclosure procedures (see Section 6.3).

Prior to this RAP's conclusion, the Environmental and Social Impact Assessment (ESIA) on the project was completed. This included a detailed Social Impact Assessment contemplating both construction and operation phases. Description of all expected socioeconomic impacts as described in the ESIA (and including impacts associated to resettlement) is included in **ANNEX 05**.

1.2

Socioeconomic Conditions along Phase I – Part B Alignment

Socio-Demographic Characteristics

The immediate DART impact area for Phase I - Parts A and B (DAI) covers 18 Wards in Kinondoni and Ilala Municipalities. The socio-demographic characteristics of the population in this area were analyzed on the basis of the 2002 Population and Housing Census and on field survey work concluded in April 2007. The total population of the project area was 479,219 in 2002. Given the annual growth rate of 4.3%, it can be estimated that this number is near 590,000 in 2007.

Table 1.2.a below consolidates the 2002 population in the Sub-Wards in the DAI of Phase I – Part B only. As can be seen, total 2002 population in this area was 196,399 inhabitants, equivalent to 41% of the population in the DAI for Phase I – Parts A and B. Most of this population is within Kinondoni Municipality.

The fertility rate for Ilala and Kinondoni Municipalities is 6.5%. Population is predominantly young, with 32% of people under the age of 15 years and an age dependency ratio of 51.24%.

Table 1.2.a
Total Population by Wards – Phase I – Part B

District	Ward	Sub-Wards	Population		
			Male	Female	Total
Ilala	Jangwani	1	3,728	3,322	7,050
	Gerezani	1	2,048	2,069	4,117
	Kariakoo	2	4,986	4,322	9,308
Total					20,475
Kinondoni	Magomeni	2	5,932	5,746	11,678
	Kimara	1	33,053	24,043	66,288
	Kinondoni	2	10,628	10,861	21,404
	Hananasifu	3	16,040	15,983	32,023
	Mwananyamala	3	21,946	22,585	44,531
Total					175,924
Grand Total					196,399

Source: United Republic of Tanzania 2002 Population and Housing Census.

The immediate DART impact area had a total of almost 102,000 households in 2002, with an average household size of 4.7. In the Ilala section, 27% of the households are female-headed while in Kinondoni 30% are female-headed (DCC, 2006). The high percentage of female-headed households has implications for poverty reduction since women have lower incomes compared to men, partly because of low educational attainment levels and limited access to other social and economic resources.

The two dominant family structures in the project area are monogamous and polygamous types, basically reflecting the marriage systems that exist in Tanzania. Family structures are also either extended or nuclear. A large proportion of the families in the project area are extended since their members include more than husband, wife and children. These extended families generally include other relatives and children living in households other than their own are very common. This is an accepted cultural practice and norm in the project area and in most other urban centers in Tanzania. These children are often attending schools, helping with household chores or working. Consequently, the large average household size in the project area is not only a reflection of high fertility rates but also the result of the extended nature of the households.

Survey work in the project impact area identified high ethnic diversity. There is no dominant ethnic group and Tanzanians of all racial or ethnic backgrounds are found. Among these are the indigenous people of the coastal area of Tanzania, the Wazaramo. Others ethnic groups include the Wachaga, Wasukuma, Wapare, Wahaya and many others. Asian ethnic groups (mainly Indian) have clusters of residences concentrated in Upanga and Kisutu Wards. The Jangwani Ward in Ilala district is home to a wide variety of Arabs, Indians and indigenous ethnic groups of Tanzania.

However, it must be pointed out that in spite of this diversity of origins, the majority of people in the DART impact area were born and raised in Tanzania.

Economic activity in the Direct Area of Influence (DAI) is characterized by high levels of informality. Trade and commerce is the dominant economic activity. Medium to large-scale businesses are common in sections of Ilala Municipality, while small to medium-scale businesses of all types occur in the Kinondoni section of the DAI.

Small-scale businesses which are dominant include street vendors and small business structures built as front extensions of existing houses. This ribbon-like pattern of occupation is present particularly along significant portions of Morogoro Road segments of Phase I – Part B and will be significantly affected by the project.

Local Urban Structure

Phase I – Part B includes the extension of Phase I – Part A along Morogoro Road, and the two branches along Msimbazi Street and Kawawa Road (see **Figure 1.1.a**).

The Morogoro Road extension will cover the segment between Ubungo Terminal and Kimara. Along this segment, Morogoro Road is a double lane (2 x 2) avenue that serves the outskirts of the city, linking sub-centers that are located linearly along it. It also serves the upcountry areas of Kibaha, Mlandizi, Chalinze and beyond. Thus, the avenue intercepts very varied urban landscapes, going through densely occupied commercial centers to residential areas and even some unoccupied areas, particularly when approaching Kimara.

Ribbon-like development fronts the right-of-way in the densely occupied segments and this kind of occupation is largely the reason why most properties affected in Phase I – Part B are totally affected.

Msimbazi Street is a single lane two-way avenue which is among the most densely congested traffic roads in the city. It extends to the Kariakoo area and receives traffic from several arterial roads. All the buses plying into the Kariakoo market and its surroundings use the street. Hence, it is busy all day as it receives traffic from early in the morning and the same traffic flows out in the evenings. Adjacent land-use is consolidated and high density with very intense commercial activity.

Whereas expropriation at Msimbazi Street branch will be quite limited as a result of project adjustments, significant impacts to commercial activity may result from limited access during construction and reduced street parking space once DART operation begins. At Kariakoo Terminal, relocation will affect worker's housing within the property of the TRC railway company.

Kawawa Road is a double lane tarmac road that functions as a ring road that connects Morogoro and Bagamoyo roads passing through the Kinondoni areas and ending up at its junction with Bagamoyo Road. It has public transport running along it that serves either Mwenge – Mtongani or Kawe – Mbagala. It also carries traffic from Tegeta to Kariakoo.

Generally, the traffic flow along this road is moderate, but it is poised to increase in the future as the redevelopment of the old and condemned city houses in Kinondoni area is implemented. High-rise apartments will replace the old structures thus increasing population and traffic.

Urban structure along the Kawawa Road branch is a predominantly rectangular urban pattern with medium density mixed-use occupation. Irregular urban pattern occurs mostly along the edges of the Sinza River valley which is intercepted perpendicularly. Major highlights are the open spaces near Morogoro Road and at Biafra Square, as well as the Sinza River valley.

Kawawa Road has a relatively wide right-of-way and accommodates the DART project with a less tight geometry than at Msimbazi Street branch. Expropriation here will concentrate at station locations.

Land Use

Along Morogoro Road between Ubungo and Kimara, predominant land use is a mix of residential, agriculture and livestock breeding areas. Commercial activities form ribbon developments fronting the right-of-way along commercial clusters near the main intersections and markets.

The Kariakoo sub-center, at the end of the Msimbazi corridor has more intense commercial activity and larger establishments than those that occur elsewhere along Phase I – Part B.

Land use along Kawawa Road is also dominated by commercial activities concentrated on some segments, with relatively homogeneous residential areas in between.

In order to better analyze current abutting land-use along the Phase I – Part B corridor, it was divided into three (03) segments, according to their particular characteristics, as follows:

Segment	Streets/Roads
1	Morogoro Rd – from Ubungo to Kimara
2	Kawawa Rd
3	Msimbazi Street

Segment 1

This segment begins at the Nelson Mandela/Sam Nujoma Road Junction, and goes up to Kimara. At this point, the general characteristics of land use change as the area contains mostly one story residential buildings and sparse commerce along the road. This commerce is not accessible directly from the road (only some petrol stations and few other services), and to gain access to the buildings it is necessary to take lateral streets.

In this area there is still a significant number of street vendors, most of them concentrated next to bus stops and to commercial areas.

The most significant points or buildings on this segment are:

- TANESCO Power Station and Offices
- Daladala Terminal
- Institutional Buildings close to the junction with Nelson Mandela/Sam Nujoma

Segment 2

Msimbazi Street is today a very congested axis, especially because of the concentration of daladala traffic. There are many tall buildings, some of them under construction, and there is a great concentration of commercial activities, both on the ground and upper floors. This segment is mainly characterized by the sales of spare parts and electronic articles, but there are also food stores, and it is possible to find many street vendors, fixed or transient, along the street. It is also near the Kariakoo Market, which will not be directly affected, but has an important influence in this area.

The most significant points or buildings in this segment are:

- Railway Crossing
- Scandinavian Bus Depot (after the Railway)

Segment 3

This segment comprises the Kawawa Road branch, starting at Morogoro Junction and ending at Ali Hassan/New Bagamoyo Road junction. The main general characteristics of this segment are the concentration of small commercial sites close to the road, and the existence of a few religious centers.

The road crosses the Sinza River Valley and, at this point, there is a great concentration of residential buildings, though most of them do not directly face the road.

There are also some open areas, one near the junction to Morogoro, that is used as a recreation park, and another one at Biafra, in front of the Biafra School Building, that is also used by the population for leisure activities.

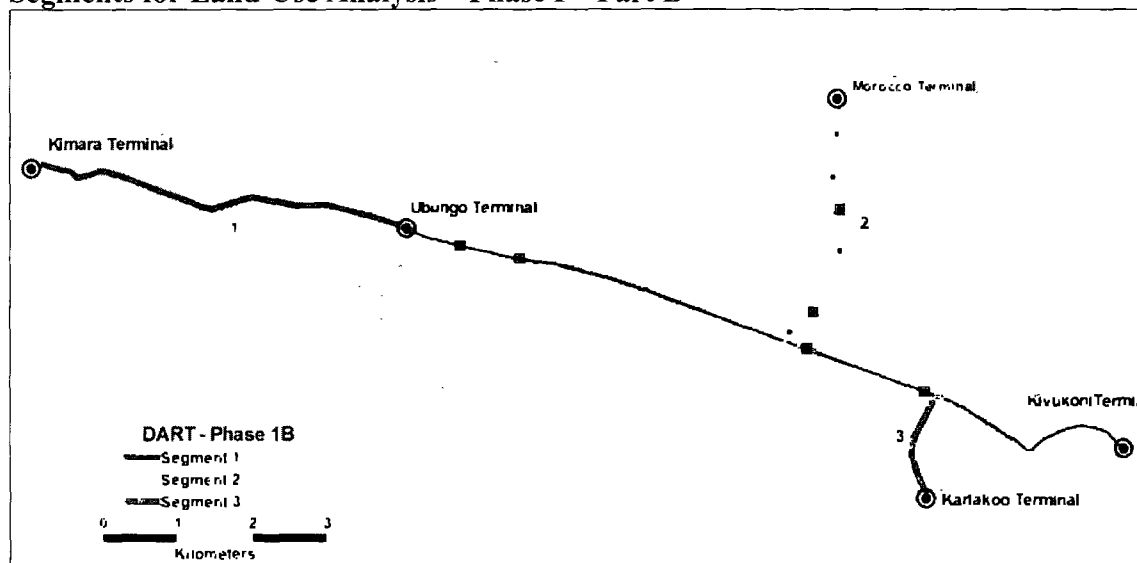
The most significant points or buildings on this stretch are:

- Magomeni Health Center
- Magomeni Roman Catholic Church

- Open University of Tanzania
- Daladala Terminal

Figure 1.2.a below provides indication of the limits of each one of the three segments analyzed above.

Figure 1.2.a
Segments for Land-Use Analysis – Phase I – Part B



Historical, Cultural and Archeological Features

No historical or cultural buildings will be directly affected by Phase I – Part B. Furthermore, unlike Part A where many historical buildings front the project's alignment, this does not occur along Part B. Similarly, there are no records of archeological remains in the vicinity of DART Phase I – Part B alignments.

Utilities

Utility networks and service coverage along the Phase I – Part B corridor include 100% coverage of water supply grids. The sewer collection grid covers only the Msimbazi Street branch. There is no sewer collection grid along Kawawa Road branch and along Morogoro Road between Ubungo and Kimara.

1.3

Consultation during RAP Preparation

Consultation and disclosure relative to DART Phase I – Part B has taken place in an effective way during RAP preparation. In this context, during execution of both the property survey and application of socioeconomic questionnaires as part of this RAP's planning process, an effective exchange of information with affected parties took place. As established in the RPF, the certified valuer and his assistants, as well as the interviewers that applied the Socio-Economic Questionnaire, ensured that:

- All PAP whose property will be directly affected were served notice as required by law and became aware of the fact and of the extent of necessary expropriation (partial or total);
- A preliminary understanding of valuation and compensation criteria was conveyed, as well as preliminary information on arrangements for payment of compensation, allowances and assistance;
- The possibility for grievance and the establishment of a legally required Grievance Committee was informed;
- The cut-off date for valuation purposes was made clear and all PAP became conscious that no speculative occupation of the future right-of-way will result in compensation and that any ongoing construction should stop;
- All PAP were informed of the likely timing of resettlement is conveyed.

Thus, it can be stated that at the time of submittal of this Resettlement Action Plan, all PAP have a preliminary understanding of the resettlement process, as well as of the main entitlements and respective eligibility criteria.

Further to the surveyed PAP, several local authorities were consulted during the RAP preparation process. As pertinent to Phase I – Part B, these included:

Ms. Elizaberth Lucas – Ward Executive Officer – Kinondoni Ward
Ms. Mary Kimatali – Magomeni Ward
Mr. Amani H. Kafugugu - Ward Executive Officer – Magomeni
Ms. Elizabeth E. Mbalale - Ward Executive Officer – Ndugumbi
Mr. Ally Mtambo – Ward Executive Officer – Kimara Ward
Ms. Magreth Mkandawile – Ward Executive Officer – Ubungo Ward
Mr. Zuhura Almasi – Ward Executive Officer – Hananasif Ward
Mr. Ratifa Ramadhani – Mwananyamala Ward
Mr. Tarimo – Ward Executive Officer – Kariakoo Ward
Mr. Faraja Machumu – Upanga West Ward
Mr. Bahati Shangama – Ward Executive Officer – Gerezani Ward
Mr. Suleman Anastasi - Transport Economist – DART

Furthermore, extensive consultation was conducted during ESIA preparation which progressed parallel to the RAP preparation process. This included a wide variety of stakeholders, including public entities with direct or indirect involvement with the project, as well as residents and businesses located along the corridor, including those that will be directly affected by expropriation.

The main public entities / public officers consulted during ESIA preparation include:

- Vice President's Office (VPO)
- Prime Minister' Office (PMO)
- National Environment Management Council (NEMC)
- Tanzania Electricity Supply Company (TANESCO)
- Dar Es Salaam Water Supply and Sanitation Company (DAWASCO)
- Tanzania Telephone Communication Limited (TTCL)
- Tanzania Railways Corporation (TRC)
- Dar Es Salaam City Council (DCC)
- The Municipalities of Temeke, Kinondoni and Ilala
- Ward Executive Officers along the alignment

In addition to public entity consultation, several local experts and university professors specialized in transportation planning, urban planning, environmental management and other pertinent skills were contacted and contributed with their views toward adjustment of the DART project to better meet local needs.

Further to consultation described above, additional consultation will take place during the RAP implementation process, as described in Section 6.1 herein.

1.4

Legal Framework

1.4.1

Land Tenure and Legal Status

The legal framework for resettlement is first and foremost based on the following articles of the Constitution of the United Republic of Tanzania (1977):

- Article 24 (1): Subject to provisions of the relevant laws of the land, every person is entitled to own property, and has a right to the protection of his property held in accordance with law.

- Article 24 (2): It shall be unlawful for any person to be deprived of property for the purposes of nationalization or any other purposes without the authority of law which makes provision for fair and adequate compensation.

Therefore, payment of compensation is both a legal and constitutional right under Article 24 of the Constitution.

Further to the Constitution, the following are the main legal instruments affecting property acquisition and resettlement in Tanzania:

- Land Acquisition Act N° 47 of 1967.
- Land Act N° 4 of 1999.
- Village Land Act N° 5 of 1999.
- Complementary regulations issued in 2001.

The legal basis for compensation / acquisition of private properties or right-of-ways for infrastructure and other development projects is contained mostly in Land Act N° 4 of 1999, Village Land Act N° 5 of 1999 and Land Acquisition Act N° 47 of 1967.

Land Act N° 4 of 1999 is basically about land tenure and land rights, but also addresses issues of compulsory acquisition, mortgages and regularization of unplanned areas.

Land Act N° 4 of 1999 and Village Land Act N° 5 of 1999 and their respective Land and Village Land Regulations, including in particular Supplement N° 16 of 2001, govern the procedures for assessment and payment of compensation for land and unexhausted improvements, loss of accommodation, loss of profit, disturbance and transport.

Section 3 (1) (a) of Land Act N° 4 of 1999 establishes that all Land in Tanzania is public land. Therefore, private land as such does not exist. Occupancy of Land by individuals is granted legally regular status through a "*grant of a right to occupancy*".

The Government leases Land under the Right of Occupancy system for varying periods at economic ground rents. Such rent is reviewed periodically (normally at 10 yearly intervals). The ground rent paid in respect to government Right of Occupancy is the Land Rent levied under section 7(2) of the Land Ordinance. All urban land in Tanzania is liable to a payment of Rates in lieu of Land Rent as per the Urban Authorities (Rating) Act of 1983.

Further to Public Land, the Land Act recognizes "Village Land" and other forms of customary tenure.

Section 3 (1) (f) of Land Act N° 4 of 1999 provides that “*interest in Land has value and that value must be taken into consideration in any transaction affecting Land*”. Therefore the, acquisition of Land will include compensating owners for bare land in addition to unexhausted improvements.

Sub-paragraph (g) of the same section provides that full, fair and prompt compensation has to be paid to any person whose “*right of occupancy or recognized long-standing occupation or customary use of land*” is affected or interfered with to their detriment. Thus, compensation for land in project-affected areas (which are urbanized and over which no customary forms of tenure prevail), is forthcoming only where its occupancy has been legally granted or is long-standing.

However, in recognition of the existence of informal types of tenure in urban areas, the Land Act provides for the validation of interests in land other than a granted right of occupancy (s. 53-55). In order to address the question of many people acquiring and occupying urban land otherwise than through a granted right of occupancy, the act introduces the concept of a residential license as a derivative right (section 23 of the Act). Any person who at the commencement of the act (1999) has occupied land in an urban area or peri-urban area as his home for no less than three years, without any officially granted right of occupancy, can receive a “residential license” and is entitled for compensation of land should it be required for a public purpose. Residential licenses have limited validity (maximum of two years) and need to be continuously renewed.

1.4.2

Property Valuation Norms

Valuation Methods are established in Land Act N° 4 of 1999.

Land

This type of compensation applies to all affected households that hold officially granted rights of occupancy or alternatively possess a residential license. Compensation is forthcoming regardless of whether there is need for resettlement or the affected party opts to rearrange within the remaining plot.

In accordance with section 3 of the 2001 Regulations of Land Act N° 4 of 1999 the basis for assessment of the value of lands affected by the project is the market value of the respective land.

Buildings

In the case of buildings, local practice in Tanzania adopts estimate of surrogate market value through Replacement Cost Methods. This method establishes the price on the basis of the estimated cost of erecting a similar unit at today's price. The observed condition of the building is taken into account and is used as a basis to allow depreciation to be discounted from the Replacement Cost figure in order to establish what would be the likely price a person may be prepared to pay for the subject property if given opportunity to buy.

The "Depreciated Replacement Cost" thus obtained is considered to be equal to "Market Value".

In cases where more than 50% of a property is affected, compulsory expropriation of 100% proceeds and valuation needs to proceed accordingly.

1.4.3

Other Legally Established Indemnification and/or Compensation

Regulations of Land Act N° 4 of 1999, made under Section 179 of the Land Regulations which became operational in May 2001, contemplate several complementary compensation allowances for which expropriated parties are eligible. These include:

- Disturbance allowance;
- Transport allowance;
- Loss of profit or accommodation allowance;
- Accommodation allowance.

Disturbance Allowance is payable as a percentage of property valuation in compliance to the provisions of Act N° 4 of 1999. The percentage is the average commercial bank rates offered on fixed deposits during 1 year.

In accordance with Section 11 of the Regulations of Land Act N° 4 of 1999, a transport allowance is paid to all project-affected persons that will need to be displaced. The payment is intended to meet transport cost incurred by property owners. It is specifically noted that valuers may apply this compensation to all properties, i.e. to the vast majority of those who will not be resettled. It may also be applied to tenants. Transport allowance is computed on the basis of market rates for transportation services within the project area, considering the average cost of transporting 12 tons over a distance of 20 Km.

In accordance with Section 9 of the 2001 Regulations of Land Act N° 4 of 1999, the net monthly profit obtained from businesses associated with the affected properties needs to be assessed. Audited accounts need to be analyzed as proof of declared profits. In the absence of audited accounts, valuers can establish loss of profit compensation on the basis of official guidelines. In this case, the amount allowed by valuers does not exceed one million shillings per annum. Compensation is forthcoming on the basis of estimated loss of profit over a 36 month period. This calculation can apply also to businesses that are only temporarily affected during the project's construction phase. This compensation will be made to all businesses regardless of the status (owner or tenant) of the affected building. However, in the case of businesses that occupy own premises, accommodation and disturbance allowance is paid and this is understood to incorporate loss of profit.

As long as the respective business is affected by the project, the loss of profit shall be exclusively and directly paid to the person who loses his/her business premises.

Accommodation allowance applies to all structures or portions of structures that are to be demolished, regardless of whether resettlement is required or not. This allowance is meant to compensate affected peoples for the costs incurred during the period when construction of new structures is under way. In accordance with Section 8 of the 2001 Regulations of Land Act N° 4 of 1999, accommodation allowance is calculated on the basis of monthly rent multiplied by 36 months. Monthly rent is equivalent to the market rental value of the affected part of the property per month.

1.4.4

Comparison with WB OP 4.12 – Gap Analysis

Based on review of pertinent Tanzanian legislation (Sections 3.1 through 3.3) and criteria adopted in the World Bank's OP 4.12, the following main gaps have been identified:

1. Land indemnification

Under Tanzanian law, all land belongs to the government and will be compensated only in cases where its occupation has been officially granted in some way or where a residential license has been obtained. Thus, some properties that are irregularly occupied will only be compensated for affected structures (immovable assets) at replacement cost. In cases where the affected family or business needs to be relocated, lack of indemnification for land at market value may mean that an equivalent alternative may not be purchased with the amount of compensation to be received. Whereas World Bank policy does not require compensation for illegal land, it does require that affected peoples end up in a condition which is at least equivalent if not better than the original situation. One frequent solution to attain this is implementation of a Resettlement Project, where housing units will be

produced and delivered at a cost that exceeds the value of compensation that eligible PAPs are entitled to receive. In other cases, where quantity or geographical dispersion of PAPs in his situation make this kind of collective solution unpractical, some form of bonus or complementation of the compensations and/or allowances foreseen in Tanzanian law may be necessary in order to comply with OP 4.12.

2. Loss of profit compensation for businesses to be relocated

Under Tanzanian law only businesses occupying rented premises will be compensated for loss of profit. This compensation is in fact quite generous, and allows for 36 months of net profit compensation, based on verification of audited accounting records. Where these records do not exist, a fixed assumption on the value of profit can be adopted.

However, no loss of profit compensation is foreseen in Tanzanian law in the case of displaced businesses occupying premises that belong to the owner of the business. Whereas it may be argued that this property owner is already receiving disturbance allowance (4% of the market value of affected property) and accommodation allowance (36 months of market rental value), lost profit can represent a value larger than these allowances. Under the principles of OP 4.12, loss of means of livelihood should be compensated separately from loss of property.

3. Lost of rental income

Compensation of non-occupant owners for lost rental income is meant to be covered by accommodation allowance. However, Tanzanian law is not clear as to whether or not the accommodation allowance is to be paid when only part of a structure is expropriated and the owner (or tenant) continues to live on the remaining part of the property. In practice in such cases no accommodation allowance is paid.

4. Compensation for additional living costs

Disturbance and accommodation allowances should jointly compensate for additional costs incurred during the transition to a new location. However, there may be some living costs which will be permanently altered as a result of resettlement. These include transportation costs to work and school, as well as on-site costs of living, such as rent, water and sewer collection fees, garbage collection fees, energy bills, land rent and property tax. Where such additional costs can be demonstrated to be a result of lack of option (as opposed to preference for the chosen new place of residence), some form of compensation or assistance should be forthcoming.

1.5 Institutional Framework

The institutional framework affecting resettlement in the DART Project will involve the following actors / entities:

- The Dar es Salaam City Council Project Management Unit (PMU),
- The Resettlement Plan Implementation Team (RPIT),
- The resettlement consultants,
- The Grievance Committee,
- The Chief Government Valuer in the Ministry of Lands, Housing and Human Settlement Development,
- Municipal Valuers,
- Authorized Municipal Land Officers,
- Ward Executive Officers,
- District Commissioners,
- Regional Commissioner.

The PMU is the main executive agency responsible for the DART Project's implementation. It will centralize project management functions and this will include coordination of the RAP implementation process. In order to do this, it will create a Resettlement Planning Implementation Team (RPIT) that will execute key activities, including assistance to PAPs and supervision of other parties involved in the process. Responsibilities of the RPIT are detailed in Section 7.0.

Resettlement consultants were hired by PMU in order to conduct social and property surveys and to assist with Resettlement Policy Framework and RAP preparation.

The PMU, with support of the RPIT and consultants as seen fit, will establish PAP eligibility for each one of the Complementary Compensation Entitlements established in the Resettlement Policy Framework; i.e. assistance and/or compensation in excess of that which is provided for under the terms of Tanzanian Law (see Section 4.2). The Grievance Committee to be created as part of the RAP implementation process will also participate of this process, to the extent that it will analyze claims for complementary compensation and may recommend that they be attended.

The main steps for establishment of the entitlements that each PAP will be eligible for under the terms of Tanzanian Law (section 4.1) are the following:

1. Property Identification and Orientation meetings with key Personnel and the affected persons.

2. Valuation of properties by the Consultant in collaboration with the Municipal Valuers.
3. Preparation of Resettlement Action Plan (RAP) by the Consultant.
4. Joint review of Valuation Report by Municipal Valuers and the Consultant. At this stage Municipal Valuers also Prepare Compensation Schedule.
5. Valuation report approval by the Chief Government Valuer in the Ministry of Lands, Housing and Human Settlement Development.
6. Submittal, by the Chief Valuer of the approved Valuation Report to Municipal Valuers.
7. Approval of the Compensation Schedule by the Authorised Land Officers and Ward Executive Officers.
8. Submittal of the Compensation Schedule to the District Commissioner for Endorsement.
9. Compensation Schedule forwarded to the Regional Commissioner for endorsement.
10. Submittal of the endorsed Compensation Schedule to the authorized Municipal Land Officer.
11. The Authorized Land Officers disclose the Compensation Schedule to the Ward Executive Officers (WEO) for display to the public and prepare the affected to receive compensation.
12. The Grievance Committee is created.
13. Payment of compensation as per approved schedule.
14. Claims are received, analyzed and decided upon by the Grievance Committee s per the procedure outlined in Section 6.2.

2.0 Summary of Property Survey Results

2.1 Affected Property Statistics by Type

Table 2.1.a below shows the composition of affected properties by type of land-use, with indication of those that will be totally vs. partially affected. As can be seen, 191 buildings will be affected, of which 172 (90.1%) will be totally affected. 146 buildings have residential use, where 80 are exclusively residential and the other 66 are mixed-use buildings having some kind of commercial activity.

Only 40 buildings (20% of the total) are exclusively commercial. However, considering commercial uses in mixed-use buildings, it can be concluded that 55.5% of affected buildings (40 + 66) have some kind of commercial activity.

Table 2.1.a
Affected Properties by Type of Land Use

Use Type	Total		Partially Affected		Totally Affected	
	Count	Percentage	Count	Percentage	Count	Percentage
Residential	80	40%	4	2.1%	76	39.8%
Commercial	40	20%	10	5.2%	30	15.7%
Mixed use	66	35%	0	0.0%	66	34.5%
Other	5	5%	5	2.6%	1	0.1%
Total	191	100%	19*	9.9%	172	90.1%

NOTE (*): 10 of these have only fences or boundary walls affected

Most of the buildings to be partially affected (10 of 19) have exclusively commercial use. Another four (04) partially affected buildings are residential. Current use in the remaining five (05) was not identified during the survey. However, in all these cases only fences or boundary walls will be affected.

Information regarding public utilities and other infrastructure servicing the buildings to be affected is summarized in **Table 2.1.b**. As can be seen, a significant percentage of interviewees did not respond to all the information requested, in particular with regards to sewer collection services. This is probably because many interviewees are tenants and/or partial occupants of buildings and are thus not necessarily involved with payment of utility bills.

Notwithstanding, it is clear that at least 67% of affected buildings are fully serviced by both water (DAWASCO) and sewer (DAWASA) systems.

A much larger percentage is served by the electricity grid. Only seven (07) properties reported having no connection to the grid.

Another aspect worth highlighting with regards to infrastructure is that 65.4% of affected properties in Part B are fronting on non-paved roads.

Table 2.1.b
Infrastructure of Affected Buildings

Infrastructure			
Sewage treatment System	No Information	63	33,0%
	DAWASA	126	66,0%
	Septic Well	2	1,0%
	Ditch	0	0,0%
	River	0	0,0%
Water	No Information	63	33,0%
	DAWASCO	128	67,0%
	Well	0	0,0%
	Other	0	0,0%
Electrical Network	Not Informed	18	9,4%
	Yes	166	86,9%
	No	7	3,7%
Is energy metered?	Not Informed	0	0,0%
	YES	166	100,0%
	No	0	0,0%
Pavement Type in front of the property	Not Informed	22	11,5%
	Asphalt	44	23,0%
	Earth	125	65,4%
	Other	0	0,0%

3.0 Summary of Social-Economic Survey Results

3.1 Categorization of Surveyed Project Affected People (PAP)

Project Affected People have been classified in this RAP according to the typology established in Section 4.0 of the Resettlement Policy Framework. A total of 459 “entities” will be affected in Phase I – Part B, as shown in **Table 3.1.a** below. As can be seen, this table includes indication of whether affected PAP will receive indemnification for land as well as buildings under the terms of Tanzanian law. This information is necessary for establishing eligibility for complementary compensation measures defined in the RPF.

Table 3.1.a
General Classification of Project Affected People

PAP Category		Total	%
Non-occupant owner of partially affected property	Will not receive compensation for land	3	0.7%
Total		3	0.7%
Non-occupant owner of totally affected property	Will not receive compensation for land	4	0.9%
	Will receive compensation for land	7	1.5%
Total		11	2.4%
Owner occupier of residence of partially affected property	Will not receive compensation for land	1	0.2%
	Will receive compensation for land	3	0.7%
Total		4	0.9%
Owner occupier of residence of totally affected property	Will not receive compensation for land	22	4.8%
	Will receive compensation for land	109	23.7%
Total		131	28.5%
Residential tenants of totally affected property	Will not receive compensation for land	93	20.3%
Total		93	20.3%
Owner of business in own premises of partially affected property	Will not receive compensation for land	6	1.3%
	Will receive compensation for land	1	0.2%
Total		7	1.5%
Owner of business in own premises of totally affected property	Will not receive compensation for land	45	9.8%
	Will receive compensation for land	11	0.4%
Total		56	12.2%
Owner of business in rented premises of partially affected property	Will not receive compensation for land	5	1.1%
Total		5	1.1%
Owner of business in rented premises of totally affected property	Will not receive compensation for land	144	31.4%
Total		144	31.4%
Uses not identified that will be partially affected	Will not receive compensation for land	1	0.2%
	Will receive compensation for land	4	0.9%
Total		5	1.1%
Grand Total		459	100.0%

Table 3.1.a includes 5 PAP classified as "other". This includes properties that will be marginally affected (only walls or fences) and applicable compensation will be limited to indemnification of land and/or improvements, without implying in relocation or even re-installation within the same plot. Thus, a Socioeconomic Questionnaire was not applied in these cases, and data included in the Inspection & Valuation Questionnaire (Property

Survey) did not always identify type of use of remaining building in the plot. In effect, as seen in **Table 2.1.a**, there are 10 properties that will be marginally affected in this way. In the other five cases, use of the remaining building was registered in the Inspection & Valuation Questionnaire and hence these PAP have been classified accordingly in **Table 3.1.a**.

Table 3.1.b consolidates data on the number of affected families in DART Phase I – Part B. As can be seen, a total of 228 families will be affected, 224 of which in totally affected properties and which will therefore need to relocate. The 228 families occupy a total of 146 buildings, indicating an average occupancy of 1.56 families per unit.

Table 3.1.c consolidates data on the number of family members in each one of the families to be affected. As noted, this information was retrieved in 180 of 228 cases.

On the basis of the families on which information was obtained, average family size is 7.9 dwellers per family in the case of owner-occupiers, and 4.28 dwellers per family in the case of tenant families. **Table 3.1.c** estimates total number of dwellers in the 228 residential units to be affected, by extrapolating on the basis of the average number of persons per family verified for the 180 families that supplied data. As can be seen, total population of affected families is estimated at 1,465 persons, including 1,067 in owner-occupier families, and 398 persons in tenant families. All tenant families will need to relocate. This is also the case with all but four (04) of the owner-occupier families.

Table 3.1.b

Number of Dwellers in Affected Residential Units as Reported in the Social-Economic Residential Questionnaire

Affected Residential Units	Total
Owner-occupant of totally affected property	131
Owner-occupant of partially affected property	4
Tenant of totally affected property	93
Tenant of totally partially property	0
Grand Total - Affected Units	228
NOTE: This number refers to affected units. There are 228 residential “entities” to be affected, in 146 properties with residential use (80 exclusively residential and with 66 mixed use)	

Table 3.1.c
Estimate of the Total Number of Dwellers in Residential Units

Number of Dwellers per Family Unit			
PAP		Only 180 Families with Data	Extrapolation for 228 Families in Total
Owner-occupier of residence that will need to resettle	Total Dwellers	976	1,067
	N° of Families Considered	123	135
	Average Dwellers per Family	7.9	7.9
Residential tenants that will relocate	Total Dwellers	244	398
	N° of Families Considered	57	93
	Average Dwellers per Family	4.28	4.28
Grand Total		1,220	1,465
Average		6.77	6.43

Table 3.1.d presents data on average total family income. This information was obtained from 180 of 228 families. As can be seen, the average monthly family income of owner-occupier families that will relocate is TZS 852,060.00 equivalent to US\$ 710.05 per month (based on data from 123 families). The average monthly family income of tenant families that will relocate is TZS 801,352.94, equivalent to US\$ 667.79 per month (data from 57 tenant families).

Table 3.1.d
Average Monthly Income of Affected Families

PAP		TZS
Owner-occupier families	Total	89.466.300,00
	Average	852.060,00
Residential tenant families	Total	40.869.000,00
	Average	801.352,94

NOTE: This information refers to 180 families, since data was not retrieved from 48 families.

Table 3.1.e consolidates data on the means of transport to work (for 1,220 dwellers belonging to 180 families) and **Table 3.1.f** totals transport costs per family. As can be seen, a significant part of persons in the sample did not inform the means used to go to work, or replied "none", meaning that they probably do not work on a regular basis (934 out of 1,220). This number seems reasonable, considering average family size and amount of children in school age.

Considering only the remaining 286 people in the sample, the most common means of transport to work is daladala (182 dwellers or 63.6%), followed by car or motorbike (57 dwellers or 20%). A total of 40 people reported walking to work.

Table 3.1.e
Means of Transport to Work of Dwellers in Affected Residential Units

PAP	Means of Transport to Work – Residential Unities	Total – dwellers	%
Owner-occupier of residence	Not Informed	739	75.7%
	0 = none	30	3.1%
	1 = walking	25	2.6%
	3 = daladala	126	12.9%
	4 = car – motorbike	49	5.0%
	5 = other	7	0.7%
Total		976	100.0%
Residential tenants	Not Informed	154	63.1%
	0 = none	11	4.5%
	1 = walking	15	6.1%
	3 = daladala	56	23.0%
	4 = car – motorbike	8	3.3%
Total		244	100.0%
Grand Total		1,220	

Total average monthly transport costs per person for owner-occupier families is TZS 15,144.23 (US\$ 12.62). In the case of tenant families, this monthly cost is TZS 7,662.50 (US\$ 6.38). This represents only 1.8% of monthly family income in the case of owner-occupier families and 0.95% in the case of tenant families.

Data regarding main other monthly expenses which are dependent on location and may be affected by resettlement was gathered on owner-occupier and/or tenant families that will need to relocate and are therefore subject to alteration of these costs. As can be seen in **Table 3.1.f**, these costs, which include land rent, taxes and utility services (as well as rent in the case of tenants) can be quite significant.

In the case of owner-occupier families, these costs averaged TZS 102,278.50 per month (equivalent to US\$ 85.20) for families living in individual houses, and TZS 161,416.67 (US\$ 134.50) for families living in rooms or apartments. The weighted average monthly cost for owner-occupier families is TZS 116,702.44 (US\$ 97.25), which corresponds to 13.7% of the average monthly family income of this category of PAP.

In the case of tenant residents that will relocate, rent and utility costs average TZS 128,500.01 per month (equivalent to US\$ 115.00) in the case of tenants in individual houses, and TZS 74,263.17 (US\$ 61.88) in the case of tenants in rented rooms or apartments. The weighted average monthly cost for tenant families is TZS 82,679.23 (US\$ 68.90), which represents 10.3% of the average monthly family income of this category of PAP.

It is worth noting that both in the case of owner-occupiers and tenants, monthly costs dependent on location represent a significantly lower percentage of total income than in the case of families to be affected along Phase I – Part A.

Table 3.1.f
Other Monthly Expenses Dependent on Location – Families to be Resettled

PAP	Type of residence:	Data	Total (TZS)	Answers	Average (TZS)
Owner-occupier of residence	Individual house	Sum of Water + Sewer	3.534.500,00	93	38.005,38
		Sum of Garbage Collection	230.800,00	93	2.481,72
		Sum of Energy	2.787.300,00	93	29.970,97
		Sum of Gas	2.593.000,00	93	27.881,72
		Sum of Land Rent	110.300,00	93	1.186,02
		Sum of Property Tax	256.000,00	93	2.752,69
	Room or apartment in individual house	Sum of Water + Sewer	1.850.500,00	30	61.683,33
		Sum of Garbage Collection	110.000,00	30	3.666,67
		Sum of Energy	1.260.000,00	30	42.000,00
		Sum of Gas	1.141.000,00	30	38.033,33
		Sum of Land Rent	314.000,00	30	10.466,67
		Sum of Property Tax	167.000,00	30	5.566,67
Residential tenants	Individual house	Cost of Rent	378.000,00	9	42.000,00
		Sum of Water + Sewer	423.000,00	9	47.000,00
		Sum of Garbage Collection	54.500,00	9	6.055,56
		Sum of Energy	78.000,00	9	8.666,67
		Sum of Gas	207.500,00	9	23.055,56
		Sum of Land Rent	3.000,00	9	333,33
		Sum of Property Tax	12.500,00	9	1.388,89
	Room or apartment in individual house	Cost of Rent	896.000,00	49	18.285,71
		Sum of Water + Sewer	879.500,00	49	17.948,98
		Sum of Garbage Collection	199.200,00	49	4.065,31
		Sum of Energy	473.000,00	49	9.653,06
		Sum of Gas	907.100,00	49	18.512,24
		Sum of Land Rent	-	47	-

Table 3.1.f - *continued***Other Monthly Expenses Dependent on Location – Families to be Resettled**

PAP	Type of residence:	Data	Total (TZS)	Answers	Average (TZS)
		Sum of Property Tax	272.500,00	47	5.797,87
	Upper floor unit	Cost of Rent	250.000,00	1	250.000,00
		Sum of Water + Sewer	50.000,00	1	50.000,00
		Sum of Garbage Collection	2.000,00	1	2.000,00
		Sum of Energy	30.000,00	1	30.000,00
		Sum of Gas	50.000,00	1	50.000,00
		Sum of Land Rent	-	1	-
		Sum of Property Tax	-	1	-

With regards to affected businesses, the Socioeconomic Questionnaire for Commercial Activities was applied on 179 out of 212 affected establishments. Total number of employees in those 179 establishments reached 419 as shown in **Table 3.1.g** below. This total does not include business owners as such. 56.8% are registered employees. On average, affected businesses have 2.34 employees. Considering the total number of commercial establishments to be affected (212), the average of 2.34 employees per establishment would result in a total of 496 employees.

Table 3.1.g

Employees by Category in Affected Commercial Establishments

Business Category	Affected Commercial Units		Affected Employees		
	Applied Questionnaires / Total PAP	%	Type of work contract	Total	%
Owner of business in own premises that will reinstall in same plot	4 / 7	2.2%	Informal	1	0.2%
			Registered employee	22	5.3%
Total				23	5.5%
Owner of business in rented premises that will reinstall in same plot	4 / 5	2.2%	Informal	2	0.5%
			Registered employee	10	2.4%
			Self employed	1	0.2%
Total				13	3.1%
Owner of business in own premises that will relocate	51 / 56	28.5%	Informal	26	6.2%
			not informed	15	3.6%
			other	12	2.9%
			Registered employee	49	11.7%
			Self employed	5	1.2%
Total				107	25.5%

Table 3.1.g – *continued...***Employees by Category in Affected Commercial Establishments**

Business Category	Affected Commercial Units		Affected Employees		
	Applied Questionnaires / Total PAP	%	Type of work contract	Total	%
Owner of business in rented premises that will relocate	120 / 144	67.0%	Informal	33	7.9%
			not informed	6	1.4%
			other	61	14.6%
			Registered employee	157	37.5%
			Self employed	19	4.5%
Total				276	65.9%
Grand Total	179 / 212	100.0%	Grand Total	419	100.0%

With regards to means of transport to work, **Table 3.1.h** below summarizes information for the 179 interviewed businesses. This information covers 579 persons, since it includes employers (160) as well as employees (419). As can be seen, 68.22% reach work by daladala, whereas only 8.45% does so by car or motorbike. A significant number of employees walk to work (9.67% of the sample).

Table 3.1.h

Means of Transport to Work – Commercial Establishments

PAP	Means of Transport to Work - Commercial Units	Total	%
Owner of business in own premises that will reinstall in same plot	1= walking	1	3,7%
	3= daladala	22	81,5%
	4= car/ motorcycle	4	14,8%
Total		27	100,0%
Owner of business in rented premises that will reinstall in same plot	3= daladala	13	76,5%
	4= car/ motorcycle	4	23,5%
Total		17	100,0%
Owner of business in own premises that will relocate	0= none	49	31,6%
	1= walking	10	6,5%
	3= daladala	86	55,5%
	4= car/ motorcycle	10	6,5%
Total		155	100,0%
Owner of business in rented premises that will relocate	0= none	29	7,6%
	1= walking	45	11,8%
	3= daladala	274	72,1%
	4= car/ motorcycle	31	8,2%
	Not Informed	1	0,3%
Total		380	100,0%
Grand Total		579	

As can be seen in **Table 3.1.i** below, the average monthly sales of commercial establishments varies most significantly as a function of the type of building occupied (i.e. independent commercial building vs. kiosk). Variation as a function of category of PAP is less significant, though it is important to point out that sales of businesses occupying own premises are significantly larger than those of businesses in rented premises.

With regards to average monthly profit, this pattern is not so clear. One aspect to highlight is that monthly profits of businesses in kiosks attached to buildings range between TZS 500.000,00 and 550.000,00 for both owner-occupied and rented facilities (US\$ 416 to US\$ 458). Profits of businesses in rooms within residential buildings are even smaller, in the range of TZS 260.000,00 to 350.000,00 (US\$ 217 to US\$ 292).

Table 3.1.i
Average Monthly Sales and Profit – Commercial Establishments

PAP	Property Type	Total	Average monthly sales per PAP category (TZS)	Average monthly sales per establishment (TZS)	Average net monthly profit per PAP category (TZS)	Average net monthly profit per establishment (TZS)
Owner of business in own premises that will reinstall in same plot	No answer (property type not identified)	4	214.910.000,00	53.727.500,00	10.340.000,00	2.585.000,00
	Not interviewed	3		-		-
Total		7	214.910.000,00	53.727.500,00	10.340.000,00	2.585.000,00
Owner of business in rented premises that will reinstall in same plot	No answer (property type not identified)	4	29.500.000,00	7.375.000,00	7.000.000,00	1.750.000,00
	Not interviewed	1		-		-
Total		5	29.500.000,00	7.375.000,00	7.000.000,00	1.750.000,00
Owner of business in own premises that will relocate	Independent Commercial Building	1	30.000.000,00	30.000.000,00	4.000.000,00	4.000.000,00
	Kiosk attached to building	17	39.420.000,00	2.318.823,53	9.500.000,00	558.823,53
	Room in residential building	2	1.800.000,00	900.000,00	700.000,00	350.000,00
	Stand-Alone Kiosk	7	215.550.000,00	30.792.857,14	104.040.000,00	14.862.857,14
	No answer (property type not identified)	24	445.567.000,00	18.565.291,67	50.879.000,00	2.119.958,33
	Not interviewed	5	-	-	-	-
Total		56	732.337.000,00	14.359.549,00	169.119.000,00	3.316.058,82
Owner of business in rented premises that will relocate	Kiosk attached to building	62	572.352.000,00	9.231.483,87	31.079.000,00	501.274,19
	Room in residential building	1	400.000,00	400.000,00	260.000,00	260.000,00
	Stand-Alone Kiosk	8	28.700.000,00	3.587.500,00	6.780.000,00	847.500,00
	No answer (property type not identified)	49	155.089.667,00	3.165.095,24	29.152.789,70	594.954,89
	Not interviewed	24		-		-
Total		144	756.541.667,00	6.304.513,89	67.271.789,70	560.598,25
Grand Total		212	1.733.288.667,00	n.a.	253.730.789,70	n.a.

NOTE: Grand average of profits for all categories is not considered a relevant indicator due to diversity.

3.2

Other Project Affected People

Other categories of PAP which may be eligible for specific compensation and/or assistance under the terms of Section 5.0 of the Resettlement Policy Framework issued for the DART project in February 2007, and which are present in Phase I – Part B, include the following:

- Owners of businesses (in either own or rented premises) that will suffer provisional closure or provisional loss of business during construction.
- Authorized non-transient street vendors.
- Daily transient vendors.
- Owners of businesses to be indirectly affected by the project.
- Other informal users of the right-of-way to be relocated.

These categories of PAP were not subject to application of the Socioeconomic Questionnaires. However, they were identified and registered in the Detailed Land-Use Survey developed as part of the project's Environmental and Social Impact Assessment (ESIA).

Along the final segment of Morogoro Road there is a concentration of small businesses, small restaurants, some residential units, and sparse vehicle-related services and petrol stations. Daily transient street vendors at this segment are concentrated mostly near the daladala stops and areas of commercial activity, whereas non-transient street vendors are concentrated in consolidated commercial areas.

Transient street vendors will not be indemnified financially but will be assisted with relocation to areas established by the City Council. Authorized non-transient street vendors may be eligible for disturbance allowance as well as relocation assistance. Though very sparse, a large variety of minor kiosks within the right-of-way remain in place overnight and were identified in the ESIA's Detailed Land-Use Survey. Along the Morogoro Road segment of Phase I – Part B this survey identified 82 non-transient street vendors within the right-of-way, as well as 166 daily transient vendors.

Indirectly affected businesses and others informally using available space within the right-of-way are not eligible for financial indemnification under the terms of the RPF, but may be eligible for relocation search assistance. In the Morogoro Road segment of Phase I – Part B, this category of PAP includes 19 businesses which use the right-of-way for exposure of goods for sale and/or restaurants that put chairs and tables on the street, as well as 7 taxi waiting spots, 6 construction truck parking areas and 1 daladala parking area.

Along the Msimbazi Street segment, 108 daily transient street vendors were identified in the ESIA land-use survey, mostly concentrated at Uhuro St and Msimbazi St junction (34%), and near the railway (19%). Non-transient street vendors are concentrated along the railway and at a nearby area exclusively designated for them and will not need to be relocated. Due to lack of left-over spaces in this street, there is not a significant number of commercial activities using the right-of-way for exposure of goods. Only 3 commercial establishments and 1 restaurant do so. Along this segment, there is also 1 taxi waiting spot and 2 daladala parking areas.

Along the Kawawa Road segment commercial activities concentrate between the Sinza River and Alli Hassan Mwiny Road. Most of the 46 daily transient street vendors and 9 non-transient vendors in this segment concentrate in this area. There are also 14 business establishments as well as 2 restaurants that use the right-of-way for exposure of goods. Finally, 5 taxi waiting spots, 2 construction truck parking areas and 1 daladala parking area are distributed along the Kawawa Road segment.

Businesses that will suffer provisional closure or provisional loss of business during construction will only be identified during the construction phase, as they put forward their claims for compensation in the terms established in the Resettlement Policy Framework. The grievance redress mechanism described in Section 6.0 of the RPF (and detailed in Section 6.2 of this RAP), will be the procedure for reception and evaluation of all such claims.

3.3

Vulnerable PAP and others Requiring Special Assistance

Identification of vulnerable PAP and others requiring special assistance was contemplated in the Residential Socioeconomic Questionnaire. Of the 228 families to be affected by Phase I – Part B, 20 declared having members with special needs, but the type of disability was not informed.

4.0

RAP Budget for Phase I – Part B

4.1

Property Indemnification and Resettlement Allowances Under the Terms of Tanzanian Law

4.1.1

Property Valuation and Allowance Allocation Criteria

The certified valuer responsible for property valuations in this RAP verified market values for land along all of the DART Phase I – Part B alignment, as well as replacement costs for the various types of buildings and improvements that will be affected by the project. The results of this study, as applicable to Phase I – Part B, are consolidated in **Table 4.1.1.a** below. As can be seen, the main difference with regards to compensation values established for Phase I – Part A is the price of unbuilt land. The same value ranges for replacement costs of existing structures were adopted on Parts A and B.

The procedure adopted for inspection and valuation of affected properties followed closely the methodology established in Section 8.0 of the RPF. Values in the table below were applied to land where legally occupied, and to buildings and improvements, on the basis of quantities that were calculated during inspection in the presence of the affected party.

Resulting valuations were then included in the Proposed Compensation Schedule (ANNEX 01) to be submitted to the City's Chief Valuer for approval. In this process, the Chief Valuer may request adjustments as deemed fit.

Table 4.1.1.a
Compensation Value Reference Table for Phase I – Part A

SN	Replacement item	Unit	Compensation rate in TZS	
1	a	Land, unbuilt, CBD (Gerezani in Ilala Ward)	Meter square	50,000
	b	Land, unbuilt, planed area other than CBD (Mwananyamala and Kinondoni)	Meter square	35,000
	c	Land, unbuilt, planed area other than CBD (Hannanasifu)	Meter square	30,000
	d	Land, unbuilt, unplanned area	Meter square	10,000
2	a	Building, framed structure, concrete blocks infill, high quality finishing and cervices	Meter square	400,000-600,000
	b	Building, block construction, high quality finishing	Meter square	200,000-300,000
	c	Building, block construction, poor quality finishing	Meter square	150,000-200,000
	d	Building constructed of mud and poles, ion sheet roof, sand cement screed floor	Meter square	100,000-150,000
	e	Building constructed of mud and poles, ion sheet roof, compacted earth floor	Meter square	50,000-100,000
3	a	Kiosk, or storage room attached to building structure, good construction quality/material	Meter square	100,000
	b	Kiosk, or storage room attached to building structure, of medium construction quality/material	Meter square	80,000
	c	Kiosk, or storage room attached to building structure, of poor construction quality/material	Meter square	60,000
4	a	Verandah, concrete slab roof, cement screed floor	Meter square	80,000
	b	Verandah, corrugated iron sheet roof, cement screed floor	Meter square	60,000
5	a	Steps, floor tiles finishing	Meter square	
	b	Steps, sand cement screed finishing	Meter square	50,000
6	a	Parking space, paved blocks floor finishing	Meter square	40,000
	b	Parking space, concrete slab floor finishing	Meter square	60,000
7	a	Fence, reinforced concrete columns, concrete blocks infill with grill	Running meter	180,000
	b	Fence, concrete blocks with grill	Running meter	150,000
	c	Fence, concrete blocks 4 courses	Running meter	80,000
	d	Fence, barbed wire on concrete poles	Running meter	60,000
	e	Fence, other materials (makuti)	Running meter	10,000
	f	Hedge	Running meter	10,000

SN		Replacement item	Unit	Compensation rate in TZS
8	a	Inspection chambers	Piece	80,000
	b	Standard septic tank	Piece	1,000,000
9	a	Storage tanks, plastic, over head on concrete tower	1000-10,000 litres	800,000
	b	Storage tanks, plastic, over head on metal pole tower	1000-10,000 litres	500,000
	c	Storage tanks, concrete underground	1000-10,000 litres	1,000,000
10	a	Pumps, gasoline, manual	Piece	2,500,000
	b	Pumps, gasoline, electronic	Piece	5,000,000

Further to land and building compensation, regulations of Land Act N° 4 of 1999, made under Section 179 of the Land Regulations which became operational in May 2001, contemplate several complementary compensation allowances for which expropriated parties are eligible. These include:

- Disturbance allowance;
- Transportation allowance;
- Loss of profit or accommodation allowance;
- Accommodation allowance.

Disturbance Allowance is payable as a percentage of property valuation in compliance to the provisions of Act N° 4 of 1999. The percentage is the average commercial bank rates offered on fixed deposits during 1 year. In the RPF it was established that a 4% per year interest rate will be adopted.

Transport allowance is computed on the basis of market rates for transportation services within the project area, considering the average cost of transporting 12 tons over a distance of 20 Km. In the RPF this cost was set at TZS 120,000.00.

Regulations of Land Act N° 4 of 1999 require that accommodation allowance be calculated on the basis of monthly rent multiplied by 36 months. Monthly rent is equivalent to the market rental value of the affected part of the property per month and was calculated by the certified valuer on the basis of market data on rental values along the Phase I – Part B alignment.

Loss of Profit Compensation is calculated on the basis of 36 months of net profit where audited accounting records. Where these records are absent, a fixed assumption of TZS 90,000.00 was adopted as established in the RPF.

All these allowances were calculated by the certified valuer on a case by case basis and have been included in the Proposed Compensation Schedule to be submitted to the City's Chief Valuer (ANNEX 01).

4.1.2

Summary of Compensation Schedule and Budget Summary

Table 4.1.2.a below summarizes the main figures of the Proposed Compensation Schedule by Ward and Municipality as well as by type of compensation or allowance. As can be seen, most of the RAP cost associated to legally established compensation and assistance results from Phase I – Part B interferences in Ilala.

Table 4.1.2.a

Compensation Schedule Summary by Ward (values in TZS)

Kinondoni					
Location (Ward)	Value of Building	Accommodation Allowance	Transport Allowance	Disturbance Allowance	Loss of Profit
Hananassif	189.928.800,00	9.720.000,00	5.040.000,00	80.416.944,00	77.760.000,00
Kimara	730.740.300,00	133.267.500,00	17.160.000,00	-	246.995.640,00
Magomeni	295.279.450,00	14.976.000,00	6.240.000,00	131.071.636,00	68.040.000,00
Mwananyamala	118.304.600,00	-	720.000,00	127.321.728,00	-
Ubungo	150.581.500,00	-	3.360.000,00	1.603.200,00	55.080.000,00
Total	1.484.834.650,00	157.963.500,00	32.520.000,00	340.413.508,00	447.875.640,00
Ilala					
Location (Ward)	Value Of Building	Accommodation Allowance	Transport Allowance	Disturbance Allowance	Loss of Profit
Gerezani	5.786.031.500,00	27.120.000,00	47.169.600,00	2.730.538.320,00	252.720.000,00
Jangwani	31.428.000,00	-	240.000,00	-	36.000.000,00
Kariakoo	22.546.000,00	14.850.000,00	480.000,00	10.822.080,00	6.480.000,00
Total	5.840.005.500,00	41.970.000,00	47.889.600,00	2.741.360.400,00	295.200.000,00

Table 4.1.2.b analyzes total compensation proposed by the valuer to the 228 families to be affected, according to value ranges. An aspect to be highlighted is that over 80% of owner-occupier families will receive compensation between TZS 50.000.000,00 and 200.000.000,00 (US\$ 41.666 and US\$ 166.667).

As can be seen, almost 97% of residential tenants will receive under TZS 5,000,000.00, equivalent to US\$ 4,167.00. This is also the case of 5 of the 135 owner-occupier families to be affected.

A similar analysis is conducted for commercial properties to be affected in **Table 4.1.2.c**. Here, it is worth highlighting that 144 out of 149 commercial tenants (almost 97%) will receive total compensation of under TZS 5,000,000.00 (US\$ 4,167.00). On the other hand,

51 out of 63 commercial establishments in own premises (almost 81%) will receive under TZS 15,000,000.00 (US\$ 12,500.00).

Table 4.1.2.d analyzes total compensation value ranges for non-resident landlords and unidentified PAPs. Average compensation for non-resident landlords is proposed at TZS 27,705,106.00, equivalent to US\$ 23,088.00.

Table 4.1.2.b
Statistical Analysis – Residential Compensation and Allowances

Indemnification Value	Residential Owners	Total (TZS)	%
TZS 250,000.00 to TZS 5,000,000.00	5	19.932.634,00	3.7%
TZS 5,000,000.00 to TZS 15,000,000.00	4	46,453,900.00	3.0%
TZS 15,000,000.00 to TZS 50,000,000.00	16	494.772.850,00	11.9%
TZS 50,000,000.00 to TZS 200,000,000.00	109	8.860.893.226,00	80.7%
TZS 200,000,000.00 to TZS 650,000,000.00	1	202.428.040,00	0.7%
Total	135	9.624.480.650,00	100%
Indemnification Value	Residential Tenants	Total (TZS)	%
TZS 250,000.00 to TZS 5,000,000.00	90	76.296.000,00	96.8%
TZS 5,000,000.00 to TZS 15,000,000.00	3	18.240.000,00	3.2%
TZS 15,000,000.00 to TZS 50,000,000.00	0	-	0%
TZS 50,000,000.00 to TZS 200,000,000.00	0	-	0%
TZS 200,000,000.00 to TZS 650,000,000.00	0	-	0%
Total	93	94.536.000,00	100%

Table 4.1.2.c
Statistical Analysis – Commercial Compensation and Allowances

Indemnification Value	Commercial Owners	Total (TZS)	%
TZS 250,000.00 to TZS 5,000,000.00	31	99.138.000,00	49.2%
TZS 5,000,000.00 to TZS 15,000,000.00	20	156.081.298,00	31.7%
TZS 15,000,000.00 to TZS 50,000,000.00	9	267.656.276,00	14.3%
TZS 50,000,000.00 to TZS 200,000,000.00	3	214.796.600,00	4.8%
TZS 200,000,000.00 to TZS 650,000,000.00	0	-	0%
Total	63	737.672.174,00	100%
Indemnification Value	Commercial Tenants	Total (TZS)	%
TZS 250,000.00 to TZS 5,000,000.00	144	483.840.000,00	96.7%
TZS 5,000,000.00 to TZS 15,000,000.00	3	20.160.000,00	2.0%
TZS 15,000,000.00 to TZS 50,000,000.00	2	56.075.640,00	1.3%
TZS 50,000,000.00 to TZS 200,000,000.00	0	-	0%
TZS 200,000,000.00 to TZS 650,000,000.00	0	-	0%
Total	149	560.075.640,00	100%

Table 4.1.2.d
Statistical Analysis – Compensation and Allowances for Non-Resident Landlords and Unidentified PAPs

Indemnification Value	(property use not identified)	Total (TZS)	%
TZS 250,000.00 to TZS 5,000,000.00	4	13.886.776,00	80%
TZS 5,000,000.00 to TZS 15,000,000.00	1	11.510.080,00	20%
TZS 15,000,000.00 to TZS 50,000,000.00	0	-	0%
TZS 50,000,000.00 to TZS 200,000,000.00	0	-	0%
TZS 200,000,000.00 to TZS 650,000,000.00	0	-	0%
Total	5	25.396.856,00	100%
Indemnification Value	Landlord	Total (TZS)	%
TZS 250,000.00 to TZS 5,000,000.00	5	16.851.000,00	35.7%
TZS 5,000,000.00 to TZS 15,000,000.00	1	14.610.680,00	7.1%
TZS 15,000,000.00 to TZS 50,000,000.00	6	202.190.598,00	42.9%
TZS 50,000,000.00 to TZS 200,000,000.00	2	154.219.200,00	14.3%
TZS 200,000,000.00 to TZS 650,000,000.00	0	-	0%
Total	14	387.871.478,00	100%

Total compensation proposed in the Property Compensation Schedule in **ANNEX 01** is summarized in **Table 4.1.2.e**. As can be seen, the final figure amounts to TZS 11,430,032,798.00 (US\$ 9,525,027.00). This value is significantly larger than the value proposed for Phase I – Part A which totaled US\$ 4,466,111.00.

Table 4.1.2.e
General Summary of Proposed Compensation Schedule

Component	Resettlement Cost (TZS)	US\$
Land and Building	7.324.840.150,00	6,104,033.00
Accommodation Allowance	199.933.500,00	166,611.00
Transport Allowance	80.409.600,00	67,008.00
Disturbance Allowance	3.081.773.908,00	2,568,145.00
Loss of Profit	743.075.640,00	619,230.00
Total	11.430.032.798,00	9,525,027.00

4.2 Complementary Compensation Entitlements under the Term of the DART Resettlement Policy Framework

4.2.1 Eligible PAP per Compensation Measure

Compensation and allowances under the terms of Tanzanian law, as calculated in the Proposed Compensation Schedule (**ANNEX 01**) and summarized in Section 4.1.2 above, may not be fully compatible with World Bank OP 4.12 requirements and hence a series of complementary compensation and assistance measures was established in the RPF (Section 5.2) to ensure that the situation of all PAP is adequately addressed.

Eligibility of Phase I – Part B affected people with regards to each one of those complementary measures is established below:

Property compensation bonus

Property Compensation Bonus is meant to assist owner-occupiers of houses and businesses that will need to relocate and will be compensated only for buildings and improvements, but not for land since this is not regularly occupied. The compensation bonus is meant to bridge the gap between market value of equivalent substitute properties and the amount to be received as compensation as per the certified valuer's valuation of buildings and improvements.

As can be seen in **Table 3.1.a**, a total of 131 owner-occupier families of totally affected residences will need to relocate. Of these, 109 will be compensated for land and 22 will not.

Similarly, 56 owners of businesses that occupy their own premises will be located and most of them (45) will not be compensated for land.

Thus, 69 "entities", including 22 families and 45 businesses, will be eligible for *Property Compensation Bonus* in Phase I – Part B.

Complementary transition allowance

Complementary Transition Allowance will be forthcoming where PAP can demonstrate that the transitional costs of relocation were higher than the transition assistance allowances received as per the Compensation Schedule to be approved by the City's Chief Valuer.

As highlighted in the RPF, this complementary allowance is not redundant or in conflict with the grievance redress procedure. Grievance redress will take place prior to compensation and resettlement. The complementary transition allowance is an *ex-post* procedure, where the affected party is allowed the chance to prove that his transitional costs were higher than assessed and consequently receives complementary compensation.

Eligibility for this measure is likely to be limited to the 131 owner-occupier families and 56 owner-occupier businesses that will need to resettle; and to a lesser extent, to the 93 residential tenants and 144 commercial tenants that will also relocate.

Complementary allowance for additional cost of living

The 131 owners-occupier families of residences that will need to relocate may be eligible for *Complementary Allowance for Additional Cost of Living* at the new place of residence. As seen in **Tables 3.1.e** and **3.1.f**, additional transport costs are not likely to be an issue, but additional land rent, tax and utility fees may be. These expenses represent, on average, 13.7% of total family income.

Tenant unexhausted improvements compensation

All affected tenants will in principle be eligible for *Tenant Unexhausted Improvements Compensation* where they can demonstrate that they implemented improvements on the basis of multi-year rental agreements which will be cut short as a result of Phase I – Part B implementation. As can be seen in **Table 3.1.a**, this complementary measure may benefit 149 commercial tenants and 93 residential tenants.

First refusal rights over future rented commercial space in terminals and feeder stations

All businesses that will need to relocate, whether in rented or owner-occupied premises, will be eligible for this measure. This will include a total of 200 commercial establishments (**Table 3.1.a**).

As mentioned in the RPF, this entitlement may be extended, subject to availability of shops or boxes, to businesses that will be partially affected by Phase I – Part B but will not need to relocate.

Moving assistance

The RPF establishes that *Moving Assistance* will be provided for PAPs that will incur in moving costs but are not eligible for transportation allowance, or in cases where said transportation allowance is clearly insufficient.

This may include larger commercial establishments that will relocate. However, none of the establishments that will relocate as a result of Phase I – Part B implementation are likely to be eligible for this measure (i.e. none will receive total compensation above TZS 200,000,000.00 as per **Table 4.1.2.c**).

Authorized street vendors to be relocated will also be eligible for *Moving Assistance*. On the basis of the Detailed Land Use Survey included in the Environmental and Social Impact Assessment, their number in Phase I – Part B is estimated at 91 (see Section 3.2 above).

Similarly, indirectly affected businesses having to relocate as a result of altered conditions (basically the 39 establishments that currently make informal use of Phase I - Part B right-of-way), may also be eligible for *Moving Assistance* to the extent they do in fact relocate.

Search assistance

All owners and tenants of houses and businesses that will be totally affected and will need to relocate will be assisted in searching alternative locations. Thus, a total of 224 families and 200 commercial establishments will be eligible for *Search Assistance* (**Table 3.1.a**).

Legal assistance for purchase

All property owners that will need to purchase alternative property will be assisted during contract negotiations in order to insure that proper terms are subscribed. As per **Table 3.1.a**, PAP eligible for *Legal Assistance* in Phase I – Part B will include 131 owner-occupier families and 56 commercial establishments currently occupying their own premises.

Vulnerable PAP special support

Results of the Socio-Economic Survey Residential Questionnaires (**ANNEX 03**) indicate that 20 of the 228 families to be affected declared have members needing special assistance. Further to these, should any previously uninformed handicapped or otherwise vulnerable persons be directly affected, *Special Support* shall be forthcoming as pertinent.

Unproductive labor compensation

Commercial activities whose level of business comes to be affected as a result of interference provoked by Phase I – Part B construction may claim Loss of Profit Compensation under the terms of Tanzanian law and will be compensated accordingly.

Unproductive Labor Compensation is a complementary measure that may apply in the case of commercial activities that are temporarily closed or suffer very significant loss of business during construction, but which intend to continue in business after the construction phase is over. This measure contemplates payment of wages of all or part of the establishments' employees for up to three months, in order to avoid unnecessary lay offs.

The quantity of commercial establishments eligible for *Unproductive Labor Compensation* cannot be known at this stage. However, it is possible to foresee that most cases are likely to occur along the Msimbazi Street segment of Phase I – Part B.

Income restoration assistance

Loss of employment may occur as a result of displacement of businesses, temporary loss of sales, or temporary closure of businesses. Only in the case of displaced businesses is it possible to estimate number of potentially affected employees. As per **Table 3.1.a**, 200 businesses will need to relocate. As per **Table 3.1.g**, these establishments have 2.34 employees on average (besides the owner). Furthermore, it is not unlikely that some loss of employment will also occur at the 7 commercial establishments that will be partially affected but will re-install in the same property.

Based on the above, it seems reasonable to assume that up to between 400 and 450 persons may be eligible for *Income Restoration Assistance* during Phase I – Part B.

Relocation assistance for informal users of the right of way

As indicated in Section 3.2, there are several types of PAP that make use of the right-of-way to be occupied by DART Phase I – Part A, whether authorized or not. These include non-transient street vendors, taxi spots, daladala parking areas, construction truck waiting areas, and others.

Based on the Detailed Land-Use Survey conducted as part of the project's Environmental and Social Impact Assessment, it is estimated that 320 non-transient vendors, 13 taxi spots, 8 truck parking areas and 04 daladala parking area will need to relocate as a result of Phase I – Part B. All these PAP will not be indemnified but

will be eligible for *Relocation Assistance* which shall result in the definition of alternative locations where affected activities will be allowed to continue in a legally regular manner, as established in the RPF.

Indirectly affected commercial relocation assistance

Abutting businesses informally occupying the right-of-way to be occupied by DART Phase I – Part B will not be able to continue doing so. On the basis of the Detailed Land Use Survey conducted as part of the project's Environmental and Social Impact Assessment (see Section 3.2 above), it is estimated that 39 such commercial establishments will be affected by Phase I – Part B.

Many of these establishments may find it unfeasible to continue in the same location. Whereas these indirectly affected businesses will not be eligible for compensation since property as such will not be affected, they will all be eligible for *Commercial Relocation Assistance*, which will include assistance with the search for alternative locations, as established in the RPF.

4.2.2

Estimated Budget for Complementary Compensation

As established in the RPF, a Resettlement Planning and Implementation Team (RPIT) will be structured in order to ensure that Resettlement Action Plans are properly implemented. The RPIT's required staffing for Phase I – Part A was established in the corresponding Resettlement Action Plan. Since it is assumed that Part A and Part B will be implemented simultaneously, some additional staff will need to be considered. Required additional staff and associated costs are detailed in Section 4.3.3. Only RPIT costs additional to those already included in the consolidated budget for RAP Phase I – Part A are being considered in the consolidated budget for RAP Phase I – Part B (Section 4.4).

This section includes an estimate of the budget necessary for implementation of the Complementary Compensation Entitlements described in Section 4.2.1 above. It is important to note that not all complementary entitlements will imply in additional RAP implementation costs, since some consist basically of assistance to affected parties to be provided by the RPIT team.

Table 4.2.2.a summarizes the budget for Complementary Compensation Entitlements. The basis for calculations for each entitlement is presented below:

Property compensation bonus

There are 22 residential and 45 commercial PAP's eligible for this measure. Based on the Compensation Value Reference Table (**Table 4.1.1.a**), an average land market price of TZS 32,000.00 per m² was adopted and an average complementary compensation area of up to 120 m² was considered for each residential PAP and 30 m² for each commercial PAP, totaling TZS 127,680,000.00 (US\$ 106,400.00).

Complementary transition allowance

The budget for this entitlement considered that, at most, up to 125 PAP may demonstrate eligibility. The average *Complementary Transition Allowance* to be awarded was estimated at TZS 1,000,000.00 per PAP, which represents an up to 20% increase over average Disturbance and/or Loss of Profit allowances in the Proposed Compensation Schedule (**ANNEX 01**).

Complementary allowance for additional cost of living

The average cost of land rent, property taxes and utility fees for owner-occupier families to be resettled, is TZS 116,702.44 per month (**Table 3.1.f**). The budget for this item considers payment of a complementary allowance equivalent to up to 30% of this cost during 24 months (period established in the RPF), to all 131 families potentially eligible.

Tenant unexhausted improvements compensation

The budget for this item admits that up to 10% of potentially eligible tenants will receive this entitlement (i.e. 24 out of a total of 242 residential and commercial tenants). The average cost of *Tenant Unexhausted Improvements Compensation* was assumed at TZS 600,000.00 on the basis of the values in **Table 4.1.1.a** (Compensation Value Reference Table).

First refusal rights over future rented commercial space in terminals and feeder stations

No specific budget has been allocated to this entitlement since its implementation will depend on RPIT time and commercial space at stations and terminals is already included in the project's investment cost.

Moving assistance

The average cost of this type of *Moving Assistance* was estimated at TZS 90,000.00, which is 75% of the transportation allowance calculated by the certified valuer as per Land Act N° 4 of 1999. On the basis of Section 4.2.1 above, it was assumed that up to 100 PAP will receive this assistance.

Search assistance, legal assistance for purchase and vulnerable PAP special support

These three entitlements will imply in no costs additional to those allocated to the RPIT in Section 4.3.3.

Unproductive labor compensation

Potential costs associated to this entitlement are very difficult to estimate. The summary budget in **Table 4.2.2.a** follows the same criteria adopted in the RAP for Phase I – Part A and considers an allowance of TZS 14,400,000.00 which is roughly equivalent to 100 man-months of wages for assistant employees in retail establishments.

Income restoration assistance

A cost of TZS 160,000.00 per person for training and outplacement assistance was considered. Total number of PAP to receive this entitlement was assumed to be up to 250 on the basis of the analysis contained in Section 4.2.1.

Relocation assistance for informal users of the right of way and indirectly affected commercial relocation assistance

These two entitlements should imply in no additional cost since they basically consist of assistance to be provided by members of the RPIT.

Application of the criteria specified above resulted in a total budget for Complementary Compensation Entitlements of TZS 440,671,440.00 (US\$ 367,226.00) as shown in **Table 4.2.2.a** below. This amount represents 3.86% of the total amount allocated in the Proposed Compensation Schedule (**ANNEX 01**) and summarized in **Table 4.1.2.e**.

Table 4.2.2.a
Summary of Complementary Compensation Budget

Complementary Entitlement	Allocated Budget (TZS)
Property compensation bonus	127.680.000,00
Complementary transition allowance	125.000.000,00
Complementary allowance for additional cost of living	110.071.440,00
Tenant unexhausted improvements compensation	14.520.000,00
First refusal rights over future commercial space	---
Moving assistance	9.000.000,00
Search assistance	---
Legal assistance for purchase	---
Vulnerable PAP special support	---
Unproductive labor compensation	14.400.000,00
Income restoration assistance	40.000.000,00
Relocation assistance for informal users of the right of way	---
Indirectly affected commercial relocation assistance	---
TOTAL	440.671.440,00

4.3 Other RAP Implementation Costs

4.3.1 Consultation and Disclosure Costs

Consultation and disclosure will be conducted by the RPIT, including implementation of the grievance redress mechanism during pre-construction and construction phases. Thus, most costs associated to this activity are already included in the Implementation Management and Monitoring Budget included in Section 4.3.3 below.

Furthermore, a consultation and disclosure budget has already been considered in the RAP for Phase I – Part A (TZS 24,000,000.00). In view of this, a complementary budget of only TZS 8,000,000.00 (US\$ 6,667.00) is being considered for consultation and disclosure in the RAP Phase I – Part B consolidated budget.

4.3.2 Property Valuation Adjustment Allowance

As described in Section 6.3 below, all PAP will have the opportunity to question proposed valuation and allowances as approved by the City's Chief Valuer. This may result in alterations of the Compensation Schedule and an allowance of TZS 571,500,000.00, equivalent to 5% of the total value in the Proposed Compensation Schedule (ANNEX 01), has been allocated to this effect.

4.3.3

RAP Implementation Management and Monitoring Costs (Resettlement Planning and Implementation Team - RPIT)

RAP implementation management and monitoring costs will be associated mostly to the Resettlement Planning and Implementation Team (RPIT) wages and social benefits, plus fixed and variable office costs and expenses. Staffing established in the RPF, as calculated in the consolidated budget for the RAP of Phase I – Part A, resulted in a total RPIT budget estimate of TZS 170,200,000.00 (including post-resettlement monitoring costs). Since Phase I – Parts A and B are to be implemented simultaneously, an additional 30%, or TZS 51,060,000.00, has been allowed for in the consolidated budget for Phase I – Part B.

4.3.4

Other Allowances

Further to the costs considered in sections 4.1.2, 4.2.2, 4.3.1, 4.3.2 and 4.3.3, allowance should be made for contingencies. Based on the level of detail of this RAP and on problems verified in part of the survey work conducted for it, it seems that a 7.0% allowance for contingencies is adequate.

4.4

Consolidated Budget for the RAP of DART Phase I – Part A

Table 4.4.a below presents the final budget for implementation of the RAP for DART Phase I – Part A.

Table 4.4.a
Consolidated Budget

Budget Component	Allocated Budget (TZS)
Proposed Compensation Schedule for Land, Buildings and Allowances (ANNEX 01)	11.430.032.798,00
Budget for Complementary Entitlements	440.671.440,00
RAP Consultation and Disclosure Costs	8.000.000,00
Property Valuation Adjustment Allowance	571.500.000,00
RAP Implementation Management and Monitoring Costs (RPIT)	51.060.000,00
Sub-Total	12.501.264.238,00
Contingencies (7.0%)	875.088.497,00
TOTAL	13.376.352.735,00

As already indicated, businesses that will suffer provisional closure or provisional loss of business during construction may request loss of profit compensation under the terms of Tanzanian law. They may do this through the grievance redress procedure established in this RAP. No specific allowance for such compensation has been included in this RAP's consolidated budget. Thus, it is assumed that the total amount of any such compensation will fall within the limits allowed for contingencies.

The total amount established above (TZS 13,376,352,735.00), equivalent to US\$ 11,146,961.00, is approximately 32% larger than the total budget estimated for this RAP in the Resettlement Policy Framework, which amounted to US\$ 8,418,000.00.

5.0 RAP Implementation Schedule

The implementation schedule presented herein assumes that Phase I – Parts A and B will be implemented simultaneously.

With the exception of general public disclosure of the Resettlement Policy Framework and activities necessary to structure the RPIT and the Grievance Committee, implementation of the RAPs for Parts A and B will effectively begin once the Proposed Compensation Schedules (**ANNEX 01**) are approved by the City's Chief Valuer. The implementation process will cover three phases:

- Pre-construction activities;
- Resettlement during construction;
- Post-resettlement monitoring.

The first two phases are expected to last up to twelve (12) months. The third phase will extend for another year as established in the RPF.

A Preliminary Schedule of activities is presented below, covering exclusively the first two of three phases above. The construction start-up date has been assumed to coincide with the finalization of public consultation meetings (as would be ideal). It is worth pointing out that this is merely indicative since the project allows for construction to begin in several segments that do not require expropriation while RAP implementation procedures are implemented elsewhere.

Thus, it is important to state that whereas the Schedule below indicates Construction Start-Up before PAP compensation and relocation is concluded, this does not mean that PAP will be required to move before they have been compensated. The RPIT will ensure that all PAP are properly compensated and assisted with relocation before work at the corresponding construction front effectively begins.

Preliminary Schedule

Activities	Months											
	1	2	3	4	5	6	7	8	9	10	11	12
General public disclosure of the RPF												
Finalization of key engineering details												
Structuring (recruitment, etc.) of the RPIT												
Implementation of the Public Information Centers)												
Approval of Comp. Schedule by Chief Valuer												
Approval of this RAP by DART PMU												
Implem. of 2 nd Phase of the Disclosure Plan												
Implementation of the Grievance Committee												
Operation of the Public Information Centers)												
Operation of the RPIT												
Public Consultation Meetings with PAP												
Construction Start-Up												
Grievance Redress												
Payment of compensations and allowances												
Moving, relocation and/or re-installation of PAP												
Demolition of affected properties												
Assistance to PAP and implementation monitoring												

6.0

Consultation and Disclosure during RAP Implementation

As indicated in the Resettlement Policy Framework, disclosure and consultation will be continuous during the resettlement planning and implementation process and will take place at two levels: the general level of the RPF and the specific level of each individual Resettlement Action Plan.

Consultation and disclosure of RPF principles and compensation packages will be initiated earlier, through official websites (Dar Es Salaam City Council, Kinondoni and Ilala municipalities, World Bank, other) as well as through media press releases and printed prospects. Information to be conveyed will focus on basic principles, eligibility criteria affecting entitlements and grievance redress procedures.

Whereas this initial disclosure of the RPF will take place prior to disclosure of the Phase I – Part B Resettlement Action Plan, the bulk of disclosure will be simultaneous.

Effective disclosure and consultation of the Resettlement Action Plans for Phase I – Parts A and B will begin after approval by the DART Project Management Unit (PMU). This will in turn take place only after the City's Chief Valuer approves the Proposed Compensation Schedule in **ANNEX 01**.

At the time of initiation of this 2nd Phase of disclosure, the Grievance Committee will have been established and will be operational, as will the two (02) Public Information Centers (PIC) proposed for the RAP for Phase I – Part A (one at a strategic location at the Central Business District and the other at Ubungo Terminal) and the additional PIC hereby proposed for Part B (at Kariakoo Terminal).

6.1

Specific DART Phase I Part B Consultation and Disclosure

Given that implementation of DART Phase I Parts A and B will be simultaneous, there will be a common Consultation and Disclosure process which, in strict accordance with the RPF, will contemplate the following:

Once the Compensation Schedules are approved by the City's Chief Valuer and the RAPs by the PMU, printed prospectus will be produced describing the DART project and explaining the resettlement procedure. Specific prospectus will be prepared for each main group of PAP categories. Thus, there will be prospectus for property owners, tenants, business owners, others. These prospectus will include information about:

- General description of the DART project and its benefits for Dar es Salaam;
- Valuation methods, including each RAP's Compensation Value Reference Table (**Table 4.1.1.a**);
- Complete specification of all entitlements;
- Detailed explanation of eligibility criteria;
- Detailed explanation of the grievance redress procedure;
- Place and time at which Public Information Meetings will be held;
- Contact instructions for the Government entity responsible for DART implementation (Dar Es Salaam City Council);
- Contact instructions for the Resettlement Planning and Implementation Team (RPIT);
- Contact instructions for the Grievance Committee;
- Address and working schedule of the three (03) Public Information Centers (PIC).

Printed prospectus will be distributed on the field and an effort will be made to deliver them personally to each affected party. Further to this, press releases on newspapers and other media will be programmed in order to ensure that the population at large is aware of the project's implementation schedule and of the places and dates at which Public Information Meetings will be held.

Three Public Information Centers (PIC) will be implemented: one at the Central Business District (precise location to be determined), one at Ubungo Terminal and the other at Kariakoo Terminal. These PICs will be staffed in order to properly attend affected PAP seeking information. Staff will be properly trained and the PICs will have at their disposal:

- Complete copies of the project's Environmental and Social Impact Assessment, the Resettlement Policy Framework and this Resettlement Action Plan;
- Illustrations and renderings of DART Phase I – Parts A and B;
- Detailed Expropriation Plans showing the limits to be affected on a property by property basis (following ANNEX 04 of this RAP);
- Detailed lay-out of stations and terminals, as necessary to allow for affected businessmen to assess the commercial spaces to be made available and over whose concession they may have first refusal rights;
- Complete list of PAP and of the entitlements each one is eligible for under the terms of each RAP;
- Copy of the specific Valuation Reports as approved.

All consultation to the Public Information Centers will be recorded in data banks and will be consolidated on a monthly basis and analyzed by the Resettlement Plan Implementation Team (RPIT). Based on results of this analysis, the contents of printed materials and/or press releases may be altered to clarify aspects which are not being clearly conveyed.

At least seven (07) Public Information Meetings will be held, three (03) of which aimed specifically at Phase I – Part B segments.

These meetings will convey detailed explanations of all entitlements, eligibility criteria and valuation methodologies. Similarly, grievance redress procedures will be explained. All meetings will be recorded and minutes summarizing the main aspects discussed will be produced and signed by City Council Representatives, the Ward Executive Officer and participating community leaders.

On the 31st of August 2007, consultation and disclosure of the RAP was initiated through news releases and display of the approved Compensation Tables at the DART office and at the Municipalities of Ilala and Kinondoni. THE RAP Report has also been made accessible to the public at the DART office, TANROADS and the two Municipalities. A report by the DART PMU on disclosure and consultation activities as of November 2nd, 2007, is included in ANNEX 06.

6.2

Grievance Redress during RAP Implementation

As legally required and established in Section 6.0 of the RPF, all PAP may question compensation and allowances allocated to them under this RAP. These claims will be addressed to a Grievance Committee that will be operational throughout the process starting prior to disclosure of individual resettlement and compensation plans, and will decide on the basis of the information at hand. A single Grievance Committee will address both Parts A and B.

Based on results of the property survey and socioeconomic survey, and taking into account the shortcomings of the data retrieved, it is expected that grievance related to resettlement compensation exercise for DART Phase I – Part B may arise principally from the following:

- Mistakes related to identification of affected property and people within the right-of-way;
- Disagreements on land and asset valuation;
- Disagreements on other compensation allowances.

As established in the RPF, all grievances will be treated according to the following schedule:

- Once received, any requests for additional information from the complainant will be issued within 15 days.
- The complainant will in turn have up to 30 days to provide the requested information, but may request for an additional period of up to 60 days on the basis of proper justification.
- Once full documentation is at hand, the Grievance Committee will notify the complainant of its decision within 20 days.

Once established, the Grievance Committee will meet weekly. However, this frequency may be increased or decreased depending on the amount of grievances at hand.

Other aspects of the grievance redress procedure, as specified in Section 6.0 of the RPF, will fully apply.

7.0

Monitoring and Reporting Plan

The DART Project Management Unit (PMU) will, under delegation from the Dar Es Salaam City Council, be responsible for RAP implementation as established in the RPF. In order to comply with this mandate, a Resettlement Planning and Implementation Team (RPIT) will be made operational with the scope of responsibilities established in Section 7.0 of the RPF. This will take place within one month of approval of the Compensation Schedule by the Chief Valuer and before initiation of disclosure and consultation as described in Section 6.0.

At the beginning of the RAP implementation process, the PMU will establish a set of indicators to facilitate assessment. As a minimum, these indicators will include:

- Grievance frequency, expressed as the percentage of valuations subject to formal questioning by PAPs.
- Grievance response, expressed as the percentage of grievance that were attended to vs. those whose claims were denied.
- Compensation payment progress, expressed both in terms of the percentage of PAPs that have been fully compensated up to a given date, and as the percentage of the total budget in the Compensation Schedule that has been effectively disbursed.

The RPIT will coordinate RAP implementation of DART Phase I – Parts A and B. This will include:

- Setting up and staffing the Public Information Centers (PIC) and training the personnel.
- Organization of the public meetings.
- Production of printed materials and implementation of all other disclosure activities.
- Ensuring proper participation of all affected peoples in the process.
- Assistance to each individual PAP in understanding corresponding valuations and complementary compensation.
- Identifying and dealing with problem cases.
- Participation in the Grievance Committee.
- Follow-up on grievance redress mechanisms and feedback to the interested PAPs.
- Follow-up on payment of compensation.
- Ensuring that all PAP receive all entitlements established for it in this RAP.
- Providing special assistance to vulnerable PAPs.
- Assistance with search for new locations in cases where relocation is necessary.
- Assistance with planning of re-installation within same plot.

- Coordination of other social assistance as per the matrix of eligibility established in this RAP.
- Monitoring of third party activities as they interfere with RAP implementation.
- Reporting as per the *Reporting Plan* described herein.
- Continuous monitoring and evaluation on the basis of pre-established indicators.
- Post-resettlement monitoring as established in the RPF.

The RPIT team will carry out inspections in every area where resettlement is taking place, with the frequency deemed necessary by the RPIT Manager. Since full documentation of all assistance provided is necessary, the key steps of each PAP relocation will need to be directly supervised.

Based on the fact Phase I Parts A and B will be implemented simultaneously, the RPIT will be staffed as follows:

- 1 Manager / coordinator
- 6 Social assistants during peak months, and 2 as a minimum during the complete process (until construction end).
- 1 Real estate specialist (during the initial months and as necessary to assist all PAPs that will relocate in finding alternative locations).
- 1 Outsourcing specialist during 5 to 8 months.
- 1 Data bank administrator during all the implementation process.
- 2 Administrative assistants during all the implementation process.

Staffing of the RPIT for post-resettlement monitoring will be limited to one (01) social assistant on a full-time basis, reporting directly to the PMU.

The RPIT Manager will be responsible for all procedures in the RAP. However, payment of compensation and allowances as per the Property Valuation and Allowance Schedule approved by the Chief Valuer will be paid directly by local authorities. The RPIT Manager will interact with authorities responsible for payment as necessary to program subsequent assistance measures.

The RPIT Manager will also administer the budget allocated in the RAP for implementation of the complementary compensation and assistance measures established in Section 4.2 herein. In cases where eligibility is based on verified results of RAP implementation, he will decide which PAP should receive such benefits on the basis of RPF principles and in consultation with the PMU.

The RPIT Manager will also interact with the responsible construction company in order to ensure that premises are only taken over after payment of compensation and after the respective PAP has been able to dismantle and remove reusable assets, except where the time limit allowed for this has been exceeded.

RPIT Social Assistants will be in charge of day-to-day supervision of RAP implementation. They will provide special support to vulnerable peoples and will assist families and businesses as necessary to ensure that applicable entitlements are properly delivered. If deemed necessary, the RPIT Manager may request recruitment of additional Social Assistants during critical periods of the resettlement process.

The real estate specialist will constitute a databank of eligible properties in order to facilitate PAP relocation. He will provide general search assistance and will also assist with review of property legal documentation in order to ensure that PAPs resettle into legally regular buildings as applicable.

The outsourcing specialist will assist employees of businesses that will be laid off, in the process of seeking new employment. When necessary, he may request that specific training be provided in order to facilitate re-employment.

The data bank administrator will permanently update information on each PAP ensuring that proper follow up on delivery of each entitlement is forthcoming.

The RPIT will also be responsible for documenting the complete resettlement process, according to the following *Reporting Plan*:

Monthly Progress Reports will be prepared and submitted during the complete RAP Phase I – Parts A and B implementation cycle, and will include at least:

- Detailed activities list for all members of the RPIT team;
- Minutes of any public meetings held;
- Total number of people and businesses relocated during the month;
- Total amount of compensation and allowances paid and explanation of any deviation from the initial budget;
- Justified description of any deviation from the proposed implementation schedule;
- List of questions registered at the Public Information Centers;
- List of grievances received by the Grievance Committee and status of resolution;
- Summary table with the situation of each PAP category at the end of the month;
- Detailed description of the situation of any vulnerable PAPs at the end of the month;
- Status of the evaluation indicators at the end of the month and discussion of variation with regards to prior months;
- Description of main problems with RAP implementation to date and suggestions on how to deal with them;

- Planned activity list for the following month.

A *RAP Implementation Phase Final Report* will be submitted at the end of the compensation and assistance process. This will include at least:

- A *Consolidated Entitlements Report*, listing all entitlements effectively delivered on a PAP by PAP basis, and comparison with the initial situation described herein.
- A *Consolidated Public Consultation and Disclosure Report*, describing all consultation and disclosure activities and summarizing main shortcomings identified.
- A *Consolidated Budget Report*, presenting the detailed breakdown of all expenses incurred during RAP implementation, as well as a comparative analysis with the initial budget.
- A *Grievance Redress Report*, listing all claims and their respective resolution, with statistical analysis and discussion of the impacts on the final cost of the RAP.
- An *Implementation Process Evaluation Report*, based on the pre-established indicators and their evolution throughout the RAP implementation process.
- Other information which the RPIT deems pertinent to include.

An *Ex-Post Report*, focusing on documentation of resettled and/or re-installed PAP, will be submitted within 60 days after conclusion of resettlement and/or re-installation activities. This will consist of individual before and after photographic records as well as a comparative table of building characteristics, available utilities and services, and costs affected by resettlement (transport, taxes, rent, utilities, other). Based on evaluation of costs affected by resettlement, eligibility for complementary allowance for additional cost of living will be established.

A *Post-Resettlement Monitoring Report* will be submitted one year after conclusion of resettlement and/or re-installation activities, documenting the result of monitoring of resettled PAP.

ANNEXES

ANNEX 01

Proposed Compensation Schedule

ANNEX 02

**Property Survey Data Bank
(Inspection and Valuation Questionnaire)**

Annex 02 – Part 1

**Sections 01, 02, 05 and 06 of Property Survey –
Inspection & Valuation Questionnaire**

Annex 02 – Part 2

**Section 04 of Property Survey –
Inspection & Valuation Questionnaire**

Annex 02 – Part 3

**Section 07 of Property Survey –
Inspection & Valuation Questionnaire**

ANNEX 03

**Socio-Economic Survey Data Bank
(Residential Resettlement Questionnaire and
Commercial Activities Questionnaire)**

Annex 03 – Part 1

**Sections 01, 02 and 06 of Socio-economic Questionnaire –
Residential Resettlement**

Annex 03 – Part 2

**Sections 03, 04 and 05 of Socio-economic Questionnaire –
Residential Resettlement**

Annex 03 – Part 3

**Sections 01 and 02 of Socio-economic Questionnaire –
Commercial Resettlement**

Annex 03 – Part 4

**Section 03 of Socio-economic Questionnaire –
Commercial Resettlement**

Annex 03 – Part 5

**Section 04 and 05 of Socio-economic Questionnaire –
Commercial Resettlement**

ANNEX 04

DART Phase I – Part B Expropriation Plans

ANNEX 05

Social Impact Assessment included in the ESIA

ANNEX 06

Social Impact Assessment included in the ESIA

VALUATION No	LOCATION	DESCRIPTION	AREA OF BUILDING	SITE COVER AREA SQ. METERS	VALUATION METHOD	ACCOMMODATION ALLOWANCE	TRANSPORT ALLOWANCE	DIFFERENCE ALLOWANCE	LOSS OF PROFITS	COMPENSATION VALUE
VAL DART II GRN 001	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 002	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 003	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 004	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 005	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 006	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 007	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
	GEREZANI	Built up area - main building	08.80	250,000.00	17,200,000.00					17,200,000.00
	GEREZANI	Outbuilding 1	7.84	200,000.00	1,568,000.00					1,568,000.00
VAL DART II GRN 008	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 008	GEREZANI	Tenant - commercial					120,000.00		2,240,000.00	2,360,000.00

VALUATION No	FUNCTION	DESCRIPTION	AREA OF BUILDING	BASE RATE METRE SQR AREA	VALUE OF BUILDING	ACCOMMODATION ALLOWANCE	TRANSPORT ALLOWANCE	DISTURBANCE ALLOWANCE	LOSS OF PROFIT	COMPENSATION VALUE
VAL DART II GRN 009	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 010	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 010	GEREZANI	Built up area - main building	68.80	250.000.00	17.200.000.00					17.200.000.00
VAL DART II GRN 011	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 011	GEREZANI	Land	570.00	59.000.00	31.800.000.00		120.000.00	24.599.600.00		56.889.600.00
VAL DART II GRN 011	GEREZANI	Occupant - residential (detms humwell as other type of occupant)				9.600.000.00	120.000.00			9.120.000.00
VAL DART II GRN 012	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 012	GEREZANI	Tenant - commercial					120.000.00		2.240.000.00	2.360.000.00
VAL DART II GRN 013	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 014	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 015	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 015	GEREZANI	Built up area - main building	68.80	250.000.00	17.200.000.00					17.200.000.00
VAL DART II GRN 016	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 016	GEREZANI	Built up area - main build								
VAL DART II GRN 016	GEREZANI	Outbuilding 1								
VAL DART II GRN 016	GEREZANI	Outbuilding 2								
VAL DART II GRN 016	GEREZANI	Land								3.360.000.00
										3.360.000.00

VALUATION No	CATEGORY	DESCRIPTION	AREA OF BUILDING	RATE PER SQ. METRE	VALUED AMOUNT	ACCOMMODATION ALLOWANCE	TRANSFERRABLE ALLOWANCE	DETERMINED ALLOWANCE	TOTAL PROFIT	COMPENSATION VALUE
VAL DART II GRN 017	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 017	GEREZANI	Tenant - commercial					120 000 00		1 240 000 00	1 360 000 00
VAL DART II GRN 018	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 018	GEREZANI	Tenant - commercial					120 000 00		1 240 000 00	1 360 000 00
VAL DART II GRN 019	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 019	GEREZANI	Built up area - main building	68 80	250 000 00	17 200 000 00					17 200 000 00
VAL DART II GRN 019	GEREZANI	Outbuilding 1								
VAL DART II GRN 019	GEREZANI	Outbuilding 2								
VAL DART II GRN 019	GEREZANI	Land								
VAL DART II GRN 020	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 020	GEREZANI	Land	636 00	50 000 00	31 800 000 00		120 000 00			1 452 000 00
VAL DART II GRN 020	GEREZANI	Commercial					120 000 00			
VAL DART II GRN 021	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 021										
VAL DART II GRN 021										
VAL DART II GRN 021										
VAL DART II GRN 022	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 022	GEREZANI	Built up area - main building	68 80	250 000 00	17 200 000 00					17 200 000 00
VAL DART II GRN 023	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 023	GEREZANI	Built up area - main building	68 80	250 000 00	17 200 000 00					17 200 000 00
VAL DART II GRN 023										
VAL DART II GRN 023										
VAL DART II GRN 024	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 025	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 025	GEREZANI	Land								
VAL DART II GRN 025	GEREZANI	Commercial								
VAL DART II GRN 026	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								

VALUATION	LOCATION	DESCRIPTION	AREA OF BUILDING	RAP PER METER SQ AREA	VALUE OF BUILDING	ACCOMMODATION ALLOWANCE	TRANSPORT ALLOWANCE	DISBURSEMENT ALLOWANCE	LOSS OF PROFIT	COMPENSATION VALUE
VAL DART II GRN 027	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 027	GEREZANI	Built up area - main building	68.80	250 000.00	17 200 000.00					17 200 000.00
VAL DART II GRN 028	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 029	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 030	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 031	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								17 200 000.00 1 568 000.00
VAL DART II GRN 032	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 032	GEREZANI	Built up area - main building	68.80	250 000.00	17 200 000.00					17 200 000.00
VAL DART II GRN 033	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 033	GEREZANI	Built up area - main building	68.80	250 000.00	17 200 000.00					17 200 000.00
VAL DART II GRN 034	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 035	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 035	GEREZANI	Land	636.00	50 000.00	31 800 000.00		120 000.00	24 960 000.00		56 880 000.00
VAL DART II GRN 036	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 037	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 037	GEREZANI	Built up area - main building	68.80	250 000.00	17 200 000.00					17 200 000.00 1 568 000.00 1 452 000.00

Unit/Station No	Item Station	Description	Unit	Sub Item	Quantity	Unit Price	Amount	Material	Labour	Equipment	Overhead & Profit	Total
VAI DARTLE GUN/047	1	excavation	1.00									
	2	main building	1.00									
	3	excavation	1.00									
	4	main building	1.00									
	5	excavation	1.00									
	6	main building	1.00									
	7	excavation	1.00									
	8	main building	1.00									
	9	excavation	1.00									
	10	main building	1.00									
	11	excavation	1.00									
	12	main building	1.00									
	13	excavation	1.00									
	14	main building	1.00									
	15	excavation	1.00									
VAI DARTLE GUN/047	1	excavation	1.00									
	2	main building	1.00									
	3	excavation	1.00									
	4	main building	1.00									
	5	excavation	1.00									
	6	main building	1.00									
	7	excavation	1.00									
	8	main building	1.00									
	9	excavation	1.00									
	10	main building	1.00									
	11	excavation	1.00									
	12	main building	1.00									
	13	excavation	1.00									
	14	main building	1.00									
	15	excavation	1.00									
VAI DARTLE GUN/047	1	excavation	1.00									
	2	main building	1.00									
	3	excavation	1.00									
	4	main building	1.00									
	5	excavation	1.00									
	6	main building	1.00									
	7	excavation	1.00									
	8	main building	1.00									
	9	excavation	1.00									
	10	main building	1.00									
	11	excavation	1.00									
	12	main building	1.00									
	13	excavation	1.00									
	14	main building	1.00									

VALUATION No	LOCATION	DESCRIPTION	AREA OF BUILDING	RATE PER METRE SQUARE	VALUABLE BUILDING	ACCOMMODATION ALLOWANCE	TRANSPORT ALLOWANCE	DIFFERENCE BETWEEN	TYPE OF TRUST	COMPENSATION VALUE
VAL DART II GRN 040	GEREZANI	Land	676,00	50 000,00	31 801 000,00		120 000,00	26 981 200,00		58 901 200,00
VAL DART II GRN 050	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 051	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 052	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 053	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 054	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 054		Built up area - main building	68,80	200 000,00	20 640 000,00					20 640 000,00
VAL DART II GRN 054		Outbuilding 1	7,84	250 000,00	1 960 000,00					1 960 000,00
							120 000,00	26 981 200,00		1 815 000,00
VAL DART II GRN 055	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								58 901 200,00
VAL DART II GRN 055	GEREZANI	Built up area - main building	68,80	200 000,00	20 640 000,00					20 640 000,00
VAL DART II GRN 056	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 056	GEREZANI	Built up area - main building	68,80	200 000,00	20 640 000,00					20 640 000,00
VAL DART II GRN 057	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 058	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 058	GEREZANI	Land	676,00	50 000,00	31 801 000,00		120 000,00	26 981 200,00		58 901 200,00
VAL DART II GRN 059	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 059	GEREZANI	Built up area - main building	68,80	200 000,00	20 640 000,00					20 640 000,00
VAL DART II GRN 060	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
	GEREZANI	Built up area - main building								0 640 000,00
	GEREZANI	Outbuilding 1								1 960 000,00
	GEREZANI	Outbuilding 2								
	GEREZANI	Land								

VALUATION No	LOCATION	DESCRIPTION	AREA OF BUILDING	RATE PER METRE SQUARED	VALUE OF BUILDING	COMPENSATION ALLOWANCE	TRANSPORT ALLOWANCE	DISURBANCE ALLOWANCE	LOSS OF PROFIT	COMPENSATION VALUE
VAL DART II GRN 061	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 061	GEREZANI	Land	636.00	50.000.00	31.800.000.00		120.000.00	26.983.200.00		58.903.200.00
VAL DART II GRN 062	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								20.640.000.00
										1.960.000.00
										1.815.000.00
										58.903.200.00
VAL DART II GRN 063	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
	GEREZANI	Land	636.00	50.000.00	31.800.000.00		120.000.00	26.983.200.00		58.903.200.00
	GEREZANI	Commercial					120.000.00		3.240.000.00	3.360.000.00
VAL DART II GRN 064	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL I										
VAL I										
VAL DART II GRN 065	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DSI										
VAL DSI										
VAL DART II GRN 066	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 066	GEREZANI		68.80	300.000.00	20.640.000.00					20.640.000.00
VAL DART II GRN 066	GEREZANI									
VAL DART II GRN 066										
VAL DART II GRN 066		Land								
VAL DART II GRN 067	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 067										20.640.000.00
VAL DART II GRN 067										1.960.000.00
VAL DART II GRN 067										
VAL DART II GRN 068	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 069	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 069	GEREZANI		68.80	300.000.00	20.640.000.00					20.640.000.00
VAL DART II GRN 069	GEREZANI									1.960.000.00
VAL DART II GRN 069										
VAL DART II GRN 069										
VAL DART II GRN 069							120.000.00		3.240.000.00	3.360.000.00
VAL DART II GRN 070	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
VAL DART II GRN 071	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								
	GEREZANI	Built up area - main building								
	GEREZANI	Outbuilding 1								
	GEREZANI	Outbuilding 2			7.20					
		Land	636.00							

VALUATION No	LOCATION	DESCRIPTION	AREA OF BUILDING	RATE PER METER SQUARE	VALUATION RATE	ACCOMMODATION ALLOWANCE	TRANSPORT ALLOWANCE	DISTURBANCE ALLOWANCE	NET PROFIT	COMPENSATION VALUE
VAL DART II GRN 072	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								20 640 000 00
										1 960 000 00
										1 815 000 00
								26 985 200 00		58 995 200 00
VAL DART II GRN 073	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								20 640 000 00
										1 960 000 00
										1 815 000 00
VAL DART II GRN 074	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								20 640 000 00
										1 960 000 00
										1 815 000 00
	GEREZANI	Outbuilding 2	7,26	250 000 00	1 815 000 00					1 815 000 00
		Land	6,56 00	50 000 00	31 800 000 00		120 000 00	26 985 200 00	1 240 000 00	58 995 200 00
VAL DART II GRN 074	GEREZANI	Commercial					120 000 00		1 240 000 00	3 360 000 00
VAL DART II GRN 075	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								20 640 000 00
										1 960 000 00
										1 815 000 00
VAL DART II GRN 075	GEREZANI	Built up area - main building	68,80	500 000 00	20 640 000 00					20 640 000 00
VAL DART II GRN 075	GEREZANI	Outbuilding 1	7,84	250 000 00	1 960 000 00					1 960 000 00
VAL DART II GRN 075	GEREZANI	Outbuilding 2	7,26	250 000 00	1 815 000 00					1 815 000 00
VAL DART II GRN 075	GEREZANI	Land	6,56 00	50 000 00	31 800 000 00		120 000 00	26 985 200 00	1 240 000 00	58 995 200 00
VAL DART II GRN 075	GEREZANI	Commercial					120 000 00		1 240 000 00	3 360 000 00
VAL DART II GRN 076	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								20 640 000 00
										1 960 000 00
										1 815 000 00
VAL DART II GRN 076	GEREZANI				20 640 000 00					20 640 000 00
VAL DART II GRN 076	GEREZANI									1 960 000 00
VAL DART II GRN 076	GEREZANI									1 815 000 00
VAL DART II GRN 077	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								20 640 000 00
										1 960 000 00
										1 815 000 00
										20 640 000 00
VAL DART II GRN 077	GEREZANI						120 000 00		1 240 000 00	3 360 000 00
VAL DART II GRN 078	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								20 640 000 00
										1 960 000 00
										1 815 000 00
										20 640 000 00
VAL DART II GRN 079	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								20 640 000 00
										1 960 000 00
										1 815 000 00
										20 640 000 00
										1 960 000 00
										1 815 000 00
VAL DART II GRN 080	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								20 640 000 00
										1 960 000 00
										1 815 000 00
										20 640 000 00
										1 960 000 00
										1 815 000 00
VAL DART II GRN 081	GEREZANI	This is a single storey semi detached Residential property with Two out buildings								20 640 000 00
										1 960 000 00
										1 815 000 00
										20 640 000 00
										1 960 000 00
										1 815 000 00
VAL DART II GRN 081	GEREZANI	Commercial					120 000 00		1 240 000 00	3 360 000 00

VALUATION No	LOCATION	DESCRIPTION	AREA OF BUILDING	NUMBER OF FLOORS	VALUABLE BUILDING	ACQUISITION ALLOWANCE	TRANSFER ALLOWANCE	DEFERRED COMPENSATION	LOSS OF PROFIT	COMPENSATION VALUE
VAL DART II GRN 081		Residential tenant This is a single stores semi detached				9 000 000 00	120 000 00			9 120 000 00
VAL DART II GRN 082	GEREZANI	Residential property with Two out buildings								
VAL DART II GRN 083	GEREZANI	This is a single stores semi detached Residential property with Two out buildings								
VAL DART II GRN 084	GEREZANI	This is a single stores semi detached Residential property with Two out buildings								
VAL DART II GRN 085	GEREZANI	This is a single stores semi detached Residential property with Two out buildings								
VAL DART II GRN 086	GEREZANI	This is a single stores semi detached Residential property with Two out buildings								
VAL DART II GRN 086	GEREZANI	Built up area - main building	68 80		500 000 00	20 640 000 00				
VAL DART II GRN 086		Outbuilding 1	7 84		250 000 00	1 960 000 00				
VAL DART II GRN 086										
VAL DART II GRN 086	GERE									
VAL DART II GRN 087	GEREZANI	This is a single stores semi detached Residential property with Two out buildings								
		Built up area - main building	68 80			20 640 000 00				20 640 000 00
		Outbuilding 1	7 84			1 960 000 00				1 960 000 00
VAL DART II GRN 087										
VAL DART II GRN 087										
VAL DART II GRN 088	GEREZANI	This is a single stores semi detached Residential property with Two out buildings								
VAL DART II GRN 089	GEREZANI	This is a single stores semi detached Residential property with Two out buildings								
VAL DART II GRN 089										
VAL DART II GRN 089	GEREZANI	This is a single stores semi detached Residential property with Two out buildings								
VAL DART II GRN 091	GEREZANI	This is a single stores semi detached Residential property with Two out buildings								
VAL DART II GRN 091	GEREZANI	Built up area								
	GEREZANI	Outbuilding								
VAL DART II GR							120 000 00	20 983 200 00		8 303 200 00
VAL DART II GRN 092	GEREZANI	This is a single stores semi detached Residential property with Two out buildings								
		Built up area - main building	68 80		500 000 00	20 640 000 00				20 640 000 00
		Outbuilding 1	7 84		250 000 00	1 960 000 00				1 960 000 00
VAL DART II GRN 092										
VAL DART II GRN 092										

VALUATION No	LOCATION	DESCRIPTION	AREA OF BUILDING	NET FLOOR AREA	VALUABLE BUILDING	ACCOMMODATION ALLOWANCE	TRANSFORM ALLOWANCE	DETERMINED ALLOTTMENT	LOSS OF PROFIT	COMPENSATION VALUE
VAL DART KN HNF 006	HANNAN ASSIE	Jewelry shop						120 000.00	2 240 000.00	2 360 000.00
VAL DART KN HNF 006	HANNAN ASSIE	electrical hardware						120 000.00	2 240 000.00	2 360 000.00
VAL DART KN HNF 007	HANNAN ASSIE	This is part of a single stores residential commercial property with a retaining wall. The hipped roof is covered with corrugated iron sheets on timber members and hardboards ceiling underneath. Walls are of sand cement blocks which are plastered and painted. Windows are of glass louvers in wooden frames. Doors are of glazed panels and timber battens. Floor finishing is of cement screed and floor tiles. It accommodates 4 shops in good condition.								
	HANNAN ASSIE									
	HANNAN ASSIE									
	HANNAN ASSIE									
	HANNAN ASSIE									
	HANNAN ASSIE									
VAL DART KN HNF 008	HANNAN ASSIE	Part of roofed verandah covered with corrugated iron sheets on timber members and hard boards ceiling underneath. It is supported in the ground by tubular metal poles. Floor finishing is of cement screed. There is a retaining wall of about 2 meters high.								
	HANNAN ASSIE									
	HANNAN ASSIE		25.00	30 000.00	768 000.00			120 000.00	1 755 552.00	5 883 552.00
VAL DART KN HNF 009	HANNAN ASSIE	This is part of a single stores commercial property with a semi-enclosed verandah. The hipped roof is covered with corrugated iron sheets on timber members and hardboards ceiling underneath. Walls are of sand cement blocks which are plastered and painted. Windows are of glass louvers in wooden frames. Doors are of glazed panels and timber battens. Floor finishing is of cement screed and floor tiles. It accommodates a restaurant, a kitchen, two toilets and a store in good condition.								
	HANNAN ASSIE									
VAL DART KN HNF 009	HANNAN ASSIE	Built up area	61.50	250 000.00	15 375 000.00					15 375 000.00
	HANNAN ASSIE	Verandah	22.64	95 000.00	9 118 000.00					9 118 000.00
	HANNAN ASSIE	Land	94.14							
VAL DART KN HNF 010	HANNAN ASSIE	This is a single stores residential commercial property with 3 outbuildings. The hipped roof is covered with corrugated iron sheets on timber members and hardboards ceiling underneath. Walls are of sand cement blocks which are plastered and painted. Windows are of wire mesh in wooden frames. Doors are of timber battens. Floor finishing is of cement screed and ceramic tiles. It accommodates 7 beds in good condition.								
	HANNAN ASSIE									
VAL D										
VAL D										
VAL DART KN HNF 010		In building 2 x HP	3.10	75 000.00	252 500.00					252 500.00
VAL DART KN HNF 010		Out building	59.51	150 000.00	8 926 500.00					8 926 500.00
VAL DART KN HNF 010										
								1 000.00	2 240 000.00	2 241 000.00
								1 000.00	2 240 000.00	2 241 000.00
								1 000.00	2 240 000.00	2 241 000.00
								1 000.00	2 240 000.00	2 241 000.00
								1 000.00	2 240 000.00	2 241 000.00

VALUATION No	LOCATION	DESCRIPTION	AREA OF BUILDING	RATE PER SQUARE METRE	VALUATION PRICE	AMOUNT PAID	UNPAID AMOUNT	TRANSFERRABLE ALLOWANCE	DEVELOPMENT ALLOWANCE	LOSS OF PROFIT	COMPENSATION VALUE
VAL DART K N KMR 003	KIMARA MATANGINI	Built up area - main building	100.87	250.000.00	25.217.500.00						25.217.500.00
VAL DART K N KMR 003	KIMARA MATANGINI	Outbuilding	31.49	150.000.00	4.723.500.00						4.723.500.00
VAL DART K N KMR 003	KIMARA MATANGINI	VIP	8.50	90.000.00	765.000.00		2.160.000.00	120.000.00			3.045.000.00
VAL DART K N KMR 003	KIMARA MATANGINI	Tenant					2.160.000.00	120.000.00			2.280.000.00
VAL DART K N KMR 003	KIMARA MATANGINI	Office accommodation						120.000.00		3.240.000.00	3.360.000.00
VAL DART K N KMR 004	KIMARA MATANGINI	This is a single storey residential property									
VAL DART K N KMR 004	KIMARA MATANGINI	Built up area - main building	73.24	250.000.00	18.310.000.00						18.310.000.00
VAL DART K N KMR 004	KIMARA MATANGINI	Outbuilding	8.41	150.000.00	1.261.500.00						1.261.500.00
VAL DART K N KMR 004	KIMARA MATANGINI	VIP	4.00	90.000.00	360.000.00		2.160.000.00	120.000.00			2.640.000.00
VAL DART K N KMR 004	KIMARA MATANGINI	Commercial						120.000.00		3.240.000.00	3.360.000.00
VAL DART K N KMR 004	KIMARA MATANGINI	Tenant - residential					4.2.000.00	120.000.00			552.000.00
VAL DART K N KMR 004	KIMARA MATANGINI	Tenant - residential					4.2.000.00	120.000.00			552.000.00
VAL DART K N KMR 004	KIMARA MATANGINI	Tenant - residential					4.2.000.00	120.000.00			552.000.00
VAL DART K N KMR 004	KIMARA MATANGINI	Tenant - residential					4.2.000.00	120.000.00			552.000.00
VAL DART K N KMR 004	KIMARA MATANGINI	Tenant - residential					4.2.000.00	120.000.00			552.000.00
VAL DART K N KMR 004	KIMARA MATANGINI	Tenant - residential					4.2.000.00	120.000.00			552.000.00
VAL DART K N KMR 004	KIMARA MATANGINI	Tenant - residential					4.2.000.00	120.000.00			552.000.00
VAL DART K N KMR 004	KIMARA MATANGINI	Tenant - residential					4.2.000.00	120.000.00			552.000.00
VAL DART K N KMR 005	KIMARA MATANGINI	This is a single storey residential property									
VAL DART K N KMR 005	KIMARA MATANGINI	Built up area - main building	31.40	250.000.00	7.850.000.00						7.850.000.00
VAL DART K N KMR 005	KIMARA MATANGINI	Outbuilding 1	100.51	200.000.00	20.102.000.00						20.102.000.00
VAL DART K N KMR 005	KIMARA MATANGINI	Outbuilding 2	2.50	150.000.00	360.000.00		1.800.000.00	120.000.00			2.514.000.00
VAL DART K N KMR 005	KIMARA MATANGINI	Jewelry						120.000.00		3.240.000.00	3.360.000.00
VAL DART K N KMR 005	KIMARA MATANGINI	Shop						120.000.00		3.240.000.00	3.360.000.00
VAL DART K N KMR 005	KIMARA MATANGINI	Shop						120.000.00		3.240.000.00	3.360.000.00
VAL DART K N KMR 005	KIMARA MATANGINI	Shop						120.000.00		3.240.000.00	3.360.000.00
VAL DART K N KMR 005	KIMARA MATANGINI	Shop						120.000.00		3.240.000.00	3.360.000.00
VAL DART K N KMR 005	KIMARA MATANGINI	Residential tenant					360.000.00	120.000.00			480.000.00
VAL DART K N KMR 005	KIMARA MATANGINI	Residential tenant					360.000.00	120.000.00			480.000.00
VAL DART K N KMR 005	KIMARA MATANGINI	Residential tenant					360.000.00	120.000.00			480.000.00
VAL DART K N KMR 005	KIMARA MATANGINI	Residential tenant					360.000.00	120.000.00			480.000.00
VAL DART K N KMR 005	KIMARA MATANGINI	Residential tenant					360.000.00	120.000.00			480.000.00
VAL DART K N KMR 006	KIMARA MATANGINI	This is a single storey commercial property									
VAL DART K N KMR 006	KIMARA MATANGINI	Built up area - main building	98.70	250.000.00	24.697.500.00						24.697.500.00
VAL DART K N KMR 006	KIMARA MATANGINI	Verandah	94.00	70.000.00	6.580.000.00						6.580.000.00
VAL DART K N KMR 006	KIMARA MATANGINI	Toilet	11.70	80.000.00	936.000.00			120.000.00			1.056.000.00
VAL DART K N KMR 006	KIMARA MATANGINI	Bar and restaurant						120.000.00		3.240.000.00	3.360.000.00
VAL DART K N KMR 006	KIMARA MATANGINI	Shop						120.000.00		3.240.000.00	3.360.000.00
VAL DART K N KMR 007	KIMARA MATANGINI	This is a single storey commercial property									
VAL DART K N KMR 007	KIMARA MATANGINI	Built up area - main building	157.74	250.000.00	39.435.000.00						39.435.000.00
VAL DART K N KMR 007	KIMARA MATANGINI	Outbuilding	32.96	100.000.00	3.296.000.00			120.000.00		3.240.000.00	6.596.000.00
VAL DART K N KMR 007	KIMARA MATANGINI	Bar and restaurant						120.000.00		3.240.000.00	3.360.000.00
VAL DART K N KMR 007	KIMARA MATANGINI	Shop						120.000.00		3.240.000.00	3.360.000.00
VAL DART K N KMR 008	--	--	--	--	--			120.000.00		3.240.000.00	3.360.000.00

VALUATION No	Use/Notes	Area/Description	Area (sq. m)	Rate (R/ha)	Area (ha)	Value (R)	Area (sq. m)	Rate (R/ha)	Value (R)	Area (sq. m)	Rate (R/ha)	Value (R)	Area (sq. m)	Rate (R/ha)	Value (R)	Area (sq. m)	Rate (R/ha)	Value (R)	
V.A.L.D.A.R.T.E.N.K.N.I.R.001																			
V.A.L.D.A.R.T.E.N.K.N.I.R.002																			
V.A.L.D.A.R.T.E.N.K.N.I.R.003																			
V.A.L.D.A.R.T.E.N.K.N.I.R.004																			
V.A.L.D.A.R.T.E.N.K.N.I.R.005																			
V.A.L.D.A.R.T.E.N.K.N.I.R.006																			
V.A.L.D.A.R.T.E.N.K.N.I.R.007																			
V.A.L.D.A.R.T.E.N.K.N.I.R.008																			
V.A.L.D.A.R.T.E.N.K.N.I.R.009																			
V.A.L.D.A.R.T.E.N.K.N.I.R.010																			
V.A.L.D.A.R.T.E.N.K.N.I.R.011																			
V.A.L.D.A.R.T.E.N.K.N.I.R.012																			
V.A.L.D.A.R.T.E.N.K.N.I.R.013																			
V.A.L.D.A.R.T.E.N.K.N.I.R.014																			
V.A.L.D.A.R.T.E.N.K.N.I.R.015																			
V.A.L.D.A.R.T.E.N.K.N.I.R.016																			
V.A.L.D.A.R.T.E.N.K.N.I.R.017																			
V.A.L.D.A.R.T.E.N.K.N.I.R.018																			
V.A.L.D.A.R.T.E.N.K.N.I.R.019																			
V.A.L.D.A.R.T.E.N.K.N.I.R.020																			
V.A.L.D.A.R.T.E.N.K.N.I.R.021																			
V.A.L.D.A.R.T.E.N.K.N.I.R.022																			
V.A.L.D.A.R.T.E.N.K.N.I.R.023																			
V.A.L.D.A.R.T.E.N.K.N.I.R.024																			
V.A.L.D.A.R.T.E.N.K.N.I.R.025																			
V.A.L.D.A.R.T.E.N.K.N.I.R.026																			
V.A.L.D.A.R.T.E.N.K.N.I.R.027																			
V.A.L.D.A.R.T.E.N.K.N.I.R.028																			
V.A.L.D.A.R.T.E.N.K.N.I.R.029																			
V.A.L.D.A.R.T.E.N.K.N.I.R.030																			
V.A.L.D.A.R.T.E.N.K.N.I.R.031																			
V.A.L.D.A.R.T.E.N.K.N.I.R.032																			
V.A.L.D.A.R.T.E.N.K.N.I.R.033																			
V.A.L.D.A.R.T.E.N.K.N.I.R.034																			
V.A.L.D.A.R.T.E.N.K.N.I.R.035																			
V.A.L.D.A.R.T.E.N.K.N.I.R.036																			
V.A.L.D.A.R.T.E.N.K.N.I.R.037																			
V.A.L.D.A.R.T.E.N.K.N.I.R.038																			
V.A.L.D.A.R.T.E.N.K.N.I.R.039																			
V.A.L.D.A.R.T.E.N.K.N.I.R.040																			
V.A.L.D.A.R.T.E.N.K.N.I.R.041																			
V.A.L.D.A.R.T.E.N.K.N.I.R.042																			
V.A.L.D.A.R.T.E.N.K.N.I.R.043																			
V.A.L.D.A.R.T.E.N.K.N.I.R.044																			
V.A.L.D.A.R.T.E.N.K.N.I.R.045																			
V.A.L.D.A.R.T.E.N.K.N.I.R.046																			
V.A.L.D.A.R.T.E.N.K.N.I.R.047																			
V.A.L.D.A.R.T.E.N.K.N.I.R.048																			
V.A.L.D.A.R.T.E.N.K.N.I.R.049																			
V.A.L.D.A.R.T.E.N.K.N.I.R.050																			

VALUATION No	LOKASI	DESKRIPSI	LUAS BANGUNAN	KETERANGAN	VALUASI PERMULAAN	VALUASI PERSEK	VALUASI PERSEK	VALUASI PERSEK	VALUASI PERSEK	VALUASI PERSEK	VALUASI PERSEK	VALUASI PERSEK	VALUASI PERSEK
VAL DART K N KMR 013	KIMARA MATANGINI	This is a single stores commercial Cum residential property.											
VAL DART K N KMR 013	KIMARA MATANGINI	Built up area - main building	79,21	250 000,00	19 802 500,00								19 802 500,00
VAL DART K N KMR 013	KIMARA MATANGINI	Outbuilding	29,674 00	150 000,00	4 451 100,00								4 451 100,00
VAL DART K N KMR 013	KIMARA MATANGINI	Verandah	19,635,00	50 000,00	981 750,00	2 160 000,00	120 000,00						3 261 750,00
VAL DART K N KMR 013	KIMARA MATANGINI	Residential tenant				540 000,00	120 000,00						660 000,00
VAL DART K N KMR 013	KIMARA MATANGINI	Residential tenant				540 000,00	120 000,00						660 000,00
VAL DART K N KMR 013	KIMARA MATANGINI	Residential tenant				540 000,00	120 000,00						660 000,00
VAL DART K N KMR 013	KIMARA MATANGINI	Residential tenant				540 000,00	120 000,00						660 000,00
VAL DART K N KMR 013	KIMARA MATANGINI	Tailoring mart					120 000,00				3 240 000,00		3 360 000,00
VAL DART K N KMR 013	KIMARA MATANGINI	TV shop					120 000,00				3 240 000,00		3 360 000,00
VAL DART K N KMR 013	KIMARA MATANGINI	Auto parts					120 000,00				3 240 000,00		3 360 000,00
VAL DART K N KMR 013	KIMARA MATANGINI	Music studio					120 000,00				3 240 000,00		3 360 000,00
VAL DART K N KMR 013	KIMARA MATANGINI	Phone vendors and accessories					120 000,00				3 240 000,00		3 360 000,00
VAL DART K N KMR 014	KIMARA MATANGINI	This is a single stores commercial Cum residential property.											
VAL DART K N KMR 014	KIMARA MATANGINI	Built up area - main building	154,71	250 000,00	38 677 500,00								38 677 500,00
VAL DART K N KMR 014	KIMARA MATANGINI	Outbuilding	43,876 00	150 000,00	6 575 400,00								6 575 400,00
VAL DART K N KMR 014	KIMARA MATANGINI	Verandah	18,00	50 000,00	900 000,00								900 000,00
VAL DART K N KMR 014	KIMARA MATANGINI	Kiosk	44,20	30 000,00	1 326 000,00	1 620 000,00	120 000,00				3 240 000,00		6 306 000,00
VAL DART K N KMR 014	KIMARA MATANGINI	Residential tenant				540 000,00	120 000,00						660 000,00
VAL DART K N KMR 014	KIMARA MATANGINI	Residential tenant				540 000,00	120 000,00						660 000,00
VAL DART K N KMR 014	KIMARA MATANGINI	Residential tenant				540 000,00	120 000,00						660 000,00
VAL DART K N KMR 014	KIMARA MATANGINI	Phone vendors					120 000,00				3 240 000,00		3 360 000,00
VAL DART K N KMR 014	KIMARA MATANGINI	Auto spares					120 000,00				3 240 000,00		3 360 000,00
VAL DART K N KMR 014	KIMARA MATANGINI	Kiosk					120 000,00				3 240 000,00		3 360 000,00
VAL DART K N KMR 014	KIMARA MATANGINI	Workshop					120 000,00				3 240 000,00		3 360 000,00
VAL DART K N KMR 014	KIMARA MATANGINI	Whole sales					120 000,00				27 917 820,00		28 037 820,00
VAL DART K N KMR 015	KIMARA MATANGINI	This is a single stores commercial Cum residential property.											
VAL DART K N KMR 015	KIMARA MATANGINI	Built up area - main building	80,20	250 000,00	20 050 000,00								20 050 000,00
VAL DART K N KMR 015	KIMARA MATANGINI	Outbuilding	31,84	150 000,00	4 776 000,00								4 776 000,00
VAL DART K N KMR 015	KIMARA MATANGINI	Verandah	40,50	50 000,00	2 028 000,00								2 028 000,00
VAL DART K N KMR 015	KIMARA MATANGINI	VIP	20,10	30 000,00	604 800,00	3 600 000,00	120 000,00						4 324 800,00
VAL DART K N KMR 015	KIMARA MATANGINI	Commercial					120 000,00				3 240 000,00		3 360 000,00
VAL DART K N KMR 015	KIMARA MATANGINI	Residential tenant					3 600 000,00	120 000,00					3 720 000,00
VAL DART K N KMR 015	KIMARA MATANGINI	Stores and office					120 000,00				27 917 820,00		28 037 820,00
VAL DART K N KMR 017	KIMARA MATANGINI	This is a single stores commercial property installed with heavy machinery.											
VAL DART K N KMR 017	KIMARA MATANGINI	Built up area - main building	89,91	250 000,00	22 477 500,00								22 477 500,00
VAL DART K N KMR 017	KIMARA MATANGINI	Verandah	21,57	50 000,00	1 078 650,00								1 078 650,00
VAL DART K N KMR 017	KIMARA MATANGINI	Hardware shop					120 000,00				3 240 000,00		3 360 000,00
VAL DART K N KMR 017	KIMARA MATANGINI	Whole sale and retail					120 000,00				3 240 000,00		3 360 000,00
VAL DART K N KMR 018	KIMARA MATANGINI	This is a single stores commercial Cum residential property.											

VAL ID No	LOCATION	Use / Purpose	Area of Plot (sqm)	Water Meter sqm	Value of Plot (RM)	Compensation Amount (RM)	Transfer Fee (RM)	Debt Balance (RM)	Loss of Profit	Compensation Total (RM)
VAL DART EN KMR 018	KIMARA MATANGINI	Built up area - main building	140.12	250,000.00	35,050,000.00					35,050,000.00
VAL DART EN KMR 018	KIMARA MATANGINI	Outbuilding 1	58.70	150,000.00	8,805,000.00					8,805,000.00
VAL DART EN KMR 018	KIMARA MATANGINI	Outbuilding 2	68.78	100,000.00	6,878,000.00	8,640,000.00	120,000.00			15,638,000.00
VAL DART EN KMR 018	KIMARA MATANGINI	Residential tenant				720,000.00	120,000.00			840,000.00
VAL DART EN KMR 018	KIMARA MATANGINI	Residential tenant				720,000.00	120,000.00			840,000.00
VAL DART EN KMR 018	KIMARA MATANGINI	Residential tenant				720,000.00	120,000.00			840,000.00
VAL DART EN KMR 018	KIMARA MATANGINI	Residential tenant				720,000.00	120,000.00			840,000.00
VAL DART EN KMR 018	KIMARA MATANGINI	Residential tenant				720,000.00	120,000.00			840,000.00
VAL DART EN KMR 018	KIMARA MATANGINI	Residential tenant				720,000.00	120,000.00			840,000.00
VAL DART EN KMR 018	KIMARA MATANGINI	Residential tenant				720,000.00	120,000.00			840,000.00
VAL DART EN KMR 018	KIMARA MATANGINI	Residential tenant				720,000.00	120,000.00			840,000.00
VAL DART EN KMR 018	KIMARA MATANGINI	Residential tenant				720,000.00	120,000.00			840,000.00
VAL DART EN KMR 018	KIMARA MATANGINI	Residential tenant				720,000.00	120,000.00			840,000.00
VAL DART EN KMR 018	KIMARA MATANGINI	Residential tenant				720,000.00	120,000.00			840,000.00
VAL DART EN KMR 019	KIMARA MATANGINI	This is a single storey residential property								
VAL DART EN KMR 019	KIMARA MATANGINI	Built up area	51.84	250,000.00	12,950,600.00	540,000.00	120,000.00			14,610,600.00
VAL DART EN KMR 019	KIMARA MATANGINI	Residential tenant				540,000.00	120,000.00			660,000.00
VAL DART EN KMR 020	KIMARA MATANGINI	This is a single storey commercial/ residential property								
VAL DART EN KMR 020	KIMARA MATANGINI	Built up area	42.25	250,000.00	10,562,500.00					10,562,500.00
VAL DART EN KMR 020	KIMARA MATANGINI	shed	48.35	50,000.00	2,418,000.00					2,418,000.00
VAL DART EN KMR 020	KIMARA MATANGINI	Outbuilding	5,352.00	100,000.00	533,200.00	720,000.00	120,000.00			1,375,200.00
VAL DART EN KMR 020	KIMARA MATANGINI	Bar and restaurant					120,000.00		5,240,000.00	5,360,000.00
VAL DART EN KMR 020	KIMARA MATANGINI	Residential tenant				720,000.00	120,000.00			840,000.00
VAL DART EN KMR 020	KIMARA MATANGINI	Retail shop					120,000.00		5,240,000.00	5,360,000.00
VAL DART EN KMR 021	KIMARA MATANGINI	This is a single storey residential property								
VAL DART EN KMR 021	KIMARA MATANGINI	Built up area	93.83	250,000.00	23,457,500.00					23,457,500.00
VAL DART EN KMR 021	KIMARA MATANGINI	Verandah	9.02	50,000.00	451,000.00					451,000.00
VAL DART EN KMR 021	KIMARA MATANGINI	VIP	3.25	70,000.00	24,511,500.00	24,150,000.00	120,000.00			24,511,500.00
VAL DART EN KMR 022	KIMARA MATANGINI	These are single storey residential properties								
VAL DART EN KMR 022	KIMARA MATANGINI	Built up area	18.00	250,000.00	4,500,000.00		120,000.00			4,620,000.00
VAL DART EN KMR 022	KIMARA MATANGINI	Business	--	--	--		120,000.00		5,240,000.00	5,360,000.00
VAL DART EN KMR 023	KIMARA MATANGINI	This is a single storey residential Property with an out building								
VAL DART EN KMR 023	KIMARA MATANGINI	Built up area - main building	97.36	250,000.00	24,340,000.00					24,340,000.00
VAL DART EN KMR 023	KIMARA MATANGINI	Outbuilding	14.40	150,000.00	2,160,000.00					2,160,000.00
VAL DART EN KMR 023	KIMARA MATANGINI	Foundation	13.28	75,000.00	990,000.00	2,772,000.00	120,000.00			3,838,000.00
VAL DART EN KMR 023	KIMARA MATANGINI	Residential tenant				720,000.00	120,000.00			840,000.00
VAL DART EN KMR 023	KIMARA MATANGINI	Residential tenant				612,000.00	120,000.00			732,000.00
VAL DART EN KMR 023	KIMARA MATANGINI	Residential tenant				720,000.00	120,000.00			840,000.00
VAL DART EN KMR 023	KIMARA MATANGINI	Residential tenant				720,000.00	120,000.00			840,000.00

VALUATION No	LOCATION	DESCRIPTION	AREA OF BUILDING	RATE PER SQUARE METRE	VALUE OF BUILDING	COMPENSATION ALLOWANCE	TRANSPORT ALLOWANCE	DESTRUCTION ALLOWANCE	LOSS OF PROFITS	COMPENSATION TOTAL
VAL DARTEN KMR 025	KIMARA MATANGINI	This is a single storey residential Property with an out building	101.22	350.000.00	35.427.000.00					35.427.000.00
VAL DARTEN KMR 025	KIMARA MATANGINI	Built up area					120.000.00		3.240.000.00	3.360.000.00
VAL DARTEN KMR 026	KIMARA MATANGINI	This is a single storey residential commercial Property								
VAL DARTEN KMR 026	KIMARA MATANGINI	Built up area	143.50	250.000.00	35.875.000.00					35.875.000.00
VAL DARTEN KMR 026	KIMARA MATANGINI	Pavement	72.70	200.000.00	14.540.000.00	1.620.000.00	120.000.00			16.280.000.00
VAL DARTEN KMR 026							120.000.00		3.240.000.00	3.360.000.00
VAL DARTEN KMR 026	KIMARA MATANGINI	Bar and restaurant					120.000.00		3.240.000.00	3.360.000.00
VAL DARTEN KMR 026	KIMARA MATANGINI	Office accommodation					120.000.00		3.240.000.00	3.360.000.00
VAL DARTEN KMR 026	KIMARA MATANGINI	Residential tenant				540.000.00	120.000.00			660.000.00
VAL DARTEN KMR 026	KIMARA MATANGINI	Residential tenant				540.000.00	120.000.00			660.000.00
VAL DARTEN KMR 026	KIMARA MATANGINI	Residential tenant				540.000.00	120.000.00			660.000.00
VAL DARTEN KMR 027	KIMARA MATANGINI	This is a single storey residential commercial Property with a basement								
VAL DARTEN KMR 027	KIMARA MATANGINI	Built up area	105.18	250.000.00	26.295.000.00					26.295.000.00
VAL DARTEN KMR 027	KIMARA MATANGINI	verandah	15.60	75.000.00	1.174.500.00	27.469.500.00	120.000.00			28.764.000.00
VAL DARTEN KMR 027	KIMARA MATANGINI	Commercial					120.000.00		3.240.000.00	3.360.000.00
VAL DARTEN KMR 028	KIMARA MATANGINI	This is a single storey residential Property								
VAL DARTEN KMR 028	KIMARA MATANGINI	Built up area	102.97	250.000.00	25.742.500.00					25.742.500.00
VAL DARTEN KMR 028	KIMARA MATANGINI	veranda	5.40	75.000.00	411.750.00	2.520.000.00	120.000.00		3.240.000.00	6.221.750.00
VAL DARTEN KMR 028	KIMARA MATANGINI	Residential tenant				2.520.000.00	120.000.00			2.640.000.00
VAL DARTEN KMR 029	KIMARA MATANGINI	This is a single storey residential commercial Property								
VAL DARTEN KMR 029	KIMARA MATANGINI	Built up area	125.79	250.000.00	31.447.500.00					31.447.500.00
VAL DARTEN KMR 029	KIMARA MATANGINI	Foundation	9.88	75.000.00	741.000.00					741.000.00
VAL DARTEN KMR 029	KIMARA MATANGINI	Outbuilding 1	21.60	150.000.00	3.240.000.00					3.240.000.00
VAL DARTEN KMR 029	KIMARA MATANGINI	Outbuilding 2	3.58	100.000.00	358.000.00	2.160.000.00	120.000.00			2.638.000.00
VAL DARTEN KMR 029	KIMARA MATANGINI	Residential tenant				540.000.00	120.000.00			660.000.00
VAL DARTEN KMR 029	KIMARA MATANGINI	Residential tenant				540.000.00	120.000.00			660.000.00
VAL DARTEN KMR 029	KIMARA MATANGINI	Residential tenant				540.000.00	120.000.00			660.000.00
VAL DARTEN KMR 029	KIMARA MATANGINI	Retail shop					120.000.00		3.240.000.00	3.360.000.00
VAL DARTEN KMR 029	KIMARA MATANGINI	Soft drinks					120.000.00		3.240.000.00	3.360.000.00
VAL DARTEN KMR 029	KIMARA MATANGINI	Retail shop					120.000.00		3.240.000.00	3.360.000.00
VAL DARTEN KMR 029	KIMARA MATANGINI	Stationary					120.000.00		3.240.000.00	3.360.000.00
VAL DARTEN KMR 031	KIMARA MATANGINI	This is a single storey residential Property								
VAL DARTEN KMR 031	KIMARA MATANGINI	Built up area	87.59	75.000.00	6.569.250.00					6.569.250.00
VAL DARTEN KMR 031	KIMARA MATANGINI	shade	40.81	50.000.00	2.040.500.00		120.000.00		3.240.000.00	5.409.500.00
VAL DARTEN KMR 031	KIMARA MATANGINI	Residential tenant				180.000.00	120.000.00			300.000.00
VAL DARTEN KMR 031	KIMARA MATANGINI	Residential tenant				180.000.00	120.000.00			300.000.00
VAL DARTEN KMR 031	KIMARA MATANGINI	Residential tenant				180.000.00	120.000.00			300.000.00
VAL DARTEN KMR 031	KIMARA MATANGINI	Residential tenant				180.000.00	120.000.00			300.000.00
VAL DARTEN KMR 031	KIMARA MATANGINI	Residential tenant				180.000.00	120.000.00			300.000.00

VALUATION No	LOCALITY	DESCRIPTION	AREA OF BUILDING	RATE PER METER SQUARE	VALUATION RATE	ACCOMMODATION ALLOWANCE	TRANSPORT ALLOWANCE	DETERMINED VALUE	TOTAL PRICE	COMPENSATION VALUE
VAL DART K N KMR 031	KIMARA MATANGINI	Residential tenant				180 000 00	120 000 00			300 000 00
VAL DART K N KMR 032	KIMARA MATANGINI	Three storey commercial property	7,36	150 000 00	594 000 00		120 000 00	1 240 000 00	1 240 000 00	1 954 000 00
VAL DART K N KMR 033	KIMARA MATANGINI	Foundation to first level	85 50	75 000 00	6 412 500 00		120 000 00	1 200 000 00	1 200 000 00	9 802 500 00
VAL DART K N KMR 034	KIMARA MATANGINI									
VAL DART K N KMD 001	KUMBU KUMBU	Part of fencing wall to residential property with a metal gate hung on Reinforced concrete poles								
VAL DART K N KMD 002	KUMBU KUMBU	Parking space and a permanent kiosk for Mount Alimulhuda hospital it is constructed of lean to roof covered with C/D on timber members and soft Board ceiling underneath. Walls are of sand cement blocks which are plastered. Windows are of fabricated metal grills. Doors are of timber. Batens floor finishing is of Cement screed. It accommodate 3 rooms in good condition								1 760 000 00
VAL DART K N KMD 004	KUMBU KUMBU	Double storey commercial property and a fencing wall								5 760 000 00
							120 000 00	4 121 584 00	4 121 584 00	5 951 984 00
VAL DART K N KMD 006	KUMBU KUMBU	Part of fencing wall and a Metal gate hung on reinforced concrete poles								920 000 00
VAL DART K N KMD 006	KUMBU KUMBU	Fencing wall	11 50	80 000 00	920 000 00		120 000 00	1 197 000 00	1 197 000 00	2 892 000 00
VAL DART K N KMD 007	KUMBU KUMBU	concrete block								
		Fencing wall	7,59	80 000 00	607 200 00					607 200 00
		Land	14,36	75 000 00	500 500 00		120 000 00	551 000 00	551 000 00	1 152 196 00
VAL DART K N M62 001	MAKUT A	Single storey residential Commercial property								
VAL DART K N M62 001	MAKUT A	Built up area	89 60	250 000 00	22 400 000 00					22 400 000 00
VAL DART K N M62 001	MAKUT A	Land								
VAL DART K N M62 002	MAKUT A	Single storey residential Commercial property								
VAL DART K N M62 002	MAKUT A	Built up area	142 33	250 000 00	35 582 500 00					35 582 500 00
VAL DART K N M62 002	MAKUT A	Land	519 00	50 000 00	25 950 000 00					25 950 000 00
VAL DART K N M62 002	MAKUT A	Outbuilding, roofed	55 019 00	150 000 00	7 952 850 00					7 952 850 00
VAL DART K N M62 002	MAKUT A	Outbuilding unroofed	25 781 00	100 000 00	2 578 100 00					2 578 100 00
VAL DART K N M62 002	MAKUT A	Fencing wall	10 00	80 000 00	800 000 00		120 000 00	920 000 00	920 000 00	35 798 450 00
VALU										
VAL DART K N M62 002	MAKUT A	Hotel and restaurant					120 000 00	1 200 000 00	1 200 000 00	3 560 600 00
VAL DART K N M62 002	MAKUT A	Single storey residential Commercial property								
VAL DART K N M62 003	MAKUT A	Outbuilding	80,40	150 000 00	12 060 000 00					12 060 000 00

VALUATIONS	LOCATIONS	DESCRIPTION	Units of Measures	RATE PER UNIT/MEASURE	VALUE OF REPAIRS	WORKING COSTS ALLOWANCE	TRANSPORT ALLOWANCE	DISURBANCE ALLOWANCE	LOSS OF PROFIT	COMPENSATION VALUE
VAL DART K N MG2 004	MAKUTA	Restaurant					120 000.00		2 240 000.00	
VAL DART K N MG2 005	MAKUTA	Single stores residential Commercial								
VAL DART K N MG2 006	MAKUTA	Workshop				120 000.00			3 240 000.00	3 360 000.00
VAL DART K N MG2 007	MAKUTA	Workshop				120 000.00			3 240 000.00	3 360 000.00
VAL DART K N MG2 008	MAKUTA	Phone accessories				120 000.00			3 240 000.00	3 360 000.00
VAL DART K N MG2 009	MAKUTA	Soft drinks				120 000.00			3 240 000.00	3 360 000.00
VAL DART K N MG2 010	MAKUTA	Hard ware				120 000.00			3 240 000.00	3 360 000.00
VAL DART K N MG2 011	MAKUTA	Boutique				120 000.00			3 240 000.00	3 360 000.00
VAL DART K N MG2 012	MAKUTA	Residential tenant			504 000.00					624 000.00
VAL DART K N MG2 013	MAKUTA	Residential tenant			504 000.00					624 000.00
VAL DART K N MNY 001	KIMARA MATANGINI	Part of office building Two pumps and paved Driveway								
VAL DART K N MNY 001	KIMARA MATANGINI	Built up area	11.50	250 000.00	2 875 000.00					2 875 000.00
VAL DART K N MNY 001	KIMARA MATANGINI	Pumps in front			20 000 000.00					20 000 000.00
VAL DART K N MNY 001	KIMARA MATANGINI	Land	28.90	35 000.00	1 011 500.00	120 000.00		1 365 520.00		2 997 020.00
VAL DART K N MNY 003	KIMARA MATANGINI	Single stores semi-detached Residential property								
VAL DART K N MNY 003	KIMARA MATANGINI	Built up area - main building	70.00	350 000.00	24 500 000.00					24 500 000.00
VAL DART K N MNY 003	KIMARA MATANGINI	Verandah	19.52	75 000.00	1 449 000.00					1 449 000.00
VAL DART K N MNY 003	KIMARA MATANGINI	Fencing wall	90.00	80 000.00	7 200 000.00					7 200 000.00
VAL DART K N MNY 003	KIMARA MATANGINI	Land	988.00	50 000.00	49 400 000.00	120 000.00		119 759 040.00		169 279 040.00
VAL DART K N MNY 004	KIMARA MATANGINI	Part of fencing wall to a residential property with a metal gate hung on reinforced concrete poles. It is constructed of sand cement blocks plastered and painted.								
VAL DART K N MNY 004	KIMARA MATANGINI	Fencing wall	15.63	80 000.00	1 250 400.00					1 250 400.00
VAL DART K N MNY 004	KIMARA MATANGINI	Land	37.02	55 000.00	2 035 700.00	120 000.00		1 227 128.00		2 617 828.00
VAL DART K N MNY 005	KIMARA MATANGINI	Part of fencing wall to a residential property with a metal gate hung on reinforced concrete poles. It is constructed of sand cement blocks plastered and painted.								
VAL DART K N MNY 005	KIMARA MATANGINI	Fencing wall	16.70	80 000.00	1 336 000.00					1 336 000.00
VAL DART K N MNY 005	KIMARA MATANGINI	Land	22.77	55 000.00	1 254 950.00	120 000.00		1 025 816.00		1 945 766.00
VAL DART K N MNY 006	KIMARA MATANGINI	Part of fencing wall to a residential property								
VAL DART K N MNY 006	KIMARA MATANGINI	Fencing wall	18.60	80 000.00	1 488 000.00					1 488 000.00
VAL DART K N MNY 006	KIMARA MATANGINI	Land	27.20	50 000.00	1 360 000.00	120 000.00		1 367 040.00		2 847 040.00

VALUATION No	LOCATION	DESCRIPTION	AREA OF BUILDING	RATE PER SQUARE METRE	VALUE OF BUILDING	COMMODIFICATION ALLOWANCE	TRANSPORT ALLOWANCE	DETERMINED ALLOWANCE	LOSS OF PROFIT	COMPENSATION VALUE
VAL DART K N N Y 005	KIMARU MATANGINI	Part of commercial property bearing a								
VAL DART K N N Y 007	KIMARU MATANGINI	Built up area	10.45	250 000.00	2 607 500.00					2 607 500.00
VAL DART K N N Y 007	KIMARU MATANGINI	Verandah	17.45	75 000.00	933 750.00					933 750.00
VAL DART K N N Y 007	KIMARU MATANGINI	Land	22.88	35 000.00	800 800.00		120 000.00	2 084 184.00		3 004 984.00
VAL DART K N U B 2 001	NH0	Single storey commercial property								
VAL DART K N U B 2 001	NH0	Built up area - main building	27.25	150 000.00	4 087 500.00					4 087 500.00
VAL DART K N U B 2 001	NH0	Verandah	6.54	75 000.00	490 500.00		120 000.00			610 500.00
VAL DART K N U B 2 001	NH0	Butchery					120 000.00		3 240 000.00	3 360 000.00
VAL DART K N U B 2 003	NH0	Single storey commercial								
VAL DART K N U B 2 003	NH0	Built up area	11.50							
VAL DART K N U B 2 004	NH0	Tenant - commercial								
VAL DART K N U B 2 004	NH0	Single storey commercial property								
VAL DART K N U B 2 004	NH0	Built up area	15.08	150 000.00	2 262 000.00		120 000.00			2 382 000.00
VAL DART K N U B 2 004	NH0	Tenant - commercial					120 000.00		3 240 000.00	3 360 000.00
VAL DART K N U B 2 007	NH0	double storey commercial property								
VAL DART K N U B 2 007	NH0	Built up area	87.00	500 000.00	43 450 000.00					43 450 000.00
VAL DART K N U B 2 007	NH0									
VAL DART K N U B 2 008	NH0	Construction								
VAL DART K N U B 2 008	NH0	Built up area	15.75	150 000.00	2 362 500.00		120 000.00		3 240 000.00	5 722 500.00
VAL DART K N U B 2 009	NH0	Single storey commercial Property under construction								
VAL DART K N U B 2 009	NH0	Built up area	7.51	150 000.00	1 129 500.00		120 000.00		3 240 000.00	4 489 500.00
VAL DART K N U B 2 010	NH0	Single storey commercial Property under construction. It is about 50% complete								
VAL DART K N U B 2 010	NH0		7.50	150 000.00	1 125 000.00		120 000.00			1 245 000.00
VAL DART K N U B 2 014	NH0	Built up area	14.64	150 000.00	2 196 000.00				3 240 000.00	5 436 000.00
VAL DART K N U B 2 014	NH0	Retail shop							3 240 000.00	5 436 000.00
VAL DART K N U B 2 017	NH0	Built up area	11.78	150 000.00	1 767 000.00		120 000.00		3 240 000.00	5 067 000.00
VAL DART K N U B 2 017	NH0	Single storey commercial building								
VAL DART K N U B 2 017	NH0	Built up area	17.48	150 000.00	2 622 000.00		120 000.00		3 240 000.00	5 982 000.00
VAL DART K N U B 2 020	NH0	Fencing wall constructed of sand cement blocks	117.80	80 000.00	9 424 000.00		120 000.00			9 544 000.00
VAL DART K N U B 2 021	NH0	Single storey commercial Property								
VAL DART K N U B 2 021	NH0	Built up area	66.55	200 000.00	13 310 000.00		120 000.00			13 430 000.00
VAL DART K N U B 2 021	NH0	Office accommodation							3 240 000.00	13 430 000.00
VAL DART K N U B 2 021	NH0	There is a work shop shade								
VAL DART K N U B 2 022	NH0	This is part of a bare land with a light fence. No access given								

Annex 2 - Part I
Data Bank related to Sections 01, 02, 03, and 04, of the Property Survey Disposition.

Code of the Property Survey Inspection & Valuation Report	Partially or totally affected	If partially affected how many items	Land Use	Is Land Rent regularly paid and accepted by the Government?	Is Property Tax regularly paid	Is Land occupancy underlying some form of regularisation procedure	Is the unit affected part of a public ROW?	Sanitary treatment system	Water	Is it a metered?	Electrical Network	Is it an energy network?	Payment type of the street in front of the property.
VAL PART 01 - REN 01	0/0		Urbano										Asfalto
VAL PART 01 - REN 02	0/0		Urbano										Asfalto
VAL PART 01 - REN 03	0/0		Urbano										Asfalto
VAL PART 01 - REN 04	0/0		Urbano										Asfalto
VAL PART 01 - REN 05	0/0		Urbano										Asfalto
VAL PART 01 - REN 06	0/0		Urbano										Asfalto
VAL PART 01 - REN 07	0/0		Urbano										Asfalto
VAL PART 01 - REN 08	0/0		Urbano										Asfalto
VAL PART 01 - REN 09	0/0		Urbano										Asfalto
VAL PART 01 - REN 10	0/0		Urbano										Asfalto
VAL PART 01 - REN 11	0/0		Urbano										Asfalto
VAL PART 01 - REN 12	0/0		Urbano										Asfalto
VAL PART 01 - REN 13	0/0		Urbano										Asfalto
VAL PART 01 - REN 14	0/0		Urbano										Asfalto
VAL PART 01 - REN 15	0/0		Urbano										Asfalto
VAL PART 01 - REN 16	0/0		Urbano										Asfalto
VAL PART 01 - REN 17	0/0		Urbano										Asfalto
VAL PART 01 - REN 18	0/0		Urbano										Asfalto
VAL PART 01 - REN 19	0/0		Urbano										Asfalto
VAL PART 01 - REN 20	0/0		Urbano										Asfalto
VAL PART 01 - REN 21	0/0		Urbano										Asfalto
VAL PART 01 - REN 22	0/0		Urbano										Asfalto
VAL PART 01 - REN 23	0/0		Urbano										Asfalto
VAL PART 01 - REN 24	0/0		Urbano										Asfalto
VAL PART 01 - REN 25	0/0		Urbano										Asfalto
VAL PART 01 - REN 26	0/0		Urbano										Asfalto
VAL PART 01 - REN 27	0/0		Urbano										Asfalto
VAL PART 01 - REN 28	0/0		Urbano										Asfalto
VAL PART 01 - REN 29	0/0		Urbano										Asfalto
VAL PART 01 - REN 30	0/0		Urbano										Asfalto
VAL PART 01 - REN 31	0/0		Urbano										Asfalto
VAL PART 01 - REN 32	0/0		Urbano										Asfalto
VAL PART 01 - REN 33	0/0		Urbano										Asfalto
VAL PART 01 - REN 34	0/0		Urbano										Asfalto
VAL PART 01 - REN 35	0/0		Urbano										Asfalto
VAL PART 01 - REN 36	0/0		Urbano										Asfalto
VAL PART 01 - REN 37	0/0		Urbano										Asfalto
VAL PART 01 - REN 38	0/0		Urbano										Asfalto
VAL PART 01 - REN 39	0/0		Urbano										Asfalto
VAL PART 01 - REN 40	0/0		Urbano										Asfalto
VAL PART 01 - REN 41	0/0		Urbano										Asfalto
VAL PART 01 - REN 42	0/0		Urbano										Asfalto
VAL PART 01 - REN 43	0/0		Urbano										Asfalto
VAL PART 01 - REN 44	0/0		Urbano										Asfalto
VAL PART 01 - REN 45	0/0		Urbano										Asfalto
VAL PART 01 - REN 46	0/0		Urbano										Asfalto
VAL PART 01 - REN 47	0/0		Urbano										Asfalto
VAL PART 01 - REN 48	0/0		Urbano										Asfalto
VAL PART 01 - REN 49	0/0		Urbano										Asfalto
VAL PART 01 - REN 50	0/0		Urbano										Asfalto
VAL PART 01 - REN 51	0/0		Urbano										Asfalto
VAL PART 01 - REN 52	0/0		Urbano										Asfalto
VAL PART 01 - REN 53	0/0		Urbano										Asfalto
VAL PART 01 - REN 54	0/0		Urbano										Asfalto
VAL PART 01 - REN 55	0/0		Urbano										Asfalto
VAL PART 01 - REN 56	0/0		Urbano										Asfalto
VAL PART 01 - REN 57	0/0		Urbano										Asfalto
VAL PART 01 - REN 58	0/0		Urbano										Asfalto
VAL PART 01 - REN 59	0/0		Urbano										Asfalto
VAL PART 01 - REN 60	0/0		Urbano										Asfalto
VAL PART 01 - REN 61	0/0		Urbano										Asfalto
VAL PART 01 - REN 62	0/0		Urbano										Asfalto
VAL PART 01 - REN 63	0/0		Urbano										Asfalto
VAL PART 01 - REN 64	0/0		Urbano										Asfalto
VAL PART 01 - REN 65	0/0		Urbano										Asfalto
VAL PART 01 - REN 66	0/0		Urbano										Asfalto
VAL PART 01 - REN 67	0/0		Urbano										Asfalto
VAL PART 01 - REN 68	0/0		Urbano										Asfalto
VAL PART 01 - REN 69	0/0		Urbano										Asfalto
VAL PART 01 - REN 70	0/0		Urbano										Asfalto
VAL PART 01 - REN 71	0/0		Urbano										Asfalto
VAL PART 01 - REN 72	0/0		Urbano										Asfalto
VAL PART 01 - REN 73	0/0		Urbano										Asfalto
VAL PART 01 - REN 74	0/0		Urbano										Asfalto
VAL PART 01 - REN 75	0/0		Urbano										Asfalto
VAL PART 01 - REN 76	0/0		Urbano										Asfalto
VAL PART 01 - REN 77	0/0		Urbano										Asfalto
VAL PART 01 - REN 78	0/0		Urbano										Asfalto
VAL PART 01 - REN 79	0/0		Urbano										Asfalto
VAL PART 01 - REN 80	0/0		Urbano										Asfalto
VAL PART 01 - REN 81	0/0		Urbano										Asfalto
VAL PART 01 - REN 82	0/0		Urbano										Asfalto
VAL PART 01 - REN 83	0/0		Urbano										Asfalto
VAL PART 01 - REN 84	0/0		Urbano										Asfalto
VAL PART 01 - REN 85	0/0		Urbano										Asfalto
VAL PART 01 - REN 86	0/0		Urbano										Asfalto
VAL PART 01 - REN 87	0/0		Urbano										Asfalto
VAL PART 01 - REN 88	0/0		Urbano										Asfalto
VAL PART 01 - REN 89	0/0		Urbano										Asfalto
VAL PART 01 - REN 90	0/0		Urbano										Asfalto
VAL PART 01 - REN 91	0/0		Urbano										Asfalto
VAL PART 01 - REN 92	0/0		Urbano										Asfalto
VAL PART 01 - REN 93	0/0		Urbano										Asfalto
VAL PART 01 - REN 94	0/0		Urbano										Asfalto
VAL PART 01 - REN 95	0/0		Urbano										Asfalto
VAL PART 01 - REN 96	0/0		Urbano										Asfalto
VAL PART 01 - REN 97	0/0		Urbano										Asfalto
VAL PART 01 - REN 98	0/0		Urbano										Asfalto
VAL PART 01 - REN 99	0/0		Urbano										Asfalto
VAL PART 01 - REN 100	0/0		Urbano										Asfalto

Legend
Yes
Not Informed


Annex 2 - Part I
This table related to Network of use and details the Property Survey Inspection

Code of the Property Survey Inspection & Validation Report	Partially or totally affected	If partially affected how many rooms	Land use	Is Land Run records and occupied by the Government?	Is Property Tax regularly paid	Is land occupancy under some form of regularization procedure	Is the unit or affected part of it within a public RDU?	Is there a common location	Water	Is sewer network	Electrical Network	If not, is energy metered?	Payment type of the property
AVI Report 1.1.1.1	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.2	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.3	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.4	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.5	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.6	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.7	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.8	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.9	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.10	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.11	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.12	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.13	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.14	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.15	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.16	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.17	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.18	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.19	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.20	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.21	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.22	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.23	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.24	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.25	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.26	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.27	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.28	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.29	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.30	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.31	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.32	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.33	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.34	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.35	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.36	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.37	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.38	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.39	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.40	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.41	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.42	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.43	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.44	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.45	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.46	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.47	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.48	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.49	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1
AVI Report 1.1.1.50	1.1.1		1.1.1					1.1.1	1.1.1				1.1.1

Annex 2 - Part 1

Data Items related to Sections 01, 02, 04, and 06 of the Property Survey Questionnaire

Code of this Property Survey Inspection & Valuation Report	Partially or totally affected	If partially affected how many rooms	Land Use	Is Land Rent regularly paid and accepted by the Government?	Is Property Tax regularly paid	Is land occupancy undergoing some form of regularization procedure	Is the unit or affected part of it within a public ROW?	Sewage treatment system	Water	Is water metered?	Electrical Network	If yes, is energy metered?	Pavement type of the street in front of the property?
VAL/DART/KN/INT/001
VAL/DART/KN/INT/002
VAL/DART/KN/INT/003
VAL/DART/KN/H/001
VAL/DART/KN/H/002
VAL/DART/KN/KMR/001
VAL/DART/KN/KMR/002
VAL/DART/KN/KMR/003
VAL/DART/KN/KMR/004
VAL/DART/KN/KMR/005
VAL/DART/KN/KMR/006
VAL/DART/KN/KMR/007
VAL/DART/KN/KMR/008	totally		Commercial						..				Earth
VAL/DART/KN/KMR/009
VAL/DART/KN/KMR/010
VAL/DART/KN/KMR/011
VAL/DART/KN/KMR/012
VAL/DART/KN/KMR/013
VAL/DART/KN/KMR/014
VAL/DART/KN/KMR/015
VAL/DART/KN/KMR/016
VAL/DART/KN/KMR/017
VAL/DART/KN/KMR/018
VAL/DART/KN/KMR/019
VAL/DART/KN/KMR/020
VAL/DART/KN/KMR/021
VAL/DART/KN/KMR/022
VAL/DART/KN/KMR/023	totally		Commercial						..				..
VAL/DART/KN/KMR/024

Legend


 Yes
 Not-Not Informed

Annex 2 - Part I
Data Bank related to Sections 01, 02, 05 and 06 of the Property Survey Ordinance.

Code of the Property Survey Inspection & Valuation Report	Partially or totally affected	It partially affected, how many items	Land Use	Is Land Rent regularly paid and accepted by the Government?	Is Property Tax regularly paid	Is land occupancy underlying some form of regularization procedure	Is the unit affected part of a public ROW?	sewer treatment system	Wells	It was metered?	Electrical Network	It was metered?	Payment type of the street in front of the property.
VAL 0001 KSNM000	Y		06.10										1
VAL 0002 KSNM000	Y		06.10										1
VAL 0003 KSNM000	Y		06.10										1
VAL 0004 KSNM000	Y		06.10										1
VAL 0005 KSNM000	Y		06.10										1
VAL 0006 KSNM000	Y		06.10										1
VAL 0007 KSNM000	Y		06.10										1
VAL 0008 KSNM000	Y		06.10										1
VAL 0009 KSNM000	Y		06.10										1
VAL 0010 KSNM000	Y		06.10										1
VAL 0011 KSNM000	Y		06.10										1
VAL 0012 KSNM000	Y		06.10										1
VAL 0013 KSNM000	Y		06.10										1
VAL 0014 KSNM000	Y		06.10										1
VAL 0015 KSNM000	Y		06.10										1
VAL 0016 KSNM000	Y		06.10										1
VAL 0017 KSNM000	Y		06.10										1
VAL 0018 KSNM000	Y		06.10										1
VAL 0019 KSNM000	Y		06.10										1
VAL 0020 KSNM000	Y		06.10										1
VAL 0021 KSNM000	Y		06.10										1
VAL 0022 KSNM000	Y		06.10										1
VAL 0023 KSNM000	Y		06.10										1
VAL 0024 KSNM000	Y		06.10										1
VAL 0025 KSNM000	Y		06.10										1
VAL 0026 KSNM000	Y		06.10										1
VAL 0027 KSNM000	Y		06.10										1
VAL 0028 KSNM000	Y		06.10										1
VAL 0029 KSNM000	Y		06.10										1
VAL 0030 KSNM000	Y		06.10										1
VAL 0031 KSNM000	Y		06.10										1
VAL 0032 KSNM000	Y		06.10										1
VAL 0033 KSNM000	Y		06.10										1
VAL 0034 KSNM000	Y		06.10										1
VAL 0035 KSNM000	Y		06.10										1
VAL 0036 KSNM000	Y		06.10										1
VAL 0037 KSNM000	Y		06.10										1
VAL 0038 KSNM000	Y		06.10										1
VAL 0039 KSNM000	Y		06.10										1
VAL 0040 KSNM000	Y		06.10										1
VAL 0041 KSNM000	Y		06.10										1
VAL 0042 KSNM000	Y		06.10										1
VAL 0043 KSNM000	Y		06.10										1
VAL 0044 KSNM000	Y		06.10										1
VAL 0045 KSNM000	Y		06.10										1
VAL 0046 KSNM000	Y		06.10										1
VAL 0047 KSNM000	Y		06.10										1
VAL 0048 KSNM000	Y		06.10										1
VAL 0049 KSNM000	Y		06.10										1
VAL 0050 KSNM000	Y		06.10										1
VAL 0051 KSNM000	Y		06.10										1
VAL 0052 KSNM000	Y		06.10										1
VAL 0053 KSNM000	Y		06.10										1
VAL 0054 KSNM000	Y		06.10										1
VAL 0055 KSNM000	Y		06.10										1
VAL 0056 KSNM000	Y		06.10										1
VAL 0057 KSNM000	Y		06.10										1
VAL 0058 KSNM000	Y		06.10										1
VAL 0059 KSNM000	Y		06.10										1
VAL 0060 KSNM000	Y		06.10										1
VAL 0061 KSNM000	Y		06.10										1
VAL 0062 KSNM000	Y		06.10										1
VAL 0063 KSNM000	Y		06.10										1
VAL 0064 KSNM000	Y		06.10										1
VAL 0065 KSNM000	Y		06.10										1
VAL 0066 KSNM000	Y		06.10										1
VAL 0067 KSNM000	Y		06.10										1
VAL 0068 KSNM000	Y		06.10										1
VAL 0069 KSNM000	Y		06.10										1
VAL 0070 KSNM000	Y		06.10										1
VAL 0071 KSNM000	Y		06.10										1
VAL 0072 KSNM000	Y		06.10										1
VAL 0073 KSNM000	Y		06.10										1
VAL 0074 KSNM000	Y		06.10										1
VAL 0075 KSNM000	Y		06.10										1
VAL 0076 KSNM000	Y		06.10										1
VAL 0077 KSNM000	Y		06.10										1
VAL 0078 KSNM000	Y		06.10										1
VAL 0079 KSNM000	Y		06.10										1
VAL 0080 KSNM000	Y		06.10										1
VAL 0081 KSNM000	Y		06.10										1
VAL 0082 KSNM000	Y		06.10										1
VAL 0083 KSNM000	Y		06.10										1
VAL 0084 KSNM000	Y		06.10										1
VAL 0085 KSNM000	Y		06.10										1
VAL 0086 KSNM000	Y		06.10										1
VAL 0087 KSNM000	Y		06.10										1
VAL 0088 KSNM000	Y		06.10										1
VAL 0089 KSNM000	Y		06.10										1
VAL 0090 KSNM000	Y		06.10										1
VAL 0091 KSNM000	Y		06.10										1
VAL 0092 KSNM000	Y		06.10										1
VAL 0093 KSNM000	Y		06.10										1
VAL 0094 KSNM000	Y		06.10										1
VAL 0095 KSNM000	Y		06.10										1
VAL 0096 KSNM000	Y		06.10										1
VAL 0097 KSNM000	Y		06.10										1
VAL 0098 KSNM000	Y		06.10										1
VAL 0099 KSNM000	Y		06.10										1
VAL 0100 KSNM000	Y		06.10										1

Legend
Yes
Not Informed

Annex 2 - Part 1
Data Bank related to Section 41 & 42 as part of the Property Survey Questionnaire

Code of the Property Survey Inspection & Valuation Report	Partially or totally affected	If partially affected how many items	Land Use	Is Land Use regularly paid and accepted by the Government?	Is Property Tax regularly paid	Is land occupancy undergiving some form of regularization procedure	In the unit or affected part of a within a public ROW?	Sewage treatment system	Water	Is water metering?	Electrical Network	Is electricity metered?	Payment type of the street in front of the property
VALPROPENST001	Y	1	Commercial										Y
VALPROPENST002	Y	1	Commercial										Y
VALPROPENST003	Y	1	Commercial										Y
VALPROPENST004	Y	1	Commercial										Y
VALPROPENST005	Y	1	Commercial										Y
VALPROPENST006	Y	1	Commercial										Y
VALPROPENST007	Y	1	Commercial										Y
VALPROPENST008	Y	1	Commercial										Y
VALPROPENST009	Y	1	Commercial										Y
VALPROPENST010	Y	1	Commercial										Y
VALPROPENST011	Y	1	Commercial										Y
VALPROPENST012	Y	1	Commercial										Y
VALPROPENST013	Y	1	Commercial										Y
VALPROPENST014	Y	1	Commercial										Y
VALPROPENST015	Y	1	Commercial										Y
VALPROPENST016	Y	1	Commercial										Y
VALPROPENST017	Y	1	Commercial										Y
VALPROPENST018	Y	1	Commercial										Y
VALPROPENST019	Y	1	Commercial										Y
VALPROPENST020	Y	1	Commercial										Y
VALPROPENST021	Y	1	Commercial										Y
VALPROPENST022	Y	1	Commercial										Y
VALPROPENST023	Y	1	Commercial										Y
VALPROPENST024	Y	1	Commercial										Y
VALPROPENST025	Y	1	Commercial										Y
VALPROPENST026	Y	1	Commercial										Y
VALPROPENST027	Y	1	Commercial										Y
VALPROPENST028	Y	1	Commercial										Y
VALPROPENST029	Y	1	Commercial										Y
VALPROPENST030	Y	1	Commercial										Y
VALPROPENST031	Y	1	Commercial										Y
VALPROPENST032	Y	1	Commercial										Y
VALPROPENST033	Y	1	Commercial										Y
VALPROPENST034	Y	1	Commercial										Y
VALPROPENST035	Y	1	Commercial										Y
VALPROPENST036	Y	1	Commercial										Y
VALPROPENST037	Y	1	Commercial										Y
VALPROPENST038	Y	1	Commercial										Y
VALPROPENST039	Y	1	Commercial										Y
VALPROPENST040	Y	1	Commercial										Y
VALPROPENST041	Y	1	Commercial										Y
VALPROPENST042	Y	1	Commercial										Y
VALPROPENST043	Y	1	Commercial										Y
VALPROPENST044	Y	1	Commercial										Y
VALPROPENST045	Y	1	Commercial										Y
VALPROPENST046	Y	1	Commercial										Y
VALPROPENST047	Y	1	Commercial										Y
VALPROPENST048	Y	1	Commercial										Y
VALPROPENST049	Y	1	Commercial										Y
VALPROPENST050	Y	1	Commercial										Y
VALPROPENST051	Y	1	Commercial										Y
VALPROPENST052	Y	1	Commercial										Y
VALPROPENST053	Y	1	Commercial										Y
VALPROPENST054	Y	1	Commercial										Y
VALPROPENST055	Y	1	Commercial										Y
VALPROPENST056	Y	1	Commercial										Y
VALPROPENST057	Y	1	Commercial										Y
VALPROPENST058	Y	1	Commercial										Y
VALPROPENST059	Y	1	Commercial										Y
VALPROPENST060	Y	1	Commercial										Y
VALPROPENST061	Y	1	Commercial										Y
VALPROPENST062	Y	1	Commercial										Y
VALPROPENST063	Y	1	Commercial										Y
VALPROPENST064	Y	1	Commercial										Y
VALPROPENST065	Y	1	Commercial										Y
VALPROPENST066	Y	1	Commercial										Y
VALPROPENST067	Y	1	Commercial										Y
VALPROPENST068	Y	1	Commercial										Y
VALPROPENST069	Y	1	Commercial										Y
VALPROPENST070	Y	1	Commercial										Y
VALPROPENST071	Y	1	Commercial										Y
VALPROPENST072	Y	1	Commercial										Y
VALPROPENST073	Y	1	Commercial										Y
VALPROPENST074	Y	1	Commercial										Y
VALPROPENST075	Y	1	Commercial										Y
VALPROPENST076	Y	1	Commercial										Y
VALPROPENST077	Y	1	Commercial										Y
VALPROPENST078	Y	1	Commercial										Y
VALPROPENST079	Y	1	Commercial										Y
VALPROPENST080	Y	1	Commercial										Y
VALPROPENST081	Y	1	Commercial										Y
VALPROPENST082	Y	1	Commercial										Y
VALPROPENST083	Y	1	Commercial										Y
VALPROPENST084	Y	1	Commercial										Y
VALPROPENST085	Y	1	Commercial										Y
VALPROPENST086	Y	1	Commercial										Y
VALPROPENST087	Y	1	Commercial										Y
VALPROPENST088	Y	1	Commercial										Y
VALPROPENST089	Y	1	Commercial										Y
VALPROPENST090	Y	1	Commercial										Y
VALPROPENST091	Y	1	Commercial										Y
VALPROPENST092	Y	1	Commercial										Y
VALPROPENST093	Y	1	Commercial										Y
VALPROPENST094	Y	1	Commercial										Y
VALPROPENST095	Y	1	Commercial										Y
VALPROPENST096	Y	1	Commercial										Y
VALPROPENST097	Y	1	Commercial										Y
VALPROPENST098	Y	1	Commercial										Y
VALPROPENST099	Y	1	Commercial										Y
VALPROPENST100	Y	1	Commercial										Y

Legend
Yes
Not informed

Annex 2 - Part 2

Data Bank related to Section 04 of the Property Survey Questionnaire

Code of this Property Survey Inspection & Valuation Report	Room N°	Current Use	Rented or Owner-Occupied (r/o)
VAL/DART/KN/UB2/001	Room N° 1	Other retail or services	R
VAL/DART/KN/UB2/001	Room N° 2	Other retail or services	R
VAL/DART/KN/UB2/002	Room N° 1	Other retail or services	R
VAL/DART/KN/UB2/002	Room N° 2	Other retail or services	R
VAL/DART/KN/UB2/003	Room N° 1	Other retail or services	R
VAL/DART/KN/UB2/004	Room N° 1	Other retail or services	R
VAL/DART/KN/UB2/005	Room N° 1	Other retail or services	R
VAL/DART/KN/UB2/006	Room N° 1	Other retail or services	R
VAL/DART/KN/UB2/008	Room N° 1	Other retail or services	R
VAL/DART/KN/UB2/008	Room N° 2	Other retail or services	R

Annex 2 - Part 3

Data Bank related to Section 07 of the Property Survey Questionnaire

Code of this Property Survey Inspection & Valuation Report	Affected Party	Compensation for Land	Compensation for Structures	Disturbance Allowance	Accomod Allowance	Transport Allowance	Loss of Profit
VAL/DART II GRN 001	Tenant - Business - Owner						
VAL/DART II GRN 001	Owner-occupier - resident						
VAL/DART II GRN 001	Tenant - Business - Owner						
VAL/DART II GRN 002	Owner-occupier - resident						
VAL/DART II GRN 002	Tenant - Business - Owner						
VAL/DART II GRN 002	Tenant - Business - Owner						
VAL/DART II GRN 002	Tenant - Business - Owner						
VAL/DART II GRN 003	Owner-occupier - resident						
VAL/DART II GRN 003	Tenant - Business - Owner						
VAL/DART II GRN 003	Tenant - Business - Owner						
VAL/DART II GRN 003	Tenant - Business - Owner						
VAL/DART II GRN 003	Tenant - Business - Owner						
VAL/DART II GRN 004	Owner-occupier - resident						
VAL/DART II GRN 004	Tenant - Business - Owner						
VAL/DART/IL/GRN/004	Tenant - Business - Owner						
VAL/DART/IL/GRN/004	Tenant - Business - Owner						
VAL/DART/IL/GRN/005	Owner-occupier - resident						
VAL/DART/IL/GRN/005	Tenant - Business - Owner						
VAL/DART/IL/GRN/005	Tenant - Business - Owner						
VAL/DART/IL/GRN/005	Tenant - Business - Owner						
VAL/DART II GRN 006	Owner-occupier - resident						
VAL/DART II GRN 006	Tenant - Business - Owner						
VAL/DART II GRN 006	Tenant - Business - Owner						
VAL/DART II GRN 006	Tenant - Business - Owner						
VAL/DART II GRN 006	Tenant - Business - Owner						
VAL/DART II GRN 007	Owner-occupier - resident						
VAL/DART II GRN 008	Tenant - resident						
VAL/DART II GRN 008	Tenant - Business - Owner						
VAL/DART II GRN 008	Tenant - Business - Owner						
VAL/DART II GRN 008	Tenant - Business - Owner						
VAL/DART II GRN 008	Tenant - Business - Owner						
VAL/DART II GRN 008	Tenant - Business - Owner						
VAL/DART II GRN 008	Owner-occupier - resident						

Legend
 Yes
 No/Not informed

Annex 2 - Part 3
Data Bank related to Section 07 of the Property Survey Questionnaire

Code of this Property Survey Inspection & Valuation Report	Affected Party	Compensation for Land	Compensation for Structures	Disturbance Allowance	Accomod Allowance	Transport Allowance	Loss of Profit
VAL/DART II GRN 009	Landlord (absent property) Owner						
VAL/DART II GRN 009	Tenant - Business Owner						
VAL/DART II GRN 009	Tenant - Business Owner						
VAL/DART II GRN 009	Tenant - Business Owner						
VAL/DART II GRN 009	Tenant - Business Owner						
VAL/DART II GRN 010	Owner occupier - resident						
VAL/DART II GRN 010	Tenant - Business Owner						
VAL/DART II GRN 011	Landlord (absent property) Owner						
VAL/DART II GRN 011	Tenant - resident						
VAL/DART II GRN 011	Tenant - Business Owner						
VAL/DART II GRN 011	Tenant - Business Owner						
VAL/DART II GRN 011	Tenant - Business Owner						
VAL/DART II GRN 012	Owner occupier - resident						
VAL/DART II GRN 012	Tenant - Business Owner						
VAL/DART II GRN 012	Tenant - Business Owner						
VAL/DART II GRN 012	Tenant - Business Owner						
VAL/DART II GRN 013	Tenant - Business Owner						
VAL/DART II GRN 013	Tenant - Business Owner						
VAL/DART II GRN 013	Tenant - Business Owner						
VAL/DART II GRN 013	Tenant - Business Owner						
VAL/DART II GRN 013	Tenant - Business Owner						
VAL/DART II GRN 013	Owner occupier - resident						
VAL/DART II GRN 014	Owner occupier - resident						
VAL/DART II GRN 015	Tenant - Business Owner						
VAL/DART II GRN 015	Tenant - Business Owner						
VAL/DART II GRN 015	Owner occupier - resident						
VAL/DART II GRN 016	Owner occupier - resident						
VAL/DART II GRN 016	Tenant - Business Owner						
VAL/DART II GRN 016	Tenant - Business Owner						
VAL/DART II GRN 016	Tenant - Business Owner						
VAL/DART II GRN 016	Tenant - Business Owner						
VAL/DART II GRN 017	Owner occupier - resident						

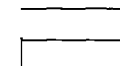
Legend
 Yes
 No/Not informed

Annex 2 - Part 3

Data Bank related to Section 07 of the Property Survey Questionnaire

Code of this Property Survey Inspection & Valuation Report	Affected Party	Compensation for Land	Compensation for Structures	Disturbance Allowance	Accomod Allowance	Transport Allowance	Loss of Profit
VAL DART II GRN 017	Tenant - Business Owner						
VAL DART II GRN 017	Tenant - Business Owner						
VAL DART II GRN 017	Tenant - Business Owner						
VAL DART II GRN 017	Tenant - Business Owner						
VAL DART II GRN 018	Owner-occupier - resident						
VAL DART II GRN 018	Tenant - Business Owner						
VAL DART II GRN 018	Tenant - Business Owner						
VAL DART II GRN 019	Owner-occupier - resident						
VAL DART II GRN 020	Owner-occupier - resident						
VAL DART II GRN 020	Tenant - Business Owner						
VAL DART II GRN 021	Owner-occupier - resident						
VAL DART II GRN 022	Owner-occupier - Business Owner						
VAL DART II GRN 022	Owner-occupier - resident						
VAL DART II GRN 023	Owner-occupier - resident						
VAL DART II GRN 024	Owner-occupier - resident						
VAL DART II GRN 025	Tenant - Business Owner						
VAL DART II GRN 025	Owner-occupier - resident						
VAL DART II GRN 026	Owner-occupier - resident						
VAL DART II GRN 027	Owner-occupier - Business Owner						
VAL DART II GRN 027	Owner-occupier - resident						
VAL DART II GRN 028	Owner-occupier - resident						
VAL DART II GRN 029	Owner-occupier - resident						
VAL DART II GRN 030	Owner-occupier - resident						
VAL DART II GRN 031	Owner-occupier - resident						
VAL DART II GRN 031	Owner-occupier - Business Owner						
VAL DART II GRN 032	Owner-occupier - resident						
VAL DART II GRN 033	Owner-occupier - resident						
VAL DART II GRN 034	Owner-occupier - resident						
VAL DART II GRN 034	Owner-occupier - Business Owner						
VAL DART II GRN 035	Owner-occupier - resident						
VAL DART II GRN 036	Owner-occupier - Business Owner						
VAL DART II GRN 036	Owner-occupier - resident						
VAL DART II GRN 038	Owner-occupier - resident						

Legend



Yes

No/Not informed

Annex 2 - Part 3

Data Bank related to Section 07 of the Property Survey Questionnaire

Code of this Property Survey Inspection & Valuation Report	Affected Party	Compensation for Land	Compensation for Structures	Disturbance Allowance	Accomod Allowance	Transport Allowance	Loss of Profit
VAL/DART/II GRN 038	Owner occupier - Business Owner						
VAL/DART/II GRN 037	Owner occupier - resident						
VAL/DART/II GRN 039	Owner occupier - resident						
VAL/DART/II GRN 040	Owner occupier - resident						
VAL/DART/II GRN 041	Owner occupier - resident						
VAL/DART/II GRN 042	Owner occupier - resident						
VAL/DART/II GRN 043	Owner occupier - resident						
VAL/DART/II GRN 044	Owner occupier - resident						
VAL/DART/II GRN 045	Owner occupier - resident						
VAL/DART/II GRN 046	Owner occupier - resident						
VAL/DART/II GRN 047	Owner occupier - Business Owner						
VAL/DART/II GRN 047	Owner occupier - resident						
VAL/DART/II GRN 048	Owner occupier - resident						
VAL/DART/II GRN 049	Owner occupier - resident						
VAL/DART/II GRN 050	Owner occupier - resident						
VAL/DART/II GRN 051	Owner occupier - resident						
VAL/DART/II GRN 052	Owner occupier - resident						
VAL/DART/II GRN 053	Owner occupier - resident						
VAL/DART/II GRN 054	Owner occupier - resident						
VAL/DART/II GRN 055	Owner occupier - resident						
VAL/DART/II GRN 056	Owner occupier - resident						
VAL/DART/II GRN 057	Owner occupier - resident						
VAL/DART/II GRN 058	Owner occupier - resident						
VAL/DART/II GRN 059	Owner occupier - resident						
VAL/DART/II GRN 060	Owner occupier - resident						
VAL/DART/II GRN 061	Owner occupier - resident						
VAL/DART/II GRN 062	Owner occupier - resident						
VAL/DART/II GRN 063	Owner occupier - Business Owner						
VAL/DART/II GRN 063	Owner occupier - resident						
VAL/DART/II GRN 064	Owner occupier - resident						
VAL/DART/II GRN 065	Owner occupier - resident						
VAL/DART/II GRN 066	Owner occupier - resident						
VAL/DART/II GRN 067	Owner occupier - resident						

Legend
 Yes
 No/Not informed

Annex 2 - Part 3

Data Bank related to Section 07 of the Property Survey Questionnaire

Code of this Property Survey Inspection & Valuation Report	Affected Party	Compensation for Land	Compensation for Structures	Disturbance Allowance	Accomod Allowance	Transport Allowance	Loss of Profit
VAL DART II GRN 068	Owner occupier - Business Owner						
VAL DART II GRN 068	Owner occupier - resident						
VAL DART II GRN 069	Owner occupier - Business Owner						
VAL DART II GRN 069	Owner occupier - resident						
VAL DART II GRN 070	Owner occupier - resident						
VAL DART II GRN 071	Owner occupier - resident						
VAL DART II GRN 072	Owner occupier - resident						
VAL DART II GRN 073	Owner occupier - resident						
VAL DART II GRN 074	Owner occupier - Business Owner						
VAL DART II GRN 074	Owner occupier - resident						
VAL DART II GRN 075	Owner occupier - Business Owner						
VAL DART II GRN 075	Owner occupier - resident						
VAL DART II GRN 076	Owner occupier - resident						
VAL DART II GRN 077	Owner occupier - Business Owner						
VAL DART II GRN 077	Owner occupier - resident						
VAL DART II GRN 078	Owner occupier - Business Owner						
VAL DART II GRN 078	Owner occupier - resident						
VAL DART II GRN 079	Owner occupier - resident						
VAL DART II GRN 080	Owner occupier - resident						
VAL DART II GRN 081	Tenant - resident						
VAL DART II GRN 081	Owner occupier - Business Owner						
VAL DART II GRN 082	Owner occupier - resident						
VAL DART II GRN 083	Owner occupier - resident						
VAL DART II GRN 085	Owner occupier - resident						
VAL DART II GRN 085	Owner occupier - Business Owner						
VAL DART II GRN 084	Owner occupier - resident						
VAL DART II GRN 086	Owner occupier - Business Owner						
VAL DART II GRN 086	Owner occupier - resident						
VAL DART II GRN 087	Owner occupier - resident						
VAL DART II GRN 088	Owner occupier - resident						
VAL DART II GRN 089	Owner occupier - resident						
VAL DART II GRN 090	Owner occupier - resident						
VAL DART II GRN 091	Owner occupier - resident						

Legend
 Yes
 No/Not Informed

Annex 2 - Part 3

Data Bank related to Section 07 of the Property Survey Questionnaire

Code of this Property Survey Inspection & Valuation Report	Affected Party	Compensation for Land	Compensation for Structures	Disturbance Allowance	Accomod Allowance	Transport Allowance	Loss of Profit
VAL DART II GRN 092	Owner-occupier - resident						
VAL DART II GRN 093	Owner-occupier - Business Owner						
VAL DART II GRN 093	Owner-occupier - resident						
VAL DART II GRN 094	Owner-occupier - resident						
VAL DART II GRN 095	Owner-occupier - resident						
VAL DART II GRN 096	Owner-occupier - resident						
VAL DART II GRN 097	Owner-occupier - Business Owner						
VAL DART II GRN 097	Owner-occupier - resident						
VAL DART II GRN 098	Owner-occupier - Business Owner						
VAL DART II GRN 098	Owner-occupier - resident						
VAL DART II GRN 099	Owner-occupier - resident						
VAL DART II GRN 100	Owner-occupier - Business Owner						
VAL DART II GRN 100	Owner-occupier - resident						
VAL DART II GRN 101	Owner-occupier - resident						
VAL DART II GRN 102	Owner-occupier - resident						
VAL DART II GRN 103	Owner-occupier - Business Owner						
VAL DART II GRN 103	Owner-occupier - resident						
VAL DART II GRN 104	Owner-occupier - resident						
VAL DART II GRN 105	Owner-occupier - resident						
VAL DART II GRN 106	Owner-occupier - resident						
VAL DART KN HNF 006	Landlord/absent property Owner						
VAL DART KN HNF 006	Tenant - resident						
VAL DART KN HNF 006	Tenant - Business Owner						
VAL DART KN HNF 006	Tenant - Business Owner						
VAL DART KN HNF 004	Owner-occupier - Business Owner						
VAL DART KN HNF 003	Owner-occupier - Business Owner						
VAL DART KN HNF 002	Owner-occupier - Business Owner						
VAL DART KN HNF 001	Owner-occupier - Business Owner						
VAL DART KN HNF 007	Tenant - Business Owner						
VAL DART KN HNF 007	Tenant - Business Owner						
VAL DART KN HNF 007	Tenant - Business Owner						
VAL DART KN HNF 007	Tenant - Business Owner						
VAL DART KN HNF 007	Landlord/absent property Owner						

Legend
 Yes
 No/Not Intormed

Annex 2 - Part 3

Data Bank related to Section 07 of the Property Survey Questionnaire

Code of this Property Survey Inspection & Valuation Report	Affected Party	Compensation for Land	Compensation for Structures	Disturbance Allowance	Accomod Allowance	Transport Allowance	Loss of Profit
VAL DART KN HNF 008	Owner-occupier - Business Owner						
VAL DART KN HNF 009	Owner-occupier - Business Owner						
VAL DART KN HNF 010	Tenant - Business Owner						
VAL DART KN HNF 010	Tenant - Business Owner						
VAL DART KN HNF 010	Tenant - Business Owner						
VAL DART KN HNF 010	Tenant - Business Owner						
VAL DART KN HNF 010	Owner-occupier - resident						
VAL DART KN HNF 011	Tenant - resident						
VAL DART KN HNF 011	Tenant - resident						
VAL DART KN HNF 011	Tenant - resident						
VAL DART KN HNF 011	Tenant - resident						
VAL DART KN HNF 011	Tenant - resident						
VAL DART KN HNF 011	Tenant - resident						
VAL DART KN HNF 011	Tenant - Business Owner						
VAL DART KN HNF 011	Tenant - Business Owner						
VAL DART KN HNF 011	Tenant - Business Owner						
VAL DART KN HNF 011	Tenant - Business Owner						
VAL DART KN HNF 011	Tenant - Business Owner						
VAL DART KN HNF 011	Tenant - Business Owner						
VAL DART KN HNF 011	Landlord (absent property) Owners						
VAL DART KN HNF 012	Tenant - resident						
VAL DART KN HNF 012	Tenant - resident						
VAL DART KN HNF 012	Tenant - resident						
VAL DART KN HNF 012	Tenant - resident						
VAL DART KN HNF 012	Tenant - Business Owner						
VAL DART KN HNF 012	Tenant - Business Owner						
VAL DART KN HNF 012	Landlord (absent property) Owners						
VAL DART KN KMR 001	Owner-occupier - resident						
VAL DART KN KMR 001	Tenant - resident						
VAL DART KN KMR 001	Tenant - resident						
VAL DART KN KMR 002	Owner-occupier - resident						
VAL DART KN KMR 002	Tenant - resident						

Legend
 Yes
 No/Not Informd

Annex 2 - Part 3

Data Bank related to Section 07 of the Property Survey Questionnaire

Code of this Property Survey Inspection & Valuation Report	Affected Party	Compensation for Land	Compensation for Structures	Disturbance Allowance	Accomod Allowance	Transport Allowance	Loss of Profit
VAL/DART KN KMR 002	Tenant - Business - Owner						
VAL/DART KN KMR 003	Owner-occupier - resident						
VAL/DART KN KMR 003	Tenant - Business - Owner						
VAL/DART KN KMR 003	Tenant - resident						
VAL/DART KN KMR 004	Owner-occupier - resident						
VAL/DART KN KMR 004	Owner-occupier - Business - Owner						
VAL/DART KN KMR 004	Tenant - resident						
VAL/DART KN KMR 004	Tenant - resident						
VAL/DART KN KMR 004	Tenant - resident						
VAL/DART KN KMR 004	Tenant - resident						
VAL/DART KN KMR 004	Tenant - Business - Owner						
VAL/DART KN KMR 005	Owner-occupier - resident						
VAL/DART KN KMR 005	Tenant - resident						
VAL/DART KN KMR 005	Tenant - resident						
VAL/DART KN KMR 005	Tenant - resident						
VAL/DART KN KMR 005	Tenant - resident						
VAL/DART KN KMR 005	Tenant - Business - Owner						
VAL/DART KN KMR 005	Tenant - Business - Owner						
VAL/DART KN KMR 005	Tenant - Business - Owner						
VAL/DART KN KMR 005	Tenant - Business - Owner						
VAL/DART KN KMR 005	Tenant - Business - Owner						
VAL/DART KN KMR 006	Landlord (absent property) - Owner						
VAL/DART KN KMR 006	Tenant - Business - Owner						
VAL/DART KN KMR 006	Tenant - Business - Owner						
VAL/DART KN KMR 007	Owner-occupier - resident						
VAL/DART KN KMR 007	Tenant - Business - Owner						
VAL/DART KN KMR 008	Landlord (absent property) - Owner						
VAL/DART KN KMR 008	Tenant - Business - Owner						
VAL/DART KN KMR 008	Tenant - Business - Owner						
VAL/DART KN KMR 009	Owner-occupier - resident						
VAL/DART KN KMR 009	Tenant - Business - Owner						
VAL/DART KN KMR 009	Tenant - Business - Owner						

Legend



Yes

No/Not Inform

Annex 2 - Part 3

Data Bank related to Section 07 of the Property Survey Questionnaire

Code of this Property Survey Inspection & Valuation Report	Affected Party	Compensation for Land	Compensation for Structures	Disturbance Allowance	Accomod Allowance	Transport Allowance	Loss of Profit
VAL DART KN KMR 009	Tenant - Business Owner						
VAL DART KN KMR 009	Tenant - Business Owner						
VAL DART KN KMR 009	Tenant - Business Owner						
VAL DART KN KMR 009	Tenant - Business Owner						
VAL DART KN KMR 009	Tenant - Business Owner						
VAL DART KN KMR 010	Owner/occupier - resident						
VAL DART KN KMR 011	Owner/occupier - resident						
VAL DART KN KMR 011	Tenant - resident						
VAL DART KN KMR 011	Tenant - resident						
VAL DART KN KMR 011	Tenant - resident						
VAL DART KN KMR 011	Tenant - resident						
VAL DART KN KMR 011	Tenant - resident						
VAL DART KN KMR 011	Tenant - Business Owner						
VAL DART KN KMR 012	Tenant - Business Owner						
VAL DART KN KMR 012	Tenant - Business Owner						
VAL DART KN KMR 012	Tenant - Business Owner						
VAL DART KN KMR 012	Tenant - Business Owner						
VAL DART KN KMR 012	Tenant - Business Owner						
VAL DART KN KMR 012	Tenant - resident						
VAL/DART/KN/KMR 012	Owner/occupier - resident						
VAL/DART/KN/KMR 012	Tenant - resident						
VAL/DART/KN/KMR 012	Tenant - Business Owner						
VAL/DART/KN/KMR 013	Owner/occupier - resident						
VAL/DART/KN/KMR 013	Tenant - resident						
VAL/DART/KN/KMR 013	Tenant - resident						
VAL/DART/KN/KMR 013	Tenant - resident						
VAL/DART/KN/KMR 013	Tenant - resident						
VAL/DART/KN/KMR 013	Tenant - Business Owner						
VAL/DART/KN/KMR 013	Tenant - Business Owner						
VAL/DART/KN/KMR 013	Tenant - Business Owner						
VAL DART KN KMR 013	Tenant - Business Owner						
VAL DART KN KMR 013	Tenant - Business Owner						
VAL DART KN KMR 014	Owner/occupier - Business Owner						
VAL DART KN KMR 014	Tenant - resident						

Legend
 Yes
 No/Not Informed

Annex 2 - Part 3
Data Bank related to Section 07 of the Property Survey Questionnaire

Code of this Property Survey Inspection & Valuation Report	Affected Party	Compensation for Land	Compensation for Structures	Disturbance Allowance	Accomod Allowance	Transport Allowance	Loss of Profit
VAL DART KN KMR 014	Tenant - resident						
VAL DART KN KMR 014	Tenant - resident						
VAL DART KN KMR 014	Tenant - Business - Owner						
VAL DART KN KMR 014	Tenant - Business - Owner						
VAL DART KN KMR 014	Tenant - Business - Owner						
VAL DART KN KMR 014	Tenant - Business - Owner						
VAL DART KN KMR 014	Tenant - Business - Owner						
VAL DART KN KMR 015	Owner-occupier - resident						
VAL DART KN KMR 015	Owner-occupier - Business Owner						
VAL DART KN KMR 015	Tenant - resident						
VAL DART KN KMR 015	Tenant - Business - Owner						
VAL DART KN KMR 017	Tenant - Business - Owner						
VAL DART KN KMR 017	Tenant - Business - Owner						
VAL DART KN KMR 017	Owner-occupier - Business Owner						
VAL DART KN KMR 018	Owner-occupier - resident						
VAL DART KN KMR 018	Tenant - resident						
VAL DART KN KMR 018	Tenant - resident						
VAL DART KN KMR 018	Tenant - resident						
VAL DART KN KMR 018	Tenant - resident						
VAL DART KN KMR 018	Tenant - resident						
VAL DART KN KMR 018	Tenant - resident						
VAL/DART KN KMR 018	Tenant - resident						
VAL/DART KN KMR 018	Tenant - resident						
VAL DART KN KMR 018	Tenant - resident						
VAL DART KN KMR 018	Tenant - resident						
VAL DART KN KMR 019	Owner-occupier - resident						
VAL DART KN KMR 019	Tenant - resident						
VAL DART KN KMR 020	Owner-occupier - resident						
VAL DART KN KMR 020	Tenant - resident						
VAL DART KN KMR 020	Tenant - Business - Owner						
VAL DART KN KMR 020	Tenant - Business - Owner						

Legend
 Yes
 No/Not Informed

Annex 2 - Part 3
Data Bank related to Section 07 of the Property Survey Questionnaire

Code of this Property Survey Inspection & Valuation Report	Affected Party	Compensation for Land	Compensation for Structures	Disturbance Allowance	Accomod Allowance	Transport Allowance	Loss of Profit
VAL/DART/KN/KMR/021	Owner-occupier - resident						
VAL/DART/KN/KMR/022	Owner-occupier - resident						
VAL/DART/KN/KMR/023	Owner-occupier - Business Owner						
VAL/DART/KN/KMR/024	Owner-occupier - resident						
VAL/DART/KN/KMR/024	Tenant - resident						
VAL/DART/KN/KMR/024	Tenant - resident						
VAL/DART/KN/KMR/024	Tenant - resident						
VAL/DART/KN/KMR/024	Tenant - resident						
VAL/DART/KN/KMR/025	Owner-occupier - resident						
VAL/DART/KN/KMR/025	Owner-occupier - Business Owner						
VAL/DART/KN/KMR/026	Owner-occupier - resident						
VAL/DART/KN/KMR/026	Owner-occupier - Business Owner						
VAL/DART/KN/KMR/026	Tenant - resident						
VAL/DART/KN/KMR/026	Tenant - resident						
VAL/DART/KN/KMR/026	Tenant - resident						
VAL/DART/KN/KMR/026	Tenant - Business Owner						
VAL/DART/KN/KMR/026	Tenant - Business Owner						
VAL/DART/KN/KMR/027	Owner-occupier - resident						
VAL/DART/KN/KMR/027	Owner-occupier - Business Owner						
VAL/DART/KN/KMR/028	Owner-occupier - Business Owner						
VAL/DART/KN/KMR/028	Tenant - resident						
VAL/DART/KN/KMR/029	Owner-occupier - resident						
VAL/DART/KN/KMR/029	Tenant - resident						
VAL/DART/KN/KMR/029	Tenant - resident						
VAL/DART/KN/KMR/029	Tenant - resident						
VAL/DART/KN/KMR/029	Tenant - Business Owner						
VAL/DART/KN/KMR/029	Tenant - Business Owner						
VAL/DART/KN/KMR/029	Tenant - Business Owner						
VAL/DART/KN/KMR/029	Tenant - Business Owner						
VAL/DART/KN/KMR/031	Tenant - resident						
VAL/DART/KN/KMR/031	Tenant - resident						
VAL/DART/KN/KMR/031	Tenant - resident						

Legend
 Yes
 No/Not Informed

Annex 2 - Part 3

Data Bank related to Section 07 of the Property Survey Questionnaire

Code of this Property Survey Inspection & Valuation Report	Affected Party	Compensation for Land	Compensation for Structures	Disturbance Allowance	Accomod Allowance	Transport Allowance	Loss of Profit
VAL DART KN KMR 031	Tenant - resident						
VAL DART KN KMR 031	Tenant - resident						
VAL DART KN KMR 031	Tenant - resident						
VAL DART KN KMR 031	Owner-occupier - Business Owner						
VAL DART KN KMR 032	Owner-occupier - Business Owner						
VAL DART KN KMR 033	Owner-occupier - Business Owner						
VAL DART KN KND 001	Owner-occupier - resident						
VAL DART KN KND 002	Owner-occupier - Business Owner						
VAL DART KN KND 004	Owner-occupier - Business Owner						
VAL DART KN KND 004A	Other						
VAL DART KN KND 006	Other						
VAL DART KN KND 007	Other						
VAL DART KN MG2 001	Owner-occupier - resident						
VAL DART KN MG2 001	Tenant - resident						
VAL DART KN MG2 001	Tenant - Business Owner						
VAL DART KN MG2 002	Owner-occupier - resident						
VAL DART KN MG2 002	Tenant - Business Owner						
VAL DART KN MG2 002	Tenant - Business Owner						
VAL DART KN MG2 002	Tenant - Business Owner						
VAL DART KN MG2 002	Tenant - Business Owner						
VAL DART KN MG2 002	Tenant - Business Owner						
VAL DART KN MG2 002	Tenant - Business Owner						
VAL DART KN MG2 002	Tenant - Business Owner						
VAL DART KN MG2 002	Tenant - Business Owner						
VAL DART KN MG2 002	Tenant - Business Owner						
VAL DART KN MG2 003	Owner-occupier - resident						
VAL DART KN MG2 003	Tenant - resident						
VAL DART KN MG2 003	Tenant - resident						
VAL DART KN MG2 003	Tenant - resident						
VAL DART KN MG2 003	Tenant - resident						
VAL DART KN MG2 003	Tenant - resident						
VAL DART KN MG2 003	Tenant - resident						
VAL DART KN MG2 003	Tenant - resident						
VAL DART KN MG2 003	Tenant - resident						
VAL DART KN MG2 003	Tenant - resident						

Legend
 Yes
 No/Not Informed

Annex 2 - Part 3

Data Bank related to Section 07 of the Property Survey Questionnaire

Code of this Property Survey Inspection & Valuation Report	Affected Party	Compensation for Land	Compensation for Structures	Disturbance Allowance	Accomod Allowance	Transport Allowance	Loss of Profit
VAL/DART KN MG2 003	Tenant - resident						
VAL/DART KN MG2 003	Tenant - resident						
VAL/DART KN MG2 003	Tenant - Business - Owner						
VAL/DART KN MG2 003	Tenant - Business - Owner						
VAL/DART KN MG2 003	Tenant - Business - Owner						
VAL/DART KN MG2 003	Tenant - Business - Owner						
VAL/DART KN MG2 004	Landlord (absent property) - Owner						
VAL/DART KN MG2 004	Tenant - Business - Owner						
VAL/DART KN MG2 005	Owner occupier - Business - Owner						
VAL/DART KN MG2 005	Tenant - resident						
VAL/DART KN MG2 005	Tenant - resident						
VAL/DART KN MG2 005	Tenant - resident						
VAL/DART KN MG2 005	Tenant - resident						
VAL/DART KN MG2 005	Tenant - Business - Owner						
VAL/DART KN MG2 005	Tenant - Business - Owner						
VAL/DART KN MG2 005	Tenant - Business - Owner						
VAL/DART KN MG2 005	Tenant - Business - Owner						
VAL/DART KN MG2 005	Tenant - Business - Owner						
VAL/DART KN MG2 005	Tenant - Business - Owner						
VAL/DART KN MG2 006	Owner occupier - Business - Owner						
VAL/DART KN MG2 007	Landlord (absent property) - Owner						
VAL/DART KN MG2 007	Tenant - resident						
VAL/DART KN MG2 007	Tenant - resident						
VAL/DART KN MG2 007	Tenant - resident						
VAL/DART KN MNY 001	Owner occupier - Business - Owner						
VAL/DART KN MNY 003	Owner occupier - resident						
VAL/DART KN MNY 004	Owner occupier - resident						
VAL/DART KN MNY 005	Owner occupier - resident						
VAL/DART KN MNY 006	Owner occupier - resident						
VAL/DART KN MNY 007	Owner occupier - Business - Owner						
VAL/DART KN UB2 001	Landlord (absent property) - Owner						
VAL/DART KN UB2 001	Tenant - Business - Owner						
VAL/DART KN UB2 001	Tenant - Business - Owner						

Legend
 Yes
 No/Not Intormed

Annex 2 - Part 3

Data Bank related to Section 07 of the Property Survey Questionnaire

Code of this Property Survey Inspection & Valuation Report	Affected Party	Compensation for Land	Compensation for Structures	Disturbance Allowance	Accomod Allowance	Transport Allowance	Loss of Profit
VAL DART KN UB2 002	Owner-occupier - Business Owner						
VAL DART KN UB2 003	Owner-occupier - Business Owner						
VAL DART KN UB2 003	Tenant - Business Owner						
VAL DART KN UB2 004	Landlord (absent property) Owner						
VAL DART KN UB2 004	Tenant - Business Owner						
VAL DART KN UB2 005	Owner-occupier - Business Owner						
VAL DART KN UB2 006	Landlord (absent property) Owner						
VAL DART KN UB2 006	Tenant - Business Owner						
VAL DART KN UB2 007	Owner-occupier - Business Owner						
VAL DART KN UB2 008	Owner-occupier - Business Owner						
VAL DART KN UB2 009	Owner-occupier - Business Owner						
VAL DART KN UB2 010	Owner-occupier - Business Owner						
VAL DART KN UB2 011	Owner-occupier - Business Owner						
VAL DART KN UB2 012	Owner-occupier - Business Owner						
VAL DART KN UB2 013	Owner-occupier - Business Owner						
VAL DART KN UB2 014	Owner-occupier - Business Owner						
VAL DART KN UB2 014	Tenant - Business Owner						
VAL DART KN UB2 015	Owner-occupier - Business Owner						
VAL DART KN UB2 016	Owner-occupier - Business Owner						
VAL DART KN UB2 017	Owner-occupier - Business Owner						
VAL DART KN UB2 020	Owner-occupier - Business Owner						
VAL DART KN UB2 021	Tenant - Business Owner						
VAL DART KN UB2 021	Owner-occupier - Business Owner						
VAL DART KN UB2 022	Other						
VAL DART II JGN 001	Owner-occupier - Business Owner						
VAL DART II JGN 002	Owner-occupier - Business Owner						
VAL DART II KKO 001	Other						
VAL DART II KKO 002	Landlord (absent property) Owner						
VAL DART II KKO 002	Tenant - Business Owner						
VAL DART II KKO 002	Tenant - Business Owner						

Legend



Yes

No/Not Informed

Annex 03 - Part 1

Data Bank related to Sections 01.02 and 03 of the Socio-Economic Survey - Residential Resettlement Household Interview

Code of Corresponding Property Survey	Code of this Socio-Economic Unit	Family Head	Type of occupant	Type of residence	Other	Cost of Rent (if tenant)	Water + Sewer	Garbage Collection	Energy	Gas	Land Rent	Property Tax	Is there any commercial use of residence?
VAL/DART/IL/GRN/001	The interview was not done												
VAL/DART/IL/GRN/002	The interview was not done												
VAL/DART/IL/GRN/003	The interview was not done												
VAL/DART/IL/GRN/004	SOC/DART/IL/GRN/004	Male	Owner-occupant	Individual house									
VAL/DART/IL/GRN/005	SOC/DART/IL/GRN/005	Female	Owner-occupant	Individual house			90 000,00	2 000,00	35 000,00	35 000,00			
VAL/DART/IL/GRN/006	SOC/DART/IL/GRN/006	Male	Owner-occupant	Individual house			90 000,00	2 000,00	50 000,00	40 000,00			
VAL/DART/IL/GRN/007	SOC/DART/IL/GRN/007	Female	Owner-occupant	Individual house				12 000,00	119 000,00				
VAL/DART/IL/GRN/008	SOC/DART/IL/GRN/008	Male	Owner-occupant	Room or apartment in individual house			72 000,00	10 000,00	56 000,00	44 000,00	20 000,00	20 000,00	
VAL/DART/IL/GRN/008	SOC/DART/IL/GRN/008A	Male	Tenant	Upper floor unit		250 000,00	50 000,00	2 000,00	30 000,00	50 000,00			
VAL/DART/IL/GRN/010	SOC/DART/IL/GRN/010	Male	Owner-occupant	Room or apartment in individual house			90 000,00	2 000,00	100 000,00	40 000,00			
VAL/DART/IL/GRN/011	The interview was not done												
VAL/DART/IL/GRN/012	The interview was not done												
VAL/DART/IL/GRN/013	The interview was not done												
VAL/DART/IL/GRN/014	SOC/DART/IL/GRN/014	Male	Owner-occupant	Room or apartment in individual house			90 000,00	2 000,00	50 000,00				
VAL/DART/IL/GRN/015	The interview was not done												
VAL/DART/IL/GRN/016	The interview was not done												
VAL/DART/IL/GRN/017	SOC/DART/IL/GRN/017	Male	Owner-occupant	Individual house					40 000,00	40 000,00			
VAL/DART/IL/GRN/018	The interview was not done									40 000,00			
VAL/DART/IL/GRN/019	The interview was not done							40 000,00	100 000,00	20 000,00			
VAL/DART/IL/GRN/020	The interview was not done								20 000,00				
VAL/DART/IL/GRN/021	SOC/DART/IL/GRN/021	Male	Owner-occupant	Individual house				2 000,00	30 000,00				
VAL/DART/IL/GRN/022	SOC/DART/IL/GRN/022	Female	Owner-occupant	Individual house			25 000,00	2 000,00	35 000,00	30 000,00			
VAL/DART/IL/GRN/023	SOC/DART/IL/GRN/023	Male	Owner-occupant	Individual house				2 000,00	30 000,00	30 000,00			
VAL/DART/IL/GRN/024	The interview was not done												
VAL/DART/IL/GRN/025	SOC/DART/IL/GRN/025	Male	Owner-occupant	Individual house			75 000,00	2 000,00	100 000,00	25 000,00	67 000,00	67 000,00	

Legend
 Yes
 Not Informed

Annex 03 - Part 1
Data Bank related to Sections 01, 02 and 06 of the Socio-economic Survey - Residential Resettlement Questionnaire

Code of Corresponding Property Survey	Code of this Socio-Economic Unit	Family Head	Type of occupant	Type of residence	Other	Cost of Rent (tenant)	Water + Sewer	Garbage Collection	Energy	Gas	Land Rent	Property Tax	Is there any commercial use of residence?
VAL/DART/IL/GRN/026	SOC/DART/IL/GRN/026	Male	Owner-occupant	Individual house			90 000,00	2 000,00	25 000,00	50 000,00			
VAL/DART/IL/GRN/027	SOC/DART/IL/GRN/027	Female	Owner-occupant	Individual house				2 000,00	25 000,00				
VAL/DART/IL/GRN/028	SOC/DART/IL/GRN/028	Female	Owner-occupant	Individual house			20 000,00	4 000,00	30 000,00	50 000,00			
VAL/DART/IL/GRN/029	SOC/DART/IL/GRN/029	Male	Owner-occupant	Room or apartment in individual house			60 000,00	2 000,00			2 000,00	1 000,00	
VAL/DART/IL/GRN/030	SOC/DART/IL/GRN/030	Male	Owner-occupant	Individual house									
VAL/DART/IL/GRN/031	SOC/DART/IL/GRN/031	Female	Owner-occupant	Individual house			60 000,00	2 000,00	20 000,00	30 000,00			
VAL/DART/IL/GRN/032	SOC/DART/IL/GRN/032	Female	Owner-occupant	Individual house			60 000,00	2 000,00	20 000,00	30 000,00			
VAL/DART/IL/GRN/033	SOC/DART/IL/GRN/033	Male	Owner-occupant	Individual house									
VAL/DART/IL/GRN/034	SOC/DART/IL/GRN/034	Male	Owner-occupant	Individual house									
VAL/DART/IL/GRN/035	SOC/DART/IL/GRN/035	Male	Owner-occupant	Individual house									
VAL/DART/IL/GRN/036	SOC/DART/IL/GRN/036	Male	Owner-occupant	Individual house									
VAL/DART/IL/GRN/037	SOC/DART/IL/GRN/037	Female	Owner-occupant	Room or apartment in individual house			6 000,00	2 000,00	40 000,00	60 000,00	2 000,00	1 000,00	
VAL/DART/IL/GRN/038	SOC/DART/IL/GRN/038	Spouse	Owner-occupant	Individual house			95 000,00	7 000,00	50 000,00	45 000,00	2 000,00	1 000,00	
VAL/DART/IL/GRN/039	SOC/DART/IL/GRN/039	Male	Owner-occupant	Individual house			150 000,00	2 000,00		60 000,00			
VAL/DART/IL/GRN/040	SOC/DART/IL/GRN/040	Spouse	Owner-occupant	Individual house			360 000,00	2 000,00	30 000,00	25 000,00			
VAL/DART/IL/GRN/041	SOC/DART/IL/GRN/041	Male	Owner-occupant	Individual house			90 000,00	2 000,00	24 000,00	40 000,00			
VAL/DART/IL/GRN/042	SOC/DART/IL/GRN/042	Male	Owner-occupant	Individual house									
VAL/DART/IL/GRN/043	SOC/DART/IL/GRN/043	Male	Owner-occupant	Room or apartment in individual house			90 000,00	2 000,00	20 000,00				
VAL/DART/IL/GRN/044	SOC/DART/IL/GRN/044	Male	Owner-occupant	Individual house									
VAL/DART/IL/GRN/045	SOC/DART/IL/GRN/045	Male	Owner-occupant	Individual house			32 000,00	2 000,00	40 000,00	51 000,00			
VAL/DART/IL/GRN/046	SOC/DART/IL/GRN/046	Male	Owner-occupant	Individual house			90 000,00	2 000,00	25 000,00	50 000,00			
VAL/DART/IL/GRN/047	SOC/DART/IL/GRN/047	Male	Owner-occupant	Individual house									
VAL/DART/IL/GRN/048	SOC/DART/IL/GRN/048	Female	Owner-occupant	Individual house									
VAL/DART/IL/GRN/049	SOC/DART/IL/GRN/049	Male	Owner-occupant	Individual house									
VAL/DART/IL/GRN/050	SOC/DART/IL/GRN/050	Female	Owner-occupant	Individual house			20 000,00	4 000,00	50 000,00	45 000,00			

Legend
 Yes
 Not informed

Annex 03 - Part 1
Data Bank related to Sections 01, 02 and 06 of the Socio-economic Survey - Residential Resettlement Questionnaire

Code of Corresponding Property Survey	Code of the Socio-Economic Unit	Family Head	Type of occupant	Type of residence	Other	Cost of Rent/tenants	Water + Sewer	Garbage Collection	Energy	Gas	Land Rent	Property Tax	Is there any commercial use of residence?
VAL/DART/IL/GRN/051	SOC/DART/IL/GRN/051	Female	Owner-occupant	Individual house				20 000,00	20 000,00				
VAL/DART/IL/GRN/052	SOC/DART/IL/GRN/052	Female	Owner-occupant	Individual house				2 000,00	55 000,00				
VAL/DART/IL/GRN/053	SOC/DART/IL/GRN/053	Male	Owner-occupant	Individual house			60 000,00	4 000,00	25 000,00	25 000,00			
VAL/DART/IL/GRN/054	SOC/DART/IL/GRN/054	Male	Owner-occupant	Individual house			226 000,00	2 000,00	32 000,00	44 000,00			
VAL/DART/IL/GRN/055	SOC/DART/IL/GRN/055	Female	Owner-occupant	Individual house			45 000,00	2 500,00	25 000,00	30 000,00			
VAL/DART/IL/GRN/056	SOC/DART/IL/GRN/056	Male	Owner-occupant	Individual house				2 000,00	10 000,00	30 000,00			
VAL/DART/IL/GRN/057	SOC/DART/IL/GRN/057	Male	Owner-occupant	Individual house			17 000,00	2 000,00	80 000,00	52 000,00			
VAL/DART/IL/GRN/058	SOC/DART/IL/GRN/058	Female	Owner-occupant	Individual house			17 000,00	2 000,00	80 000,00	27 000,00			
VAL/DART/IL/GRN/059	SOC/DART/IL/GRN/059	Female	Owner-occupant	Individual house									
VAL/DART/IL/GRN/060	SOC/DART/IL/GRN/060	Male	Owner-occupant	Room or apartment in individual house			60 000,00	4 000,00	20 000,00				
VAL/DART/IL/GRN/061	SOC/DART/IL/GRN/061	Male	Owner-occupant	Individual house			12 000,00	2 000,00	30 000,00	27 000,00			
VAL/DART/IL/GRN/062	SOC/DART/IL/GRN/062	Male	Owner-occupant	Individual house			12 000,00	2 000,00	30 000,00	27 000,00			
VAL/DART/IL/GRN/063	SOC/DART/IL/GRN/063	Male	Owner-occupant	Individual house									
VAL/DART/IL/GRN/064	SOC/DART/IL/GRN/064	Male	Owner-occupant	Individual house									
VAL/DART/IL/GRN/065	SOC/DART/IL/GRN/065	Male	Owner-occupant	Individual house			32 500,00	2 000,00	28 000,00	30 000,00			
VAL/DART/IL/GRN/066	SOC/DART/IL/GRN/066	Male	Owner-occupant	Individual house			10 000,00	2 000,00	10 000,00	30 000,00			
VAL/DART/IL/GRN/067	SOC/DART/IL/GRN/067	Female	Owner-occupant	Individual house									
VAL/DART/IL/GRN/068	SOC/DART/IL/GRN/068	Female	Owner-occupant	Individual house									
VAL/DART/IL/GRN/069	SOC/DART/IL/GRN/069	Spouse	Owner-occupant	Individual house									
VAL/DART/IL/GRN/070	SOC/DART/IL/GRN/070	Male	Owner-occupant	Room or apartment in individual house			90 000,00	2 000,00	7 000,00	40 000,00	135 000,00		
VAL/DART/IL/GRN/071	SOC/DART/IL/GRN/071	Male	Owner-occupant	Room or apartment in individual house			20 000,00	6 000,00	30 000,00	22 000,00			
VAL/DART/IL/GRN/072	SOC/DART/IL/GRN/072	Male	Owner-occupant	Room or apartment in individual house			36 000,00	2 000,00	35 000,00	22 000,00		95 000,00	
VAL/DART/IL/GRN/073	SOC/DART/IL/GRN/073	Spouse	Owner-occupant	Individual house			45 000,00	2 000,00	35 000,00	44 000,00			
VAL/DART/IL/GRN/074	SOC/DART/IL/GRN/074	Spouse	Owner-occupant	Room or apartment in individual house			75 000,00						
VAL/DART/IL/GRN/075	SOC/DART/IL/GRN/075	Male	Owner-occupant	Individual house			25 000,00	15 000,00	45 000,00	10 000,00			

Legend
 Yes
 No/Not Informed

Annex 03 - Part 1

Data Bank related to Sections 01, 02 and 06 of the Socio-economic Survey - Residential Resettlement Questionnaire

Code of Corresponding Property Survey	Code of this Socio-Economic Unit	Family Head	Type of occupant	Type of residence	Other	Cost of Rent/tenants	Water + Sewer	Garbage Collection	Energy	Gas	Land Rent	Property Tax	If there is a commercial use of residence *
VAL/DART/IL/GRN/076	SOC/DART/IL/GRN/076	Female	Owner-occupant	Room or apartment in individual house			45 000,00	2 000,00	15 000,00				
VAL/DART/IL/GRN/077	SOC/DART/IL/GRN/077	Female	Owner-occupant	Room or apartment in individual house									
VAL/DART/IL/GRN/078	SOC/DART/IL/GRN/078	Female	Owner-occupant	Room or apartment in individual house									
VAL/DART/IL/GRN/079	SOC/DART/IL/GRN/079	Female	Owner-occupant	Room or apartment in individual house									
VAL/DART/IL/GRN/080	SOC/DART/IL/GRN/080	Female	Owner-occupant	Room or apartment in individual house									
VAL/DART/IL/GRN/081	SOC/DART/IL/GRN/081	Male	Tenant	Room or apartment in individual house			20 000,00	2 000,00	40 000,00	40 000,00		200 000,00	
VAL/DART/IL/GRN/082	SOC/DART/IL/GRN/082	Male	Owner-occupant	Individual house				2 000,00	15 000,00	22 000,00			
VAL/DART/IL/GRN/083	SOC/DART/IL/GRN/083	Male	Owner-occupant	Individual house			45 000,00	2 000,00	20 000,00	80 000,00			
VAL/DART/IL/GRN/084	SOC/DART/IL/GRN/084	Female	Other	Individual house			90 000,00	2 000,00	35 000,00	36 000,00			
VAL/DART/IL/GRN/085	SOC/DART/IL/GRN/085	Female	Other	Individual house									
VAL/DART/IL/GRN/086	SOC/DART/IL/GRN/086	Female	Other	Individual house									
VAL/DART/IL/GRN/087	SOC/DART/IL/GRN/087	Male	Owner-occupant	Individual house									
VAL/DART/IL/GRN/088	SOC/DART/IL/GRN/088	Female	Owner-occupant	Individual house									
VAL/DART/IL/GRN/089	SOC/DART/IL/GRN/089	Male	Owner-occupant	Individual house			15 000,00	2 000,00	35 000,00	27 000,00			
VAL/DART/IL/GRN/090	SOC/DART/IL/GRN/090	Female	Owner-occupant	Individual house			25 000,00	2 000,00	40 000,00	27 000,00			
VAL/DART/IL/GRN/091	SOC/DART/IL/GRN/091	Male	Tenant	Individual house		130 000,00	63 000,00	2 000,00	10 000,00	30 000,00			
VAL/DART/IL/GRN/092	SOC/DART/IL/GRN/092	Other	Owner-occupant	Individual house									
VAL/DART/IL/GRN/093	SOC/DART/IL/GRN/093	Female	Other	Individual house									
VAL/DART/IL/GRN/094	SOC/DART/IL/GRN/094	Other	Other	Individual house									
VAL/DART/IL/GRN/095	SOC/DART/IL/GRN/095	Male	Owner-occupant	Individual house									
VAL/DART/IL/GRN/096	SOC/DART/IL/GRN/096	Female	Other	Individual house									
VAL/DART/IL/GRN/097	SOC/DART/IL/GRN/097	Female	Other	Individual house									
VAL/DART/IL/GRN/098	SOC/DART/IL/GRN/098	Female	Other	Individual house									
VAL/DART/IL/GRN/099	SOC/DART/IL/GRN/099	Female	Owner-occupant	Individual house			15 000,00	2 000,00	35 000,00	27 000,00			
VAL/DART/IL/GRN/100	SOC/DART/IL/GRN/100	Male	Owner-occupant	Individual house			90 000,00	2 000,00	35 000,00				

Legend
 Yes
 Not informed

Annex 03 - Part 1

Data Bank related to Section 01.01 and 06 of the Survey - Residential Resettlement Data Bank

Code of Corresponding Property Survey	Code of this Socio-Economic Unit	Family Head	Type of occupant	Type of residence	Other	Cost of Rent (tenant)	Water + Sewer	Garbage Collection	Energy	Gas	Land Rent	Property Tax	Is there any commercial use of residence?
VAL/DART/IL/GRN/101	SOC/DART/IL/GRN/101	Male	Owner-occupant	Room or apartment in individual house			8 500,00	2 000,00	150 000,00	50 000,00			
VAL/DART/IL/GRN/102	SOC/DART/IL/GRN/102	Male	Owner-occupant	Room or apartment in individual house			12 000,00	1 000,00	1 000,00	20 000,00			
VAL/DART/IL/GRN/103	SOC/DART/IL/GRN/103	Male	Owner-occupant	Room or apartment in individual house			75 000,00	1 000,00	35 000,00	60 000,00			
VAL/DART/IL/GRN/104	SOC/DART/IL/GRN/104	Female	Owner-occupant	Room or apartment in individual house			30 000,00	2 000,00	20 000,00	88 000,00			
VAL/DART/IL/GRN/105	SOC/DART/IL/GRN/105	Female	Other	Room or apartment in individual house			30 000,00	2 000,00	20 000,00	88 000,00			
VAL/DART/IL/GRN/106	SOC/DART/IL/GRN/106	Other	Other	Room or apartment in individual house			30 000,00	2 000,00	20 000,00	88 000,00			
VAL/DART/KN/HNF/000	SOC/DART/KN/HNF/000												
VAL/DART/KN/HNF/000	SOC/DART/KN/HNF/000												
VAL/DART/KN/HNF/011	SOC/DART/KN/HNF/011	Male	Tenant	Room or apartment in individual house		20 000,00	15 000,00	2 000,00	5 000,00	30 000,00			
VAL/DART/KN/HNF/011	SOC/DART/KN/HNF/011A	Male	Tenant	Room or apartment in individual house		40 000,00	60 000,00	5 000,00	35 000,00	40 000,00			
VAL/DART/KN/HNF/011	SOC/DART/KN/HNF/011B	Female	Tenant	Room or apartment in individual house		20 000,00	30 000,00	1 000,00	48 000,00	30 000,00			
VAL/DART/KN/HNF/011	SOC/DART/KN/HNF/011C	Female	Tenant	Room or apartment in individual house		20 000,00	30 000,00	1 000,00	48 000,00	30 000,00			
VAL/DART/KN/HNF/011	SOC/DART/KN/HNF/011D	Male	Tenant	Room or apartment in individual house		20 000,00	3 000,00	2 000,00	4 000,00	3 000,00			
VAL/DART/KN/HNF/011	SOC/DART/KN/HNF/011E	Female	Tenant	Room or apartment in individual house		25 000,00	3 000,00	2 000,00	4 000,00	15 000,00			
VAL/DART/KN/HNF/011	SOC/DART/KN/HNF/011F	Male	Tenant	Room or apartment in individual house		20 000,00	10 000,00	1 000,00	5 000,00	25 000,00			
VAL/DART/KN/HNF/012	SOC/DART/KN/HNF/012A	Female	Tenant	Room or apartment in individual house		20 000,00	30 000,00	1 000,00	48 000,00	30 000,00			
VAL/DART/KN/HNF/012	SOC/DART/KN/HNF/012B	Female	Tenant	Room or apartment in individual house		20 000,00		5 000,00	12 000,00	50 000,00			
VAL/DART/KN/HNF/012	SOC/DART/KN/HNF/012C	Female	Tenant	Room or apartment in individual house			30 000,00	1 000,00	12 000,00	30 000,00		20 000,00	
VAL/DART/KN/HNF/012	SOC/DART/KN/HNF/012D	Female	Tenant	Room or apartment in individual house		20 000,00	30 000,00	1 000,00	48 000,00	30 000,00			
VAL/DART/KN/KMR/001	SOC/DART/KN/KMR/001	Male	Tenant	Room or apartment in individual house		15 000,00			15 000,00	15 000,00		12 500,00	
VAL/DART/KN/KMR/001	SOC/DART/KN/KMR/001A	Female	Owner-occupant	Individual house					15 000,00	22 000,00		12 500,00	
VAL/DART/KN/KMR/001	SOC/DART/KN/KMR/001B	Male	Tenant	Individual house		60 000,00			20 000,00	25 500,00		12 500,00	
VAL/DART/KN/KMR/002	SOC/DART/KN/KMR/002	Male	Owner-occupant	Individual house									
VAL/DART/KN/KMR/000	SOC/DART/KN/KMR/000												
VAL/DART/KN/KMR/000	SOC/DART/KN/KMR/000												

Legend
 Yes
 Not/Not informed

Annex 03 - Part I

Data Bank related to Sections 01, 02 and 06 of the Socio-Economic Survey - Residential Resettlement of Chetumal City

Code of Corresponding Property Survey	Code of this Socio-Economic Unit	Family Head	Type of occupant	Type of residence	Other	Cost of Rent/tenant	Water + Sewer	Garbage Collection	Energy	Gas	Land Rent	Property Tax	If there any commercial use of residence *
VAL/DART/KN/KMR/003	SOC/DART/KN/KMR/003	Male	Owner-occupant	Individual house					15 000,00	20 000,00		10 000,00	
VAL/DART/KN/KMR/003	SOC/DART/KN/KMR/003A	Female	Tenant	Room or apartment in individual house		60 000,00			15 600,00	22 000,00		10 000,00	
VAL/DART/KN/KMR/004	SOC/DART/KN/KMR/004	Female	Tenant	Room or apartment in individual house		12 000,00	15 000,00	3 000,00		19 000,00			
VAL/DART/KN/KMR/004	SOC/DART/KN/KMR/004A	Male	Tenant	Room or apartment in individual house		12 000,00	54 000,00	1 800,00		18 000,00			
VAL/DART/KN/KMR/004	SOC/DART/KN/KMR/004B	Male	Tenant	Room or apartment in individual house		12 000,00	24 000,00	8 000,00		19 000,00			
VAL/DART/KN/KMR/004	SOC/DART/KN/KMR/004C	Male	Tenant	Room or apartment in individual house		12 000,00	30 000,00	3 600,00		15 000,00			
VAL/DART/KN/KMR/004	SOC/DART/KN/KMR/004D	Female	Tenant	Room or apartment in individual house									
VAL/DART/KN/KMR/005	SOC/DART/KN/KMR/005	Female	Tenant	Room or apartment in individual house									
VAL/DART/KN/KMR/005	SOC/DART/KN/KMR/005A	Male	Tenant	Room or apartment in individual house		15 000,00	26 000,00	24 000,00	4 000,00	10 500,00			
VAL/DART/KN/KMR/005	The interview was not done		Tenant										
VAL/DART/KN/KMR/005	The interview was not done		Tenant										
VAL/DART/KN/KMR/005	The interview was not done		Tenant										
VAL/DART/KN/KMR/005	The interview was not done		Tenant										
VAL/DART/KN/KMR/005	The interview was not done		Tenant										
VAL/DART/KN/KMR/009	SOC/DART/KN/KMR/009	Female	Tenant	Room or apartment in individual house									
VAL/DART/KN/KMR/010	SOC/DART/KN/KMR/010	Female	Owner-occupant	Individual house									
VAL/DART/KN/KMR/011	SOC/DART/KN/KMR/011	Female	Tenant	Room or apartment in individual house									
VAL/DART/KN/KMR/011	SOC/DART/KN/KMR/011A	Female	Tenant	Room or apartment in individual house									
VAL/DART/KN/KMR/011	SOC/DART/KN/KMR/011B	Female	Tenant	Room or apartment in individual house									
VAL/DART/KN/KMR/011	SOC/DART/KN/KMR/011C	Female	Tenant	Room or apartment in individual house									
VAL/DART/KN/KMR/011	SOC/DART/KN/KMR/011D	Female	Tenant	Room or apartment in individual house									
VAL/DART/KN/KMR/011	SOC/DART/KN/KMR/011E	Female	Tenant	Room or apartment in individual house									
VAL/DART/KN/KMR/012	SOC/DART/KN/KMR/012	Female	Tenant	Room or apartment in individual house									
VAL/DART/KN/KMR/012	SOC/DART/KN/KMR/012A	Male	Tenant	Room or apartment in individual house		12 000,00			5 000,00				
VAL/DART/KN/KMR/012	SOC/DART/KN/KMR/012C		Tenant	Room or apartment in individual house		13 000,00			4 000,00				

Legend
 Yes
 No/Not Informe

Annex 03 - Part 1

Data Bank related to Section 01.02 and 06 of the Socio-economic Survey - Residential Resettlement Questionnaire

Code of Corresponding Property Survey	Code of this Socio-Economic Unit	Family Head	Type of occupant	Type of residence	Other	Cost of Rent (if tenant)	Water + Sewer	Garbage Collection	Energy	Gas	Land Rent	Property Tax	If there any commercial use of residence?
VAL/DART/KN/KMR/013	SOC/DART/KN/KMR/013	Female	Tenant	Room or apartment in individual house									
VAL/DART/KN/KMR/013	SOC/DART/KN/KMR/013	Female	Tenant	Room or apartment in individual house									
VAL/DART/KN/KMR/013	SOC/DART/KN/KMR/013	Female	Tenant	Room or apartment in individual house									
VAL/DART/KN/KMR/013	SOC/DART/KN/KMR/013	Female	Tenant	Room or apartment in individual house									
VAL/DART/KN/KMR/013	SOC/DART/KN/KMR/013	Female	Tenant	Room or apartment in individual house									
VAL/DART/KN/KMR/013	SOC/DART/KN/KMR/013	Female	Tenant	Room or apartment in individual house									
VAL/DART/KN/KMR/013	The interview was not done		Tenant										
VAL/DART/KN/KMR/013	The interview was not done		Tenant										
VAL/DART/KN/KMR/013	The interview was not done		Tenant										
VAL/DART/KN/KMR/015	SOC/DART/KN/KMR/015	Female	Tenant	Room or apartment in individual house		40 000,00	45 000,00	15 000,00	5 000,00				
VAL/DART/KN/KMR/015	The interview was not done		Tenant										
VAL/DART/KN/KMR/018	SOC/DART/KN/KMR/018	Male	Tenant	Room or apartment in individual house		8 000,00			3 000,00				
VAL/DART/KN/KMR/018	SOC/DART/KN/KMR/018A	Male	Tenant	Room or apartment in individual house		10 000,00			3 000,00				
VAL/DART/KN/KMR/018	SOC/DART/KN/KMR/018B	Female	Tenant	Room or apartment in individual house		15 000,00							
VAL/DART/KN/KMR/018	The interview was not done		Tenant										
VAL/DART/KN/KMR/018	The interview was not done		Tenant										
VAL/DART/KN/KMR/018	The interview was not done		Tenant										
VAL/DART/KN/KMR/018	The interview was not done		Tenant										
VAL/DART/KN/KMR/018	The interview was not done		Tenant										
VAL/DART/KN/KMR/018	The interview was not done		Tenant										
VAL/DART/KN/KMR/018	The interview was not done		Tenant										
VAL/DART/KN/KMR/018	The interview was not done		Tenant										
VAL/DART/KN/KMR/018	The interview was not done		Tenant										
VAL/DART/KN/KMR/018	The interview was not done		Tenant										
VAL/DART/KN/KMR/018	The interview was not done		Tenant										
VAL/DART/KN/KMR/018	The interview was not done		Tenant										
VAL/DART/KN/KMR/018	The interview was not done		Tenant										
VAL/DART/KN/KMR/018	The interview was not done		Tenant										
VAL/DART/KN/KMR/018	The interview was not done		Tenant										
VAL/DART/KN/KMR/018	The interview was not done		Tenant										
VAL/DART/KN/KMR/019	SOC/DART/KN/KMR/019	Female	Tenant	Room or apartment in individual house		15 000,00	5 000,00	2 000,00	5 000,00	5 000,00			
VAL/DART/KN/KMR/019	SOC/DART/KN/KMR/019A	Female	Owner-occupant	Individual house			15 000,00	5 000,00	15 000,00	21 000,00	15 000,00	10 000,00	

Legend

	Yes
	Not/Not Informed

Annex 03 - Part 1

Data Bank related to Sections 01.01 and 02 of the Socio-economic Survey - Residential Resettlement Questionnaire

Code of Corresponding Property Survey	Code of this Socio-Economic Unit	Family Head	Type of occupant	Type of residence	Other	Cost of Rental tenants	Water + Sewer	Garbage Collection	Energy	Gas	Land Rent	Property Tax	Is there any commercial use of residence?
VAL/DART/KN/KMR/020	The interview was not done												
VAL/DART/KN/KMR/020	The interview was not done												
VAL/DART/KN/KMR/021	SOC/DART/KN/KMR/021	Male	Owner-occupant	Individual house			30 000,00	500,00	20 000,00	30 000,00		30 000,00	
VAL/DART/KN/KMR/022	SOC/DART/KN/KMR/022	Female	Owner-occupant	Individual house			90 000,00	2 000,00	15 000,00	22 000,00		10 000,00	
VAL/DART/KN/KMR/024	The interview was not done												
VAL/DART/KN/KMR/024	SOC/DART/KN/KMR/024A	Female	Tenant	Room or apartment in individual house		20 000,00	12 000,00		5 000,00	21 000,00			
VAL/DART/KN/KMR/024	The interview was not done												
VAL/DART/KN/KMR/024	The interview was not done												
VAL/DART/KN/KMR/024	The interview was not done												
VAL/DART/KN/KMR/025													
VAL/DART/KN/KMR/026													
VAL/DART/KN/KMR/026													
VAL/DART/KN/KMR/026													
VAL/DART/KN/KMR/026													
VAL/DART/KN/KMR/027													
VAL/DART/KN/KMR/027													
VAL/DART/KN/KMR/028	SOC/DART/KN/KMR/028	Male	Tenant	Individual house		80 000,00	124 000,00	4 000,00	16 000,00				
VAL/DART/KN/KMR/029	SOC/DART/KN/KMR/029	Female											
VAL/DART/KN/KMR/029	The interview was not done												
VAL/DART/KN/KMR/029	The interview was not done												
VAL/DART/KN/KMR/029													
VAL/DART/KN/KMR/029													
VAL/DART/KN/KMR/031	SOC/DART/KN/KMR/031	Male	Tenant	Room or apartment in individual house		5 000,00	45 000,00	8 000,00	2 000,00	30 000,00			
VAL/DART/KN/KMR/031													
VAL/DART/KN/KMR/031	SOC/DART/KN/KMR/031B	Male	Tenant	Room or apartment in individual house		5 000,00	45 000,00	4 800,00	3 000,00	30 000,00			
VAL/DART/KN/KMR/031	SOC/DART/KN/KMR/031C	Female	Tenant	Room or apartment in individual house		5 000,00	18 000,00	2 800,00	2 000,00	30 000,00			

Legend
 Yes
 Not Inform.

Annex 03 - Part 1
Data Bank related to Sections 01, 02 and 06 of the Socio-economic Survey - Residential Resettlement Questionnaire

Code of Corresponding Property Survey	Code of this Socio-Economic Unit	Family Head	Type of occupant	Type of residence	Other	Cost of Rent (if tenant)	Water + Sewer	Garbage Collection	Energy	Gas	Land Rent	Property Tax	Is there any commercial use of residence?
VAL/DART/KN/KMR001	The interview was not done												
VAL/DART/KN/KMR001	The interview was not done												
VAL/DART/KN/MG2/001	SOC/DART/KN/MG2/001	Male	Tenant	Room or apartment in individual house		10 000,00	30 000,00	15 000,00	8 000,00	18 000,00	28 000,00	10 000,00	
VAL/DART/KN/MG2/001	SOC/DART/KN/MG2/001	Male	Tenant	Room or apartment in individual house		10 000,00	30 000,00	15 000,00	8 000,00	18 000,00	28 000,00	10 000,00	
VAL/DART/KN/MG2/002	SOC/DART/KN/MG2/002	Male	Tenant	Room or apartment in individual house			30 000,00	15 000,00	8 000,00	18 000,00	28 000,00	10 000,00	
VAL/DART/KN/MG2/003	SOC/DART/KN/MG2/003	Male	Tenant	Room or apartment in individual house		10 000,00	30 000,00	15 000,00	8 000,00	18 000,00	28 000,00	10 000,00	
VAL/DART/KN/MG2/003	SOC/DART/KN/MG2/003A	Male	Tenant	Room or apartment in individual house			30 000,00	15 000,00	8 000,00	18 000,00	28 000,00	10 000,00	
VAL/DART/KN/MG2/003	SOC/DART/KN/MG2/003B	Male	Tenant	Room or apartment in individual house		10 000,00							
VAL/DART/KN/MG2/003	SOC/DART/KN/MG2/003	Male	Tenant	Room or apartment in individual house									
VAL/DART/KN/MG2/003	The interview was not done												
VAL/DART/KN/MG2/003	The interview was not done												
VAL/DART/KN/MG2/003	The interview was not done												
VAL/DART/KN/MG2/003	The interview was not done												
VAL/DART/KN/MG2/003	The interview was not done												
VAL/DART/KN/MG2/003	The interview was not done												
VAL/DART/KN/MG2/003	The interview was not done												
VAL/DART/KN/MG2/003	The interview was not done												
VAL/DART/KN/MG2/005	SOC/DART/KN/MG2/005	Male	Tenant	Room or apartment in individual house		10 000,00	10 000,00	3 000,00	10 000,00	30 000,00			
VAL/DART/KN/MG2/005	SOC/DART/KN/MG2/005A	Female	Tenant	Room or apartment in individual house		10 000,00	10 000,00	3 000,00	10 000,00	45 000,00			
VAL/DART/KN/MG2/005	SOC/DART/KN/MG2/005B	Male	Tenant	Room or apartment in individual house		20 000,00	10 000,00	3 000,00	10 000,00	45 000,00			
VAL/DART/KN/MG2/007	The interview was not done												
VAL/DART/KN/MG2/007	SOC/DART/KN/MG2/007	Female	Tenant	Room or apartment in individual house		50 000,00		2 000,00	25 000,00	30 000,00		10 000,00	
VAL/DART/KN/MG2/007	SOC/DART/KN/MG2/007A	Male	Other	Individual house		100.000	90 000,00	2 000,00	40 000,00	10 000,00			
VAL/DART/KN/MG2/007	The interview was not done												
VAL/DART/KN/MG2/007	The interview was not done												
VAL/DART/KN/MG2/007	RENCE												

Legend
 Yes
 Not/Not Informed

Annex 03 - Part 1

Data Bank related to Sections 01, 02, and 06 of the Socio-Economic Survey - Residential Resettlement Questionnaire

Code of Corresponding Property Survey	Code of this Socio-Economic Unit	Family Head	Type of occupant	Type of residence:	Other:	Cost of Rent if tenants	Water + Sewer	Garbage Collection	Energy	Gas	Land Rent	Property Tax	Is there any commercial use of residence?
VAL DAB E KN KN 000	FENCE		0000										
VAL DAB E KN MN 000	FENCE		0000										
VAL DAB E KN MN 000	FENCE		0000										

Legend
 Yes
 Not/Not Informed

Annex 03 - Part 2
Duty Bank related to Sections 107, 107.1 and 106 of the Mortgage Lending Act - Residential Residential on Employment

End of corresponding Project Series	Rate of the Service Economic Type	Net Debt	Rating for Family Risk	Age	Marital Status	Level of Education	Type of Work Contract	Other Income Sources	Average Monthly Income (TZS)	Income Stabilization	Mean of Expenditure to Work	Level of Expenditure to Work (TZS)	Average Weekly Expenditure to Work (TZS)	Period Spending (TZS)	Mean of Expenditure to School	Level of Expenditure to School	Average Weekly Expenditure to School (TZS)
VALDARTU/GRSN01	27	Single	Basic School	...	...	...	...	...	...	...	...	...	...	...
VALDARTU/GRSN02	42	Married	Basic School	...	...	...	...	...	...	...	...	...	...	...
VALDARTU/GRSN03
VALDARTU/GRSN04
VALDARTU/GRSN05
VALDARTU/GRSN06
VALDARTU/GRSN07
VALDARTU/GRSN08
VALDARTU/GRSN09
VALDARTU/GRSN10
VALDARTU/GRSN11
VALDARTU/GRSN12
VALDARTU/GRSN13
VALDARTU/GRSN14
VALDARTU/GRSN15
VALDARTU/GRSN16
VALDARTU/GRSN17
VALDARTU/GRSN18
VALDARTU/GRSN19
VALDARTU/GRSN20
VALDARTU/GRSN21
VALDARTU/GRSN22
VALDARTU/GRSN23
VALDARTU/GRSN24
VALDARTU/GRSN25
VALDARTU/GRSN26
VALDARTU/GRSN27
VALDARTU/GRSN28
VALDARTU/GRSN29
VALDARTU/GRSN30
VALDARTU/GRSN31
VALDARTU/GRSN32
VALDARTU/GRSN33
VALDARTU/GRSN34
VALDARTU/GRSN35
VALDARTU/GRSN36
VALDARTU/GRSN37
VALDARTU/GRSN38
VALDARTU/GRSN39
VALDARTU/GRSN40
VALDARTU/GRSN41
VALDARTU/GRSN42
VALDARTU/GRSN43
VALDARTU/GRSN44
VALDARTU/GRSN45
VALDARTU/GRSN46
VALDARTU/GRSN47
VALDARTU/GRSN48
VALDARTU/GRSN49
VALDARTU/GRSN50

Annex 03 - Part 2
Data Bank related to Neighbors 02 and the end of the Survey component Survey - Residential Reassessment Observation Data

Individual Responding Proprietor Name	Code of the Survey Component Type	No. of Dwellers	Relation to Family Head	Sex	Age	Marital Status	Level of Education	Type of Work Function	Other Income Sources	Average Monthly Income (TZS)	Type of Occupation	Manual Transportation Means	Type of Transportation Means	Average Weekly Transportation Cost (TZS)	Period Used for Analysis	Method of Transportation Used	Year of Travel to School	Average Weekly Transportation Cost (TZS)	
VALDART/IL/C/N001	SPK-DART-IL-GRN-01	Other	other relative	M	1	Single	Intermediate School										40	130700	
VALDART/IL/C/N002	SPK-DART-IL-GRN-02	Other	other relative	F	1	Single	Intermediate School												
VALDART/IL/C/N003	SPK-DART-IL-GRN-03	Other	other relative	M	1	Single	Intermediate School												
VALDART/IL/C/N004	SPK-DART-IL-GRN-04	Other	other relative	F	1	Single	Intermediate School												
VALDART/IL/C/N005	SPK-DART-IL-GRN-05	Other	other relative	M	1	Single	Intermediate School												
VALDART/IL/C/N006	SPK-DART-IL-GRN-06	Other	other relative	F	1	Single	Intermediate School												
VALDART/IL/C/N007	SPK-DART-IL-GRN-07	Other	other relative	M	1	Single	Intermediate School												
VALDART/IL/C/N008	SPK-DART-IL-GRN-08	Other	other relative	F	1	Single	Intermediate School												
VALDART/IL/C/N009	SPK-DART-IL-GRN-09	Other	other relative	M	1	Single	Intermediate School												
VALDART/IL/C/N010	SPK-DART-IL-GRN-10	Other	other relative	F	1	Single	Intermediate School												
VALDART/IL/C/N011	SPK-DART-IL-GRN-11	Other	other relative	M	1	Single	Intermediate School												
VALDART/IL/C/N012	SPK-DART-IL-GRN-12	Other	other relative	F	1	Single	Intermediate School												
VALDART/IL/C/N013	SPK-DART-IL-GRN-13	Other	other relative	M	1	Single	Intermediate School												
VALDART/IL/C/N014	SPK-DART-IL-GRN-14	Other	other relative	F	1	Single	Intermediate School												
VALDART/IL/C/N015	SPK-DART-IL-GRN-15	Other	other relative	M	1	Single	Intermediate School												
VALDART/IL/C/N016	SPK-DART-IL-GRN-16	Other	other relative	F	1	Single	Intermediate School												
VALDART/IL/C/N017	SPK-DART-IL-GRN-17	Other	other relative	M	1	Single	Intermediate School												
VALDART/IL/C/N018	SPK-DART-IL-GRN-18	Other	other relative	F	1	Single	Intermediate School												
VALDART/IL/C/N019	SPK-DART-IL-GRN-19	Other	other relative	M	1	Single	Intermediate School												
VALDART/IL/C/N020	SPK-DART-IL-GRN-20	Other	other relative	F	1	Single	Intermediate School												
VALDART/IL/C/N021	SPK-DART-IL-GRN-21	Other	other relative	M	1	Single	Intermediate School												
VALDART/IL/C/N022	SPK-DART-IL-GRN-22	Other	other relative	F	1	Single	Intermediate School												

Annex 03 - Part 2

Data Bank related to Section 01.05, part 06, of the Supplemental Resettlement Obligations

Code of Corresponding Property Service	Code of the Social-Economic Unit	Rel. to Dyallers	Residence in Family Head	Sex	Age	Marital Status	Level of Education	Type of Work Contract	Employment Status	Average Monthly Income (TJS)	Income from Occupating	Means of Transport to Work	Time of Travel to Work	Average Weekly Transport Expenditure (TZS)	Period (Qualifying)	Means of Transport to School	Time of Travel to School	Average Weekly Transport Expenditure (TZS)
VALDART/IL/GRN/03				M	13	Single	Basic School											
VALDART/IL/GRN/03				M	16	Single	Basic School											
VALDART/IL/GRN/03				F	12	Single	Basic School											
VALDART/IL/GRN/03				F	9	Single	Basic School											
VALDART/IL/GRN/03				F	6	Single	None											
VALDART/IL/GRN/03				M	4	Single	None											
VALDART/IL/GRN/03				F	20	Single	Basic School											
VALDART/IL/GRN/03				F	20	Single	Intermediate School											
VALDART/IL/GRN/03				M	18	Single	Basic School											
VALDART/IL/GRN/03				M	30	Single	University											
VALDART/IL/GRN/03				M	10	Single	Basic School											
VALDART/IL/GRN/04			Family Head	F	42	Single	Intermediate School	Registered employee		499,000.00	19	daily	100	2,000.00	Morning	daily	100	100.00
VALDART/IL/GRN/04			Other	F	14	Single	Intermediate School								Morning	daily	120	100.00
VALDART/IL/GRN/04			Other	M	12	Single	Intermediate School								Morning	daily	100	100.00
VALDART/IL/GRN/04			Other	F	9	Single	Basic School								Morning	daily	105	100.00
VALDART/IL/GRN/04			Other	M	5	Single	Basic School								Morning	daily	120	100.00
VALDART/IL/GRN/04			Other relative	M	14	Single	Intermediate School								Morning	daily	100	100.00
VALDART/IL/GRN/04			Other	F	15	Single	Intermediate School								Morning	daily	100	100.00

Annex 03 - Part 2

Data Bank related to Nations 01, 02 and the end of the Sample response Survey - Residential Resettlement Questionnaire

Color of Corresponding Response Survey	Code of the Sample Resettlement	No. of Dweller	Response to Family Head	Sex	Age	Marital Status	Level of Education	Type of Work	Other Income Source	Average Monthly Income (TZS)	Time of Occupation	Method of Transport to Work	Level of Income	Average Weekly Transport Cost (TZS)	Period of Spending	Method of Transport to School	Time of Transport to School	Average Weekly Transport Cost (TZS)
VAJUBAR/IZAKR/V008	SOC DART II, ERN 008	Other	son/daughter	F	17	Single	Intermediate School			17 000,00					Morning	data/da	120	1 000,00
VAJUBAR/IZAKR/V008	SOC DART II, ERN 008	Other	son/daughter	M	19	Single	Intermediate School								Morning	data/da	180	1 000,00
VAJUBAR/IZAKR/V008	SOC DART II, ERN 008	Other	son/daughter	M	17	Single	Intermediate School								Occasional	car-motorbike	8 hrs	14 000,00
VAJUBAR/IZAKR/V008	SOC DART II, ERN 008	Other	son/daughter	F	14	Single	Intermediate School								Morning	data/da	120	1 000,00
VAJUBAR/IZAKR/V008	SOC DART II, ERN 008	Other	son/daughter	M	8	Single	Basic School								Personal	car-motorbike	240	5 000,00
VAJUBAR/IZAKR/V008	SOC DART II, ERN 008	Other	son/daughter	M	5	Single	Basic School								Morning	data/da	180	30 000,00
VAJUBAR/IZAKR/V008	SOC DART II, ERN 008	Other	son/daughter	F	17	Single	Intermediate School								Morning	data/da	120	1 000,00

Annex 03 - Part 2
Data Bank related to Section of 02 and the of the Socio-economic Survey - Residential Resillience of Dwellers

Code of Responding Property Sector	Code of the Socio-Economic Survey	Relationship	Sex	Age	Marital Status	Level of Education	Type of Work	Other Income Sources	Average Monthly Income (USD)	Time to Acquisition	Means of Transport to Work	Time of Transport to Work (hrs)	Average Weekly Transport to Work (USD)	Period Spent (hours)	Means of Transport to School	Time of Transport to School	Average Weekly Transport to School (USD)
VA/VAFTI/GRN04	SOC DART II GRN 04I	Other	F	27	Single	University											
VA/VAFTI/GRN04	SOC DART II GRN 04I	Other	M	25	Single	Intermediate School											
VA/VAFTI/GRN04	SOC DART II GRN 04I	Other	F	22	Single	Investor											
VA/VAFTI/GRN04	SOC DART II GRN 04I	Other	F	18	Single	Intermediate School											
VA/VAFTI/GRN04	SOC DART II GRN 04I	Other	M	15	Single	Intermediate School											
VA/VAFTI/GRN04	SOC DART II GRN 04I	Other	F	12	Single	Basic School											
VA/VAFTI/GRN04	SOC DART II GRN 04I	Other	F	22	Single	Basic School											

Annex 03 - Part 2

Table 3.1: Bank related to Sections 01, 02 and 03 of the Socio-economic Survey - Residential Resettlement Occupants

Code of corresponding Property Survey	Code of the Socio-Economic Survey	No. of Dweller	Relationship with Head	Sex	Age	Marital Status	Level of Education	Type of Work Contract	Owner Income Source	Average Monthly Income (TZS)	Years in Occupation	Means of Transport to Work	Time of Travel to School	Period of Study	Means of Transport to School	Time of Travel to School	Average Weekly Transportation Cost to Work (TZS)	Average Monthly Transportation Cost to School (TZS)
VAL/DART/IL/GRN/04	SOC DART II GRN 041	1	son/daughter	M	12	Single	Basic School							Afternoon	walking	120		
VAL/DART/IL/GRN/04	SOC DART II GRN 042	1	son/daughter	M	16	Single	Intermediate School							Full time	car-motorbike	120		1 000 000
VAL/DART/IL/GRN/04	SOC DART II GRN 043	1	son/daughter	M	9	Single	Basic School							Afternoon	walking	120		
VAL/DART/IL/GRN/04	SOC DART II GRN 044	1	son/daughter	F	4	Single	None				33	daladala		Afternoon	walking	120		
VAL/DART/IL/GRN/04	SOC DART II GRN 045	1	Not applicable	M	59	Married	Basic School	Registered employee							none		2 800 000	
VAL/DART/IL/GRN/04	SOC DART II GRN 046	1	Not applicable	F	41	Married	Basic School								none			
VAL/DART/IL/GRN/04	SOC DART II GRN 047	1	son/daughter	M	25	Single	Intermediate School								none			
VAL/DART/IL/GRN/04	SOC DART II GRN 048	1	son/daughter	F	22	Single	University							Full time	none			
VAL/DART/IL/GRN/04	SOC DART II GRN 049	1	son/daughter	M	17	Single	Intermediate School							Full time	none			
VAL/DART/IL/GRN/04	SOC DART II GRN 050	1	son/daughter	F	21	Single	Intermediate School							Morning	daladala	60		400 000
VAL/DART/IL/GRN/04	SOC DART II GRN 051	1	aggregates	F	27	Single	Basic School								none			
VAL/DART/IL/GRN/04	SOC DART II GRN 052	1	Grandson/Granddaughter	F	1	Single	None								none			
VAL/DART/IL/GRN/04	SOC DART II GRN 053	1	Family Head	M	49	Married	Basic School	Registered employee			12	daladala			none			
VAL/DART/IL/GRN/04	SOC DART II GRN 054	1	Spouse	F	39	Married	Intermediate School								none			
VAL/DART/IL/GRN/04	SOC DART II GRN 055	1	Other relative	M	30	Single	Intermediate School	Self-employed			3	daladala			none			
VAL/DART/IL/GRN/04	SOC DART II GRN 056	1	Other relative	M	31	Single	Basic School								none			
VAL/DART/IL/GRN/04	SOC DART II GRN 057	1	son/daughter	M	14	Single	Intermediate School							Morning	daladala	30		5 000 000
VAL/DART/IL/GRN/04	SOC DART II GRN 058	1	son/daughter	F	9	Single	Basic School							Morning	daladala	60		20 000 per month
VAL/DART/IL/GRN/04	SOC DART II GRN 059	1	son/daughter	M	2	Single	None								none			

Annex 03 - Part 2

Data Bank related to Nations III 02 and III of the National Economic Survey - Residential Resettlement of Displaced Persons

Code of Corresponding Property Survey	Code of the Sub-Economic Unit	Head of Dweller	Relation to Family Head	Sex	Age	Marital Status	Level of Education	Type of Work Contract	Other Income Sources	Average Monthly Income (TZS)	Time in Occupation	Means of Transport to Work	Time of Travel to Work	Average Weekly Earnings from Work (TZS)	Period (Morning)	Means of Transport to School	Time of Travel to School	Average Weekly Earnings from School (TZS)	
VALDARTULGRN053	SAC DART II GRN 053	Other	son/daughter	M	23	Single	University												
VALDARTULGRN053	SOC DART II GRN 053	Other	son/daughter	F	7	Single	Basic School												
VALDARTULGRN053	SOC DART II GRN 053	Other	aggregates	F	26	Single	Basic School												
VALDARTULGRN054	SAC DART II GRN 054	Family Head	Not applicable	M	56	Married	Intermediate School	Registered employee		900 000.00	35	daladala	30	2 400.00					
VALDARTULGRN054	SOC DART II GRN 054	Spouse	Not applicable	F	46	Married	Basic School											2 000.00	
VALDARTULGRN054	SOC DART II GRN 054	Other	son/daughter	F	20	Single	Intermediate School											2 800.00	
VALDARTULGRN054	SOC DART II GRN 054	Other	son/daughter	F	26	Single	Intermediate School											2 000.00	
VALDARTULGRN054	SOC DART II GRN 054	Other	son/daughter	M	24	Single	Intermediate School											2 000.00	
VALDARTULGRN054	SOC DART II GRN 054	Other	son/daughter	M	22	Single	Intermediate School											2 000.00	
VALDARTULGRN054	SOC DART II GRN 054	Other	other relative	M	16	Single	Intermediate School											1 000.00	
VALDARTULGRN055	SOC DART II GRN 055	Family Head	Not applicable	M	36	Married	Intermediate School	Registered employee		275 000.00	10	daladala	30						
VALDARTULGRN055	SOC DART II GRN 055	Spouse	Not applicable	F	30	Married	University			600 000.00	2.5	daladala	60						
VALDARTULGRN055	SOC DART II GRN 055	Other	other relative	F	26	Single	Basic School												
VALDARTULGRN055	SOC DART II GRN 055	Other	aggregates	F	26	Single	Basic School												
VALDARTULGRN056	SOC DART II GRN 056	Family Head	Not applicable	M		Married	University	Registered employee			22	daladala	30	1 000.00					
VALDARTULGRN056	SOC DART II GRN 056	Spouse	Not applicable	F		Married	Basic School												
VALDARTULGRN056	SOC DART II GRN 056	Other	son/daughter	M	18	Single	Intermediate School												
VALDARTULGRN056	SOC DART II GRN 056	Other	son/daughter	M	13	Single	Basic School												

Annex 03 - Part 2
Data Bank related to the National of 02 and the end of the Survey - Residential Reassessment Questionnaire

Code of Responding Province Survey	Code of this Year Response Unit	Net Employer	Position in Family Head	Sex	Age	Marital Status	Level of Education	Type of Work Sector	Type Industry	Average Monthly Income (\$/M)	Form of Occupation	Number of Employees	Form of Employment	Average Weekly Income (\$/W)	Form of Employment (Salaried)	Number of Employees at Work	Average Weekly Income (\$/W)	Form of Employment (Salaried)	Number of Employees at Work	Average Weekly Income (\$/W)
VADARTIL/CRN066	001			M	48	M	High School	Government	Public	1,800	Full-time	1	Full-time	1,800	Full-time	1	1,800	Full-time	1	1,800
VADARTIL/CRN066	001			F	45	M	High School	Government	Public	1,500	Full-time	1	Full-time	1,500	Full-time	1	1,500	Full-time	1	1,500
VADARTIL/CRN066	001			M	42	M	High School	Government	Public	2,000	Full-time	1	Full-time	2,000	Full-time	1	2,000	Full-time	1	2,000
VADARTIL/CRN066	001			F	40	M	High School	Government	Public	1,600	Full-time	1	Full-time	1,600	Full-time	1	1,600	Full-time	1	1,600
VADARTIL/CRN066	001			M	38	M	High School	Government	Public	1,900	Full-time	1	Full-time	1,900	Full-time	1	1,900	Full-time	1	1,900
VADARTIL/CRN066	001			F	36	M	High School	Government	Public	1,700	Full-time	1	Full-time	1,700	Full-time	1	1,700	Full-time	1	1,700
VADARTIL/CRN066	001			M	34	M	High School	Government	Public	2,100	Full-time	1	Full-time	2,100	Full-time	1	2,100	Full-time	1	2,100
VADARTIL/CRN066	001			F	32	M	High School	Government	Public	1,800	Full-time	1	Full-time	1,800	Full-time	1	1,800	Full-time	1	1,800
VADARTIL/CRN066	001			M	30	M	High School	Government	Public	2,200	Full-time	1	Full-time	2,200	Full-time	1	2,200	Full-time	1	2,200
VADARTIL/CRN066	001			F	28	M	High School	Government	Public	1,900	Full-time	1	Full-time	1,900	Full-time	1	1,900	Full-time	1	1,900
VADARTIL/CRN066	001			M	26	M	High School	Government	Public	2,300	Full-time	1	Full-time	2,300	Full-time	1	2,300	Full-time	1	2,300
VADARTIL/CRN066	001			F	24	M	High School	Government	Public	2,000	Full-time	1	Full-time	2,000	Full-time	1	2,000	Full-time	1	2,000
VADARTIL/CRN066	001			M	22	M	High School	Government	Public	2,400	Full-time	1	Full-time	2,400	Full-time	1	2,400	Full-time	1	2,400
VADARTIL/CRN066	001			F	20	M	High School	Government	Public	2,100	Full-time	1	Full-time	2,100	Full-time	1	2,100	Full-time	1	2,100
VADARTIL/CRN066	001			M	18	M	High School	Government	Public	2,500	Full-time	1	Full-time	2,500	Full-time	1	2,500	Full-time	1	2,500
VADARTIL/CRN066	001			F	16	M	High School	Government	Public	2,200	Full-time	1	Full-time	2,200	Full-time	1	2,200	Full-time	1	2,200
VADARTIL/CRN066	001			M	14	M	High School	Government	Public	2,600	Full-time	1	Full-time	2,600	Full-time	1	2,600	Full-time	1	2,600
VADARTIL/CRN066	001			F	12	M	High School	Government	Public	2,300	Full-time	1	Full-time	2,300	Full-time	1	2,300	Full-time	1	2,300
VADARTIL/CRN066	001			M	10	M	High School	Government	Public	2,700	Full-time	1	Full-time	2,700	Full-time	1	2,700	Full-time	1	2,700
VADARTIL/CRN066	001			F	8	M	High School	Government	Public	2,400	Full-time	1	Full-time	2,400	Full-time	1	2,400	Full-time	1	2,400
VADARTIL/CRN066	001			M	6	M	High School	Government	Public	2,800	Full-time	1	Full-time	2,800	Full-time	1	2,800	Full-time	1	2,800
VADARTIL/CRN066	001			F	4	M	High School	Government	Public	2,500	Full-time	1	Full-time	2,500	Full-time	1	2,500	Full-time	1	2,500
VADARTIL/CRN066	001			M	2	M	High School	Government	Public	2,900	Full-time	1	Full-time	2,900	Full-time	1	2,900	Full-time	1	2,900

Annex 03 - Part 2

Data Bank related to Nations III, 02 and 04 of the Socio-Economic Survey - Resettled at Kessilanga of District of Ulu

Code of corresponding Property Survey	Code of the Socio-Economic Unit	No of Dweller	Relationship to the Head	Sex	Age	Marital Status	Level of Education	Type of Work Contract	CRS Income Source	Average Monthly Income (TZS)	Time in Occupation	Means of Transport to Work	Period of Standings	Means of Transport to School	Time of Travel to School	Average Weekly Transport to School Cost (TZS)
VALDARTIL/GRN/061	SOC DART IL GRN 061	Family Head	Not applicable	M	31	Live together	Intermediate School	Registered employee	200,000.00	200,000.00	20	daladala	none	daladala	120	100.00
VALDARTIL/GRN/061	SOC DART IL GRN 061	Spouse	Not applicable	F	50	Live together	Intermediate School	Registered employee					Morning	daladala	120	100.00
VALDARTIL/GRN/061	SOC DART IL GRN 061	Other	son/daughter	F	32	Single	Basic School						Morning	daladala	105	100.00
VALDARTIL/GRN/061	SOC DART IL GRN 061	Other	son/daughter	F	20	Single	Intermediate School						Morning	daladala	120	100.00
VALDARTIL/GRN/061	SOC DART IL GRN 061	Other	son/daughter	F	18	Single	Intermediate School						Morning	daladala	105	100.00
VALDARTIL/GRN/061	SOC DART IL GRN 061	Other	son/daughter	F	12	Single	Basic School						Morning	daladala	105	100.00
VALDARTIL/GRN/061	SOC DART IL GRN 061	Other	grandson/granddaughter	M	34	Single	Intermediate School						Morning	daladala	105	100.00
VALDARTIL/GRN/061	SOC DART IL GRN 061	Other	grandson/granddaughter	M	10	Single	Basic School	Registered employee			20	daladala	none	daladala	105	100.00
VALDARTIL/GRN/062	SOC DART IL GRN 062	Family Head	Not applicable	M	50	Live together	Intermediate School	Registered employee	200,000.00	200,000.00	20	daladala	none	daladala	150	900.00
VALDARTIL/GRN/062	SOC DART IL GRN 062	Spouse	Not applicable	F	42	Live together	Intermediate School	Registered employee					Morning	daladala	150	900.00
VALDARTIL/GRN/062	SOC DART IL GRN 062	Other	son/daughter	F	25	Single	University						Morning	daladala	120	100.00
VALDARTIL/GRN/062	SOC DART IL GRN 062	Other	son/daughter	F	22	Single	Intermediate School						Morning	daladala	100	100.00
VALDARTIL/GRN/062	SOC DART IL GRN 062	Other	son/daughter	M	20	Single	Intermediate School						Morning	daladala	100	100.00
VALDARTIL/GRN/062	SOC DART IL GRN 062	Other	son/daughter	M	17	Single	Intermediate School						Morning	daladala	100	100.00
VALDARTIL/GRN/062	SOC DART IL GRN 062	Other	son/daughter	F	16	Single	Intermediate School						Morning	daladala	100	100.00
VALDARTIL/GRN/062	SOC DART IL GRN 062	Other	grandson/granddaughter	F	18	Single	Intermediate School						Morning	daladala	120	100.00
VALDARTIL/GRN/062	SOC DART IL GRN 062	Other	grandson/granddaughter	F	16	Single	Intermediate School						Morning	daladala	100	100.00
VALDARTIL/GRN/063	SOC DART IL GRN 063	Family Head	Not applicable	M	59	Married	Basic School	Registered employee	120,000.00	120,000.00	35	em-motohake	none	daladala	120	4,000.00
VALDARTIL/GRN/063	SOC DART IL GRN 063	Spouse	Not applicable	F	53	Married	Basic School	Other	200,000.00	200,000.00	15	walking	none	daladala	60	3,500.00
VALDARTIL/GRN/063	SOC DART IL GRN 063	Other	son/daughter	F	30	Single	Basic School	Registered employee	80,000.00	80,000.00	7	daladala	daladala	daladala	60	3,500.00
VALDARTIL/GRN/063	SOC DART IL GRN 063	Other	son/daughter	M	24	Single	University						Morning	daladala	120	7,000.00
VALDARTIL/GRN/063	SOC DART IL GRN 063	Other	son/daughter	M	22	Married	Intermediate School						Morning	daladala	120	7,000.00
VALDARTIL/GRN/063	SOC DART IL GRN 063	Other	son/daughter	M	20	Single	University						Morning	daladala	60	3,500.00

Annex 03 - Part 2
Data Bank on the Status of the Non-economic Status Residential Resettlement Applicants

Code of Corresponding Property Values	Code of the Status Economic Data	% of Family	Relation to Family Head	Sex	Age	Marital Status	Level of Education	Type of Work Contract	Other Income Sources	Average Monthly Income (TZS)	Time in Occupation	Means of Transport to Work	Period (Working Hours)	Average Weekly Transport Cost to Work (TZS)	Time of Travel to Work	Means of Transport to School	Period (Working Hours)	Average Weekly Transport Cost to School (TZS)
VAZDARTILGRN065	VAZDARTILGRN065	1	Not applicable	M	47	Married	Basic School	Registered employee	Retirement	142,000/00	35	daladala	Full time	1,000/00	60	none	Full time	1,000/00
VAZDARTILGRN066	SOC DARTILGRN065	1	Spouse	F	42	Married	Intermediate School									none		
VAZDARTILGRN067	SOC DARTILGRN065	1	Other son/daughter	M	23	Single	Intermediate School									none		
VAZDARTILGRN068	SOC DARTILGRN065	1	Other son/daughter	F	20	Single	Intermediate School									none		
VAZDARTILGRN069	SOC DARTILGRN065	1	Other son/daughter	M	18	Single	Intermediate School									none		
VAZDARTILGRN070	SOC DARTILGRN065	1	Other son/daughter	F	5	Single	Under 2 years									none		
VAZDARTILGRN071	SOC DARTILGRN065	1	Other son/daughter	F	5	Single	Under 2 years									none		
VAZDARTILGRN072	SOC DARTILGRN066	1	Family Head	M	55	Married	Intermediate School	Registered employee	Retirement	300,000/00	30	daladala	Full time	1,000/00	60	none	Full time	1,000/00
VAZDARTILGRN073	SOC DARTILGRN066	1	Spouse	F	50	Married	Basic School									none		
VAZDARTILGRN074	SOC DARTILGRN066	1	Other son/daughter	M	30	Single	Intermediate School									none		
VAZDARTILGRN075	SOC DARTILGRN066	1	Other son/daughter	F	25	Single	Intermediate School									none		
VAZDARTILGRN076	SOC DARTILGRN066	1	Other son/daughter	M	20	Single	Intermediate School									none		
VAZDARTILGRN077	SOC DARTILGRN066	1	Other son/daughter	M	10	Single	Basic School									none		
VAZDARTILGRN078	SOC DARTILGRN066	1	Other relative	M	50	Single	Basic School									none		
VAZDARTILGRN079	SOC DARTILGRN067	1	Family Head	M	53	Married	Intermediate School	Pre-Contract	Other	600,000/00	35	daladala	Full time	1,000/00	60	none	Full time	1,000/00
VAZDARTILGRN080	SOC DARTILGRN067	1	Spouse	F	37	Married	Intermediate School	Self-employed		300,000/00	15	none	Full time			none	Full time	
VAZDARTILGRN081	SOC DARTILGRN067	1	Other son/daughter	M	26	Single	Intermediate School									none		
VAZDARTILGRN082	SOC DARTILGRN067	1	Other son/daughter	F	17	Single	Basic School									daladala	Afternoon	200/00
VAZDARTILGRN083	SOC DARTILGRN067	1	Other son/daughter	F	16	Single	Basic School									daladala	Afternoon	200/00
VAZDARTILGRN084	SOC DARTILGRN067	1	Other son/daughter	M	15	Single	Basic School									cat-motoflake	Full time	200/00
VAZDARTILGRN085	SOC DARTILGRN067	1	Other son/daughter	M	11	Single	Basic School									cat-motoflake	Full time	200/00

Annex 03 - Part 2
Data Bank related to Nations 01, 02 and 03 of the Socio-economic Survey: Residential Resettlement Outcomes

Code of Corresponding Property Survey	Code of the Socio-Economic Type	No. of Dwellers	Relation to Family Head	Sex	Age	Marital Status	Level of Education	Type of Work	OREG Income Source	Average Monthly Income (TZS)	Time in Occupation	Means of Transport to Work	Time of Transport to Work	Average Weekly Transport Time to Work (TZS)	Period Available	Means of Transport to School	Time of Transport to School	Average Weekly Transport Time to School (TZS)
VALDARTILGRN069
VALDARTILGRN070
VALDARTILGRN071
VALDARTILGRN072
VALDARTILGRN073
VALDARTILGRN074
VALDARTILGRN075
VALDARTILGRN076
VALDARTILGRN077
VALDARTILGRN078
VALDARTILGRN079
VALDARTILGRN080
VALDARTILGRN081
VALDARTILGRN082
VALDARTILGRN083
VALDARTILGRN084
VALDARTILGRN085
VALDARTILGRN086
VALDARTILGRN087
VALDARTILGRN088
VALDARTILGRN089
VALDARTILGRN090
VALDARTILGRN091
VALDARTILGRN092
VALDARTILGRN093
VALDARTILGRN094
VALDARTILGRN095
VALDARTILGRN096
VALDARTILGRN097
VALDARTILGRN098
VALDARTILGRN099
VALDARTILGRN100

Annex 03 - Part 2
Data Bank related to Sections 01, 02 and 03 of the Survey program Survey - Residential Resettlement (Questionnaire)

Code of Corresponding Property Survey	Code of the Survey Economic Type	No. of Dwellers	Bedrooms	Yes	Age	Marital Status	Level of Education	Type of Work	Other Income Sources	Average Monthly Income (USD)	Time in Occupation	Measure of Frequency to Work	Type of Frequency to Work	Average Weekly Frequency to Work (USD)	Period (Months)	Measure of Frequency to School	Type of Frequency to School	Average Monthly Frequency to School (USD)	
VA/DK/RT/DE/GR/W/7	SOC/PAE/IL/GR/077	Other	graduate	M	9	Single	Basic School					none			none			120	100000

Annex 03 - Part 2

Table 3.2: Data on the Socio-economic Survey: Residential Resettlement Questionnaire

Code of Corresponding Property Survey	Code of the Socio-Economic Unit	N of Dwellers	Relation to Family Head	Sex	Age	Marital Status	Level of Education	Type of Work Contract	Index Income Category	Average Monthly Income (TZS)	Time in Occupation	Means of Transport to Work	Time of Travel to Work	Average Weekly Transport Cost to Work (TZS)	Period (Seasons)	Means of Transport to School	Time of Travel to School	Average Weekly Transport Cost to School (TZS)
VALDART/GRN078		1	son/daughter	M	11	Single	Basic School	Registered employee	1	100,000.00	31	datadala	60	500.00		none	120	50,000.00
VALDART/GRN078	SOC/DART/GRN078	Other	son/daughter	M	11	Single	Basic School	Registered employee	1	100,000.00	31	datadala	60	500.00		none	120	50,000.00
VALDART/GRN078	SOC/DART/GRN078	Other	other relative	M	62	Married	Intermediate School	Registered employee	1	500,000.00	2	datadala	60	500.00		none	120	50,000.00
VALDART/GRN078	SOC/DART/GRN078	Other	aggregates	F	7	Single	Basic School	Registered employee	1	150,000.00	1	datadala	60	500.00		none	120	50,000.00
VALDART/GRN079	SOC/DART/GRN079	Family Head	Not applicable	M	47	Widow	Basic School	Registered employee	1	1,000,000.00	31	datadala	60	500.00		none	120	50,000.00
VALDART/GRN079	SOC/DART/GRN079	Other	son/daughter	F	25	Married	University	Registered employee	1	500,000.00	2	datadala	60	500.00		none	120	50,000.00
VALDART/GRN079	SOC/DART/GRN079	Other	son/daughter	M	28	Single	University	Registered employee	1	150,000.00	1	datadala	60	500.00		none	120	50,000.00
VALDART/GRN079	SOC/DART/GRN079	Other	son/daughter	M	19	Single	Intermediate School	Registered employee	1	1,000,000.00	31	datadala	60	500.00		none	120	50,000.00
VALDART/GRN079	SOC/DART/GRN079	Other	son/daughter	M	16	Single	Intermediate School	Registered employee	1	500,000.00	2	datadala	60	500.00		none	120	50,000.00
VALDART/GRN079	SOC/DART/GRN079	Other	son/daughter	F	20	Single	Intermediate School	Registered employee	1	150,000.00	1	datadala	60	500.00		none	120	50,000.00
VALDART/GRN079	SOC/DART/GRN079	Other	son/daughter	M	15	Single	Intermediate School	Registered employee	1	1,000,000.00	31	datadala	60	500.00		none	120	50,000.00
VALDART/GRN079	SOC/DART/GRN079	Other	son/daughter	F	16	Single	Intermediate School	Registered employee	1	500,000.00	2	datadala	60	500.00		none	120	50,000.00
VALDART/GRN080	SOC/DART/GRN080	Family Head	Not applicable	M	60	Live together	Intermediate School	Registered employee	1	500,000.00	20	none		2,800.00		none		
VALDART/GRN080	SOC/DART/GRN080	Spouse	Not applicable	F	50	Live together	Basic School	Self-employed	1	300,000.00	20	none		2,800.00		none		
VALDART/GRN080	SOC/DART/GRN080	Other	son/daughter	F	33	Single	Intermediate School	Informal	1	200,000.00	4	datadala	80	2,800.00		none		
VALDART/GRN080	SOC/DART/GRN080	Other	son/daughter	F	20	Single	Basic School	Informal	1	100,000.00	4	datadala	80	2,800.00		none		
VALDART/GRN080	SOC/DART/GRN080	Other	son/daughter	M	19	Single	Intermediate School	Informal	1	100,000.00	4	datadala	80	2,800.00		none		
VALDART/GRN080	SOC/DART/GRN080	Other	son/daughter	F	17	Single	Basic School	Informal	1	100,000.00	4	datadala	80	2,800.00		none		
VALDART/GRN080	SOC/DART/GRN080	Other	son/daughter	F	15	Single	Basic School	Informal	1	100,000.00	4	datadala	80	2,800.00		none		
VALDART/GRN080	SOC/DART/GRN080	Other	son/daughter	M	12	Single	Basic School	Informal	1	100,000.00	4	datadala	80	2,800.00		none		
VALDART/GRN080	SOC/DART/GRN080	Other	son/daughter	F	9	Single	Basic School	Informal	1	100,000.00	4	datadala	80	2,800.00		none		
VALDART/GRN080	SOC/DART/GRN080	Other	son/daughter	F	17	Single	Basic School	Informal	1	100,000.00	4	datadala	80	2,800.00		none		
VALDART/GRN081	SOC/DART/GRN081	Family Head	Not applicable	M	37	Married	Intermediate School	Registered employee	1	1,000,000.00	31	datadala	60	500.00		none		

Annex 03 - Part 2
Data Back related to Sections 01, 02 and 04 of the Survey Questionnaire - Residential Reassessment of Dwellers

Name of the Preparation Property System	Name of the Sub-System	Year of Birth	Beginning of Family Head	Sex	Age	Marital Status	Level of Education	Type of Work	Other Income Sources	Average Monthly Income (USD)	Type of Expenses	Material Expenses for Work	Time of Expenses for Work	Average Weekly Expenses for Work (USD)	Period (Months)	Means of Transport to School	Time of Transport to School	Average Weekly Expenses for Transport (USD)
VAD/ARTIL/RSN007	SOC/DARTEL/RSN007	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN008	SOC/DARTEL/RSN008	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN009	SOC/DARTEL/RSN009	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN010	SOC/DARTEL/RSN010	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN011	SOC/DARTEL/RSN011	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN012	SOC/DARTEL/RSN012	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN013	SOC/DARTEL/RSN013	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN014	SOC/DARTEL/RSN014	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN015	SOC/DARTEL/RSN015	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN016	SOC/DARTEL/RSN016	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN017	SOC/DARTEL/RSN017	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN018	SOC/DARTEL/RSN018	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN019	SOC/DARTEL/RSN019	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN020	SOC/DARTEL/RSN020	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN021	SOC/DARTEL/RSN021	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN022	SOC/DARTEL/RSN022	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN023	SOC/DARTEL/RSN023	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN024	SOC/DARTEL/RSN024	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN025	SOC/DARTEL/RSN025	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN026	SOC/DARTEL/RSN026	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN027	SOC/DARTEL/RSN027	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN028	SOC/DARTEL/RSN028	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN029	SOC/DARTEL/RSN029	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN030	SOC/DARTEL/RSN030	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN031	SOC/DARTEL/RSN031	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN032	SOC/DARTEL/RSN032	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN033	SOC/DARTEL/RSN033	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN034	SOC/DARTEL/RSN034	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN035	SOC/DARTEL/RSN035	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN036	SOC/DARTEL/RSN036	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN037	SOC/DARTEL/RSN037	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN038	SOC/DARTEL/RSN038	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN039	SOC/DARTEL/RSN039	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN040	SOC/DARTEL/RSN040	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN041	SOC/DARTEL/RSN041	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN042	SOC/DARTEL/RSN042	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN043	SOC/DARTEL/RSN043	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN044	SOC/DARTEL/RSN044	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN045	SOC/DARTEL/RSN045	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN046	SOC/DARTEL/RSN046	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN047	SOC/DARTEL/RSN047	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN048	SOC/DARTEL/RSN048	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN049	SOC/DARTEL/RSN049	Other	single	M	1	1	Primary								1	on foot	15	15
VAD/ARTIL/RSN050	SOC/DARTEL/RSN050	Other	single	M	1	1	Primary								1	on foot	15	15

Annex 03 - Part 2

Data Bank related to Sections 011, 012 and 013 of the Socio-economic Survey - Residential Reassessment Database

Code of Corresponding Property Survey	Code of this Survey - Economic Unit	Nº of Dwellers	Relation to Family Head	Sex	Age	Marital Status	Level of Education	Type of Work	Other Income Sources	Average Monthly Income (TZS)	Time in Occupation	Means of Transport to Work	Type of Transport to Work	Average Weekly Transport Cost to Work (TZS)	Period (Working)	Means of Transport to School	Type of Transport to School	Average Weekly Transport Cost (TZS)
VAD/AR/11/4/GRN/900	11-4-GRN-000	1	son/daughter	M	28	Single	Intermediate School								Monthly	Walk	120	10000
VAD/AR/11/4/GRN/900	11-4-GRN-000	1	son/daughter	F	39	Single	University											
VAD/AR/11/4/GRN/900	11-4-GRN-000	1	son/daughter	M	21	Single	University											
VAD/AR/11/4/GRN/900	11-4-GRN-000	1	son/daughter	M	16	Single	Intermediate School											
VAD/AR/11/4/GRN/900	11-4-GRN-000	1	son/daughter	F	11	Single	Base School											
VAD/AR/11/4/GRN/900	11-4-GRN-000	1	son/daughter	M	15	Single	Intermediate School											
VAD/AR/11/4/GRN/900	11-4-GRN-000	1	son/daughter	M	28	Single	Intermediate School											
VAD/AR/11/4/GRN/900	11-4-GRN-000	1	son/daughter	F	39	Single	University											
VAD/AR/11/4/GRN/900	11-4-GRN-000	1	son/daughter	M	21	Single	University											
VAD/AR/11/4/GRN/900	11-4-GRN-000	1	son/daughter	M	16	Single	Intermediate School											
VAD/AR/11/4/GRN/900	11-4-GRN-000	1	son/daughter	F	11	Single	Base School											
VAD/AR/11/4/GRN/900	11-4-GRN-000	1	son/daughter	M	15	Single	Intermediate School											

Annex 03 - Part 2
 Data Bank 1: List of the Successive Survey Results of Residential Resettlement Occupants

Name of Corresponding Property Survey	Code of the Sub-Location Unit	No. of Dwellers	Relationship to Family Head	Sex	Age	Marital Status	Level of Education	Type of Work Contract	Employment Sector	Average Monthly Income (TZS)	Time in Occupation	Means of Transport to Work	Time of Transport to School	Mode of Transport to School	Time of Transport to School	Average Weekly Transport Cost to Work (TZS)	Period Available	Average Weekly Transport Cost to School (TZS)
		1	Head	M	55	Married	University	Registered employee	Public	600,000.00	30	DalaDala	Full time	DalaDala	90	3,500.00	Full time	1,200.00
		1	Head	F	57	Married	University	Registered employee	Public	400,000.00	25	DalaDala	Full time	DalaDala	240	1,200.00	Full time	1,200.00
		1	Head	M	21	Single	University	Registered employee	Public	450,000.00	35	DalaDala	Full time	DalaDala	100	2,000.00	Morning	100.00
		1	Head	F	22	Single	University	Registered employee	Public	450,000.00	35	DalaDala	Full time	DalaDala	100	2,000.00	Morning	100.00
		1	Head	M	19	Single	University	Registered employee	Public	450,000.00	35	DalaDala	Full time	DalaDala	100	2,000.00	Morning	100.00
		1	Head	F	16	Single	University	Registered employee	Public	450,000.00	35	DalaDala	Full time	DalaDala	100	2,000.00	Morning	100.00
		1	Head	M	21	Single	University	Registered employee	Public	450,000.00	35	DalaDala	Full time	DalaDala	100	2,000.00	Morning	100.00
		1	Head	F	22	Single	University	Registered employee	Public	450,000.00	35	DalaDala	Full time	DalaDala	100	2,000.00	Morning	100.00
		1	Head	M	19	Single	University	Registered employee	Public	450,000.00	35	DalaDala	Full time	DalaDala	100	2,000.00	Morning	100.00
		1	Head	F	16	Single	University	Registered employee	Public	450,000.00	35	DalaDala	Full time	DalaDala	100	2,000.00	Morning	100.00
		1	Head	M	21	Single	University	Registered employee	Public	450,000.00	35	DalaDala	Full time	DalaDala	100	2,000.00	Morning	100.00
		1	Head	F	22	Single	University	Registered employee	Public	450,000.00	35	DalaDala	Full time	DalaDala	100	2,000.00	Morning	100.00
		1	Head	M	19	Single	University	Registered employee	Public	450,000.00	35	DalaDala	Full time	DalaDala	100	2,000.00	Morning	100.00
		1	Head	F	16	Single	University	Registered employee	Public	450,000.00	35	DalaDala	Full time	DalaDala	100	2,000.00	Morning	100.00
		1	Head	M	21	Single	University	Registered employee	Public	450,000.00	35	DalaDala	Full time	DalaDala	100	2,000.00	Morning	100.00
		1	Head	F	22	Single	University	Registered employee	Public	450,000.00	35	DalaDala	Full time	DalaDala	100	2,000.00	Morning	100.00
		1	Head	M	19	Single	University	Registered employee	Public	450,000.00	35	DalaDala	Full time	DalaDala	100	2,000.00	Morning	100.00
		1	Head	F	16	Single	University	Registered employee	Public	450,000.00	35	DalaDala	Full time	DalaDala	100	2,000.00	Morning	100.00
		1	Head	M	21	Single	University	Registered employee	Public	450,000.00	35	DalaDala	Full time	DalaDala	100	2,000.00	Morning	100.00
		1	Head	F	22	Single	University	Registered employee	Public	450,000.00	35	DalaDala	Full time	DalaDala	100	2,000.00	Morning	100.00
		1	Head	M	19	Single	University	Registered employee	Public	450,000.00	35	DalaDala	Full time	DalaDala	100	2,000.00	Morning	100.00
		1	Head	F	16	Single	University	Registered employee	Public	450,000.00	35	DalaDala	Full time	DalaDala	100	2,000.00	Morning	100.00

Annex 03 - Part 2
Data Bank related to National 01 02 and to the Socio-economic Survey - Residential Reassessment (the survey)

Code of Responding Property Space	Code of the Socio-Economic Survey	Sex of Dweller	Relation to Family Head	Sex	Age	Marital Status	Level of Education	Type of Job Category	Other Source	Average Monthly Income (TZS)	Time in Occupation	Means of Transport to Work	Time of Travel to Work (TZS)	Average Weekly Hours of Transport to Work (TZS)	Period of Transport to Work	Means of Transport to School	Time of Travel to School	Average Weekly Hours of Transport to School (TZS)
VADWARTH/GRV106	SOC DART IL GRN 106	Other	other relative	F	26	Single	Intermediate School	Registered employee	Other	410,000.00					Afternoon	datadi	60	600.00
VADWARTH/GRV106	SOC DART IL GRN 106	Female Head	Not applicable	F	30	Single	Intermediate School	Registered employee	Other	410,000.00						none		
VADWARTH/GRV106	SOC DART IL GRN 106	Other	other relative	F	23	Single	Intermediate School											
VADWARTH/GRV106	SOC DART IL GRN 106	Other	other relative	F	26	Single	Intermediate School											

Annex 03 - Part 2

Data Bank related to Section 01.02 in the Socio-economic Survey - Residential Resettlement Questionnaire

Code of Corresponding Property Survey	Code of the Socio-Economic Unit	N° of Dweller	Relation to Family Head	Sex	Age	Marital Status	Level of Education	Type of Work Contract	Other Income Sources	Average Monthly Income (TZS)	Time in Occupation	Means of Transport to Work	Time of Travel to Work	Average Weekly Transport Cost to Work (TZS)	Period (Student)	Means of Transport to School	Time of Travel to School	Average Weekly Transport Cost to School (TZS)
VAL/DART/IL/GRN/106	SOC/DART IL GRN 106	Other	other relative	F	25	Single	Intermediary School								Occasional	daladala	80	500.00
VAL/DART/IL/GRN/106	SOC/DART IL GRN 106	Other	other relative	F	14	Single	Basic School								Full time	car-motorbike		25.000.00
VAL/DART/IL/GRN/106	SOC/DART IL GRN 106	Other	son/daughter	M		Single	None									none		
VAL/DART/KN/HNF/010	SOC/DART KN HNF 010A	Family Head	Not applicable	F	53	Married	Intermediary School	Registered employee			31	daladala	60	7.000.00		none		
VAL/DART/KN/HNF/010	SOC/DART KN HNF 010A	Spouse	Not applicable	M	46	Married	Intermediary School									none		
VAL/DART/KN/HNF/010	SOC/DART KN HNF 010A	Other	Not applicable	M	58	Married	Intermediary School									none		
VAL/DART/KN/HNF/010	SOC/DART KN HNF 010A	Other	son/daughter	M	35	Single	Basic School									none		
VAL/DART/KN/HNF/010	SOC/DART KN HNF 010A	Other	son/daughter	M	27	Single	Intermediary School									none		
VAL/DART/KN/HNF/010	SOC/DART KN HNF 010A	Other	son/daughter	F	26	Single	Intermediary School									none		
VAL/DART/KN/HNF/010	SOC/DART KN HNF 010A	Other	son/daughter	F	24	Single	Intermediary School									none		
VAL/DART/KN/HNF/010	SOC/DART KN HNF 010A	Other	son/daughter	M	14	Single	Intermediary School									none		
VAL/DART/KN/HNF/010	SOC/DART KN HNF 010A	Other	son/daughter	M	18	Single	Intermediary School									none		
VAL/DART/KN/HNF/010	SOC/DART KN HNF 010A	Other	son/daughter	F	17	Single	Intermediary School									none		
VAL/DART/KN/HNF/010	SOC/DART KN HNF 010A	Other	grandson/granddaughter	M	4	Single										none		
VAL/DART/KN/HNF/010	SOC/DART KN HNF 010A	Other	other relative	M	48											none		
VAL/DART/KN/HNF/011	SOC/DART KN HNF 011	Family Head	Not applicable	M	41	Live together	Basic School	Registered employee		100.000.00	3	daladala	60	30.000.00		none		
VAL/DART/KN/HNF/011	SOC/DART KN HNF 011	Spouse	Not applicable	F	27	Live together	Basic School									none		
VAL/DART/KN/HNF/011	SOC/DART KN HNF 011	Other	son/daughter	M	10	Single	Basic School								Full time	daladala	90	15.000.00
VAL/DART/KN/HNF/011	SOC/DART KN HNF 011	Other	son/daughter	F	9	Single	Basic School								Full time	daladala	30	10.000.00
VAL/DART/KN/HNF/011	SOC/DART KN HNF 011A	Family Head	Not applicable	M	38	Married	Basic School	Informal		200.000.00	5	daladala	60	7.000.00		none		
VAL/DART/KN/HNF/011	SOC/DART KN HNF 011A	Spouse	Not applicable	F	38	Married	Basic School	Informal		200.000.00	5	daladala	60	7.000.00		none		
VAL/DART/KN/HNF/011	SOC/DART KN HNF 011A	Other	son/daughter	F	14	Single	Basic School								Full time	car-motorbike	30	
VAL/DART/KN/HNF/011	SOC/DART KN HNF 011A	Other	son/daughter	F	11	Single	Basic School								Full time	car-motorbike	30	
VAL/DART/KN/HNF/011	SOC/DART KN HNF 011B	Family Head	Not applicable	M	60	Married	Intermediary School	Informal	Retirement	100.000.00	20	daladala	60	30.000.00		none		
VAL/DART/KN/HNF/011	SOC/DART KN HNF 011B	Spouse	Not applicable	F	55	Married	Basic School	Informal	Retirement	80.000.00	20	daladala	60	30.000.00		none		
VAL/DART/KN/HNF/011	SOC/DART KN HNF 011B	Other	son/daughter	M	25	Married	University								Full time	daladala	60	1.500.00
VAL/DART/KN/HNF/011	SOC/DART KN HNF 011B	Other	son/daughter	M	23	Single	Intermediary School								Full time	daladala	60	1.500.00
VAL/DART/KN/HNF/011	SOC/DART KN HNF 011B	Other	son/daughter	F	20	Single	Intermediary School								Full time	daladala	60	1.500.00
VAL/DART/KN/HNF/011	SOC/DART KN HNF 011B	Other	son/daughter	F	19	Single	Intermediary School								Full time	daladala	60	1.500.00
VAL/DART/KN/HNF/011	SOC/DART KN HNF 011B	Other	son/daughter	F	16	Single	Intermediary School								Full time	daladala	60	1.500.00

Annex 03 - Part 2
Data Bank related to Section 01, 02 and 06 of the Socio-economic Survey - Residential Resettlement Displacement

Code of Corresponding Problem Survey	Range of this Variable (Numeric Unit)	N of Dwellers	Relation to Family Head	Sex	Age	Marital Status	Level of Education	Type of Work Contract	Other Income Sources	Average Monthly Income (USD)	Time to Occupation	Means of Transport to Work	Time of Transit to Work	Average Weeks Transport Cost to Work (TZS)	Period Absence (Months)	Means of Transport to School	Time of Transit to School	Average Weeks Transport Cost to School (TZS)
VACURU/RES/01/001	1-10	1	Head	M	37	Married	Basic School	Self-employed		1,000,000	15	Walking	15min	1,000,000	0	Walking	30min	0
VACURU/RES/01/002	1-10	1	Head	F	35	Married	Basic School	Self-employed		1,000,000	15	Walking	15min	1,000,000	0	Walking	30min	0
VACURU/RES/01/003	1-10	1	Head	M	35	Married	Basic School	Self-employed		1,000,000	15	Walking	15min	1,000,000	0	Walking	30min	0
VACURU/RES/01/004	1-10	1	Head	M	35	Married	Basic School	Self-employed		1,000,000	15	Walking	15min	1,000,000	0	Walking	30min	0
VACURU/RES/01/005	1-10	1	Head	M	35	Married	Basic School	Self-employed		1,000,000	15	Walking	15min	1,000,000	0	Walking	30min	0
VACURU/RES/01/006	1-10	1	Head	M	35	Married	Basic School	Self-employed		1,000,000	15	Walking	15min	1,000,000	0	Walking	30min	0
VACURU/RES/01/007	1-10	1	Head	F	35	Married	Basic School	Self-employed		1,000,000	15	Walking	15min	1,000,000	0	Walking	30min	0
VACURU/RES/01/008	1-10	1	Head	M	35	Married	Basic School	Self-employed		1,000,000	15	Walking	15min	1,000,000	0	Walking	30min	0
VACURU/RES/01/009	1-10	1	Head	M	35	Married	Basic School	Self-employed		1,000,000	15	Walking	15min	1,000,000	0	Walking	30min	0
VACURU/RES/01/010	1-10	1	Head	F	35	Married	Basic School	Self-employed		1,000,000	15	Walking	15min	1,000,000	0	Walking	30min	0
VACURU/RES/01/011	1-10	1	Head	F	35	Married	Basic School	Self-employed		1,000,000	15	Walking	15min	1,000,000	0	Walking	30min	0
VACURU/RES/01/012	1-10	1	Head	M	35	Married	Basic School	Self-employed		1,000,000	15	Walking	15min	1,000,000	0	Walking	30min	0
VACURU/RES/01/013	1-10	1	Head	M	35	Married	Basic School	Self-employed		1,000,000	15	Walking	15min	1,000,000	0	Walking	30min	0
VACURU/RES/01/014	1-10	1	Head	F	35	Married	Basic School	Self-employed		1,000,000	15	Walking	15min	1,000,000	0	Walking	30min	0
VACURU/RES/01/015	1-10	1	Head	F	35	Married	Basic School	Self-employed		1,000,000	15	Walking	15min	1,000,000	0	Walking	30min	0
VACURU/RES/01/016	1-10	1	Head	M	35	Married	Basic School	Self-employed		1,000,000	15	Walking	15min	1,000,000	0	Walking	30min	0
VACURU/RES/01/017	1-10	1	Head	F	35	Married	Basic School	Self-employed		1,000,000	15	Walking	15min	1,000,000	0	Walking	30min	0
VACURU/RES/01/018	1-10	1	Head	M	35	Married	Basic School	Self-employed		1,000,000	15	Walking	15min	1,000,000	0	Walking	30min	0
VACURU/RES/01/019	1-10	1	Head	M	35	Married	Basic School	Self-employed		1,000,000	15	Walking	15min	1,000,000	0	Walking	30min	0
VACURU/RES/01/020	1-10	1	Head	M	35	Married	Basic School	Self-employed		1,000,000	15	Walking	15min	1,000,000	0	Walking	30min	0
VACURU/RES/01/021	1-10	1	Head	F	35	Married	Basic School	Self-employed		1,000,000	15	Walking	15min	1,000,000	0	Walking	30min	0
VACURU/RES/01/022	1-10	1	Head	M	35	Married	Basic School	Self-employed		1,000,000	15	Walking	15min	1,000,000	0	Walking	30min	0
VACURU/RES/01/023	1-10	1	Head	F	35	Married	Basic School	Self-employed		1,000,000	15	Walking	15min	1,000,000	0	Walking	30min	0
VACURU/RES/01/024	1-10	1	Head	M	35	Married	Basic School	Self-employed		1,000,000	15	Walking	15min	1,000,000	0	Walking	30min	0
VACURU/RES/01/025	1-10	1	Head	F	35	Married	Basic School	Self-employed		1,000,000	15	Walking	15min	1,000,000	0	Walking	30min	0

Annex 03 - Part 2
Data Bank to Section 01, 02, and 06 of the Non-employment Survey - Residential Resettlement Occupants

Code of Corresponding Property Survey	Code on the Survey Economic Unit	No. of Dwellers	Relation to Family Head	Sex	Age	Marital Status	Level of Education	Type of Work Contract	Other Income Sources	Average Monthly Income (TZS)	Time in Occupation	Means of Transport to Work	Time of Transport to Work	Average Weekly Commute to Work (TZS)	Period (months)	Means of Transport to School	Time of Transport to School	Average Weekly Transport to School (TZS)
S.A.P.001.E.1.1.1.1.1.1.1	1	1	Head of household	M	30	Married	Secondary	Full-time	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.2	2	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.3	3	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.4	4	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.5	5	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.6	6	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.7	7	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.8	8	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.9	9	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.10	10	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.11	11	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.12	12	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.13	13	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.14	14	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.15	15	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.16	16	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.17	17	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.18	18	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.19	19	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.20	20	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.21	21	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.22	22	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.23	23	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.24	24	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.25	25	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.26	26	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.27	27	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.28	28	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.29	29	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.30	30	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.31	31	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.32	32	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.33	33	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.34	34	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.35	35	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.36	36	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.37	37	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.38	38	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.39	39	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000
S.A.P.001.E.1.1.1.1.1.1.40	40	1	Spouse	F	30	Married	Secondary	None	None	1,000,000	1	None	10	500,000	1	None	10	500,000

Annex 03 - Part 2
Data Bank related to Schedules 01, 02 and 03 of the Survey on Economic Status - Residential Reassessment on Questionnaire

Code of corresponding Project Survey	Code of this Survey Economic Unit	No. of Inhabitants	Reference Family Head	Sex	Age	Marital Status	Level of Education	Type of Work Employment	Other Income Source	Average Monthly Income (TZS)	Type of Occupation	Mean of Transport to Work	Time of Travel to Work	Average Weekly Transport Cost to Work (TZS)	Period (Transport)	Mean of Transport to School	Time of Travel to School	Average Weekly Transport Cost to School (TZS)
VAD/ART/KN/KAR/004	NOE/ART/KN/KAR/004	Chief	single/dweller	M	10	Single	High School								10/15 min	walking		
VAD/ART/KN/KAR/004	NOE/ART/KN/KAR/004	Chief	single/dweller	M	10	Single	High School								10/15 min	walking		
VAD/ART/KN/KAR/004	NOE/ART/KN/KAR/004	Chief	single/dweller	M	10	Single	High School								10/15 min	walking		
VAD/ART/KN/KAR/004	NOE/ART/KN/KAR/004	Chief	single/dweller	M	10	Single	High School								10/15 min	walking		
VAD/ART/KN/KAR/004	NOE/ART/KN/KAR/004	Chief	single/dweller	M	10	Single	High School								10/15 min	walking		
VAD/ART/KN/KAR/004	NOE/ART/KN/KAR/004	Chief	single/dweller	M	10	Single	High School								10/15 min	walking		
VAD/ART/KN/KAR/004	NOE/ART/KN/KAR/004	Chief	single/dweller	M	10	Single	High School								10/15 min	walking		
VAD/ART/KN/KAR/004	NOE/ART/KN/KAR/004	Chief	single/dweller	M	10	Single	High School								10/15 min	walking		
VAD/ART/KN/KAR/004	NOE/ART/KN/KAR/004	Chief	single/dweller	M	10	Single	High School								10/15 min	walking		
VAD/ART/KN/KAR/004	NOE/ART/KN/KAR/004	Chief	single/dweller	M	10	Single	High School								10/15 min	walking		
VAD/ART/KN/KAR/004	NOE/ART/KN/KAR/004	Chief	single/dweller	M	10	Single	High School								10/15 min	walking		

Annex 03 - Part 2
Data Bank Related to Sections 01, 02, and 04 of the Socio-economic Survey - Results of Residential Resettlement Objectives-III

Code of Corresponding Property Survey	Code of the Socio-Economic Unit	Nº of Dwellers	Relation to Family Head	Sex	Age	Marital Status	Level of Education	Type of Work Contract	Emp. Income Source	Average Monthly Income (TZS)	Time in Occupation	Means of Transport to Work	Time of Transport to Work	Average Weekly Transport Expenditure to Work (TZS)	Period (Working)	Means of Transport to School	Time of Transport to School	Average Weekly Transport Expenditure to School (TZS)
S.A. 004111 - 1 - MR-004	004111 - 1 - MR-004	004	Head of Family	M	38	Married	Primary School	Self-employed	Own	1,800,000	10	Motorcycle	15	1,800,000	10	Motorcycle	15	1,800,000
S.A. 004111 - 1 - MR-004	004111 - 1 - MR-004	004	Head of Family	F	35	Married	Primary School	Self-employed	Own	1,800,000	10	Motorcycle	15	1,800,000	10	Motorcycle	15	1,800,000
S.A. 004111 - 1 - MR-004	004111 - 1 - MR-004	004	Head of Family	M	32	Married	Primary School	Self-employed	Own	1,800,000	10	Motorcycle	15	1,800,000	10	Motorcycle	15	1,800,000
S.A. 004111 - 1 - MR-004	004111 - 1 - MR-004	004	Head of Family	F	28	Married	Primary School	Self-employed	Own	1,800,000	10	Motorcycle	15	1,800,000	10	Motorcycle	15	1,800,000
S.A. 004111 - 1 - MR-004	004111 - 1 - MR-004	004	Head of Family	M	25	Married	Primary School	Self-employed	Own	1,800,000	10	Motorcycle	15	1,800,000	10	Motorcycle	15	1,800,000
S.A. 004111 - 1 - MR-004	004111 - 1 - MR-004	004	Head of Family	F	22	Married	Primary School	Self-employed	Own	1,800,000	10	Motorcycle	15	1,800,000	10	Motorcycle	15	1,800,000
S.A. 004111 - 1 - MR-004	004111 - 1 - MR-004	004	Head of Family	M	18	Married	Primary School	Self-employed	Own	1,800,000	10	Motorcycle	15	1,800,000	10	Motorcycle	15	1,800,000
S.A. 004111 - 1 - MR-004	004111 - 1 - MR-004	004	Head of Family	F	15	Married	Primary School	Self-employed	Own	1,800,000	10	Motorcycle	15	1,800,000	10	Motorcycle	15	1,800,000
S.A. 004111 - 1 - MR-004	004111 - 1 - MR-004	004	Head of Family	M	12	Married	Primary School	Self-employed	Own	1,800,000	10	Motorcycle	15	1,800,000	10	Motorcycle	15	1,800,000
S.A. 004111 - 1 - MR-004	004111 - 1 - MR-004	004	Head of Family	F	10	Married	Primary School	Self-employed	Own	1,800,000	10	Motorcycle	15	1,800,000	10	Motorcycle	15	1,800,000
S.A. 004111 - 1 - MR-004	004111 - 1 - MR-004	004	Head of Family	M	8	Married	Primary School	Self-employed	Own	1,800,000	10	Motorcycle	15	1,800,000	10	Motorcycle	15	1,800,000
S.A. 004111 - 1 - MR-004	004111 - 1 - MR-004	004	Head of Family	F	5	Married	Primary School	Self-employed	Own	1,800,000	10	Motorcycle	15	1,800,000	10	Motorcycle	15	1,800,000
S.A. 004111 - 1 - MR-004	004111 - 1 - MR-004	004	Head of Family	M	3	Married	Primary School	Self-employed	Own	1,800,000	10	Motorcycle	15	1,800,000	10	Motorcycle	15	1,800,000
S.A. 004111 - 1 - MR-004	004111 - 1 - MR-004	004	Head of Family	F	2	Married	Primary School	Self-employed	Own	1,800,000	10	Motorcycle	15	1,800,000	10	Motorcycle	15	1,800,000
S.A. 004111 - 1 - MR-004	004111 - 1 - MR-004	004	Head of Family	M	1	Married	Primary School	Self-employed	Own	1,800,000	10	Motorcycle	15	1,800,000	10	Motorcycle	15	1,800,000
S.A. 004111 - 1 - MR-004	004111 - 1 - MR-004	004	Head of Family	F	0	Married	Primary School	Self-employed	Own	1,800,000	10	Motorcycle	15	1,800,000	10	Motorcycle	15	1,800,000

Annex 03 - Part 2

Bar Bank related to Surveys 01, 02 and 06 of the Socio-economic Survey - Residential Resettlement of Otago Campsite

Code of Corresponding Property Survey	Code of the Socio-Economic Survey	Age	Sex	Marital Status	Level of Education	Type of Work Contract	Gross Monthly Income (TZS)	Average Monthly Income (TZS)	Time in Occupation	Metric of Transport to Work	Time of Travel to Work	Average Weekly Transport Cost to Work (TZS)	Period Transport (vehicles)	Metric of Transport to School	Time of Travel to School	Average Weekly Transport to School Cost (TZS)
V.01	01	35	M	Single	Secondary	Full-time	1,000,000	1,000,000	10	Public	15	1,000,000	1	Public	15	1,000,000
V.02	02	35	M	Single	Secondary	Full-time	1,000,000	1,000,000	10	Public	15	1,000,000	1	Public	15	1,000,000
V.03	03	35	M	Single	Secondary	Full-time	1,000,000	1,000,000	10	Public	15	1,000,000	1	Public	15	1,000,000
V.04	04	35	M	Single	Secondary	Full-time	1,000,000	1,000,000	10	Public	15	1,000,000	1	Public	15	1,000,000
V.05	05	35	M	Single	Secondary	Full-time	1,000,000	1,000,000	10	Public	15	1,000,000	1	Public	15	1,000,000
V.06	06	35	M	Single	Secondary	Full-time	1,000,000	1,000,000	10	Public	15	1,000,000	1	Public	15	1,000,000
V.07	07	35	M	Single	Secondary	Full-time	1,000,000	1,000,000	10	Public	15	1,000,000	1	Public	15	1,000,000
V.08	08	35	M	Single	Secondary	Full-time	1,000,000	1,000,000	10	Public	15	1,000,000	1	Public	15	1,000,000
V.09	09	35	M	Single	Secondary	Full-time	1,000,000	1,000,000	10	Public	15	1,000,000	1	Public	15	1,000,000
V.10	10	35	M	Single	Secondary	Full-time	1,000,000	1,000,000	10	Public	15	1,000,000	1	Public	15	1,000,000
V.11	11	35	M	Single	Secondary	Full-time	1,000,000	1,000,000	10	Public	15	1,000,000	1	Public	15	1,000,000
V.12	12	35	M	Single	Secondary	Full-time	1,000,000	1,000,000	10	Public	15	1,000,000	1	Public	15	1,000,000
V.13	13	35	M	Single	Secondary	Full-time	1,000,000	1,000,000	10	Public	15	1,000,000	1	Public	15	1,000,000
V.14	14	35	M	Single	Secondary	Full-time	1,000,000	1,000,000	10	Public	15	1,000,000	1	Public	15	1,000,000
V.15	15	35	M	Single	Secondary	Full-time	1,000,000	1,000,000	10	Public	15	1,000,000	1	Public	15	1,000,000
V.16	16	35	M	Single	Secondary	Full-time	1,000,000	1,000,000	10	Public	15	1,000,000	1	Public	15	1,000,000
V.17	17	35	M	Single	Secondary	Full-time	1,000,000	1,000,000	10	Public	15	1,000,000	1	Public	15	1,000,000
V.18	18	35	M	Single	Secondary	Full-time	1,000,000	1,000,000	10	Public	15	1,000,000	1	Public	15	1,000,000
V.19	19	35	M	Single	Secondary	Full-time	1,000,000	1,000,000	10	Public	15	1,000,000	1	Public	15	1,000,000
V.20	20	35	M	Single	Secondary	Full-time	1,000,000	1,000,000	10	Public	15	1,000,000	1	Public	15	1,000,000
V.21	21	35	M	Single	Secondary	Full-time	1,000,000	1,000,000	10	Public	15	1,000,000	1	Public	15	1,000,000
V.22	22	35	M	Single	Secondary	Full-time	1,000,000	1,000,000	10	Public	15	1,000,000	1	Public	15	1,000,000
V.23	23	35	M	Single	Secondary	Full-time	1,000,000	1,000,000	10	Public	15	1,000,000	1	Public	15	1,000,000
V.24	24	35	M	Single	Secondary	Full-time	1,000,000	1,000,000	10	Public	15	1,000,000	1	Public	15	1,000,000
V.25	25	35	M	Single	Secondary	Full-time	1,000,000	1,000,000	10	Public	15	1,000,000	1	Public	15	1,000,000
V.26	26	35	M	Single	Secondary	Full-time	1,000,000	1,000,000	10	Public	15	1,000,000	1	Public	15	1,000,000
V.27	27	35	M	Single	Secondary	Full-time	1,000,000	1,000,000	10	Public	15	1,000,000	1	Public	15	1,000,000
V.28	28	35	M	Single	Secondary	Full-time	1,000,000	1,000,000	10	Public	15	1,000,000	1	Public	15	1,000,000
V.29	29	35	M	Single	Secondary	Full-time	1,000,000	1,000,000	10	Public	15	1,000,000	1	Public	15	1,000,000
V.30	30	35	M	Single	Secondary	Full-time	1,000,000	1,000,000	10	Public	15	1,000,000	1	Public	15	1,000,000

Annex 03 - Part 2

Data Bank related to Sections 01, 02 and 06 of the Socioeconomic Survey - Residential Resettlement Questionnaire

Code of Corresponding Property Survey	Code of this Socio-Economic Unit	N° of Dweller	Relation to Family Head	Sex	Age	Marital Status	Level of Education	Type of Work Contract	Other Income Sources	Average Monthly Income (TZS)	Time in Occupation	Means of Transport to Work	Time of Travel to Work	Average Weekly Transport Expenditure (TZS)	Period (Season)	Means of Transport to School	Time of Travel to School	Average Weekly Transport School Cost (TZS)
VAL/DART/KN/KMR/013	SOC/DART/KN/KMR/013C	Other	son/daughter	F	10	Single	Basic School								Morning	daladala	120	100.00
VAL/DART/KN/KMR/013	SOC/DART/KN/KMR/013C	Other	son/daughter	M	8	Single	Basic School								Morning	daladala	120	100.00
VAL/DART/KN/KMR/013	SOC/DART/KN/KMR/013C	Other	son/daughter	M	6	Single	None									none		
VAL/DART/KN/KMR/013	SOC/DART/KN/KMR/013C	Other	son/daughter	F	3	Single	None									none		
VAL/DART/KN/KMR/013	SOC/DART/KN/KMR/013D	Family Head	Not applicable	F	30	Married	Basic School	Registered employee		200.000.00	13	daladala	69	600.00		none		
VAL/DART/KN/KMR/013	SOC/DART/KN/KMR/013D	Spouse	Not applicable	F	28	Married	Intermediary School									none		
VAL/DART/KN/KMR/013	SOC/DART/KN/KMR/013D	Other	son/daughter	F	7	Single	Basic School								Morning	walking	10	
VAL/DART/KN/KMR/015	SOC/DART/KN/KMR/015	Family Head	Not applicable	F		Single	University	Registered employee		250.000.00	1	walking	40			none		
VAL/DART/KN/KMR/015	SOC/DART/KN/KMR/015	Other	#N/D	F		Single	Basic School	Registered employee		40.000.00	1	walking				none		
VAL/DART/KN/KMR/018	SOC/DART/KN/KMR/018	Family Head	Not applicable	M	30	Live together	Basic School	Self-employed		400.000.00	8	daladala	20	400.00		none		
VAL/DART/KN/KMR/018	SOC/DART/KN/KMR/018	Spouse	Not applicable	F	29	Live together	Basic School									none		
VAL/DART/KN/KMR/018	SOC/DART/KN/KMR/018	Other	son/daughter	F	1	Single										none		
VAL/DART/KN/KMR/018	SOC/DART/KN/KMR/018A	Family Head	Not applicable	M	46	Married	Basic School	Informal		350.000.00	4	walking	20			none		
VAL/DART/KN/KMR/018	SOC/DART/KN/KMR/018A	Spouse	Not applicable	F	35	Married	Basic School									none		
VAL/DART/KN/KMR/018	SOC/DART/KN/KMR/018A	Other	son/daughter	M	14										Morning	daladala	30	200.00
VAL/DART/KN/KMR/018	SOC/DART/KN/KMR/018A	Other	son/daughter	M	7										Morning	walking	10	
VAL/DART/KN/KMR/018	SOC/DART/KN/KMR/018A	Other	son/daughter	F	3											none		
VAL/DART/KN/KMR/018	SOC/DART/KN/KMR/018B	Family Head	Not applicable	F	25	Separated	Basic School	Informal		300.000.00	3	daladala	30	400.00		none		
VAL/DART/KN/KMR/018	SOC/DART/KN/KMR/018B	Other	son/daughter	F	-										Morning	walking	15	
VAL/DART/KN/KMR/019	SOC/DART/KN/KMR/019	Family Head	Not applicable	F	25	Married	Basic School									none		
VAL/DART/KN/KMR/019	SOC/DART/KN/KMR/019	Spouse	Not applicable	M	30	Married	Intermediary School	Self-employed		80.000.00	9	daladala	80	1.500.00		none		
VAL/DART/KN/KMR/019	SOC/DART/KN/KMR/019	Other	son/daughter	M	5	Single	Basic School								Morning	daladala	80	700.00
VAL/DART/KN/KMR/019	SOC/DART/KN/KMR/019A	Family Head	Not applicable	F	75	Widow	None	Self-employed	Other	115.000.00	42	daladala	240	7.000.00		none		
VAL/DART/KN/KMR/019	SOC/DART/KN/KMR/019A	Other	son/daughter	F	37	Single	Basic School	Self-employed		70.000.00	6	walking	5			none		
VAL/DART/KN/KMR/019	SOC/DART/KN/KMR/019A	Other	son/daughter	F	40	Single	Basic School	Self-employed		100.000.00	7	car-motorbike	1440	8.900.00		none		
VAL/DART/KN/KMR/019	SOC/DART/KN/KMR/019A	Other	son/daughter	M	42	Married	Basic School	Self-employed		60.000.00	10	daladala	120	1.000.00		none		
VAL/DART/KN/KMR/019	SOC/DART/KN/KMR/019A	Other	son/daughter	M	34	Single	Basic School	Self-employed		35.000.00	6	daladala	60	1.000.00		none		
VAL/DART/KN/KMR/019	SOC/DART/KN/KMR/019A	Other	son/daughter	M	29	Married	Basic School	Self-employed		40.000.00	7	daladala	48	1.000.00		none		
VAL/DART/KN/KMR/019	SOC/DART/KN/KMR/019A	Other	son/daughter	M	27	Single	Basic School	Self-employed		30.000.00	5	daladala	80	1.000.00		none		
VAL/DART/KN/KMR/019	SOC/DART/KN/KMR/019A	Other	grandson/granddaughter	F	20	Single	Basic School									none		

Annex 03 - Part 2
Pilot Bank related to Nations 01, 02 and 06 of the Socio-economic Survey - Residential Resettlement Questionnaire

Code of origin/Place of origin	Code of this Survey	Sex	Relation to Family Head	Sex	Age	Marital Status	Level of Education	Type of Work Contract	Other Income Sources	Average Monthly Income (TZS)	Turn in Transportation	Mean of Transport to Work	Time of Transport to Work	Average Weekly Transport to Work (TZS)	Period (Yearly)	Mean of Transport to School	Time of Transport to School	Average Weekly Transport to School (TZS)
VA/D/RT/K/N/K/B/001	SOC DART KN KKR 01A	M	son	M	27	Married	High School	Government		1,000,000	1	Walking	15	1,000,000	1	Walking	15	1,000,000
VA/D/RT/K/N/K/B/001	SOC DART KN KKR 01A	F	daughter	F	4	Single	High School	Government		1,000,000	1	Walking	15	1,000,000	1	Walking	15	1,000,000
VA/D/RT/K/N/K/B/001	SOC DART KN KKR 01A	M	son	M	41	Married	High School	Government		1,000,000	1	Walking	15	1,000,000	1	Walking	15	1,000,000
VA/D/RT/K/N/K/B/001	SOC DART KN KKR 01B	M	son	M	39	Married	High School	Government		1,000,000	1	Walking	15	1,000,000	1	Walking	15	1,000,000
VA/D/RT/K/N/K/B/001	SOC DART KN KKR 01B	F	daughter	F	10	Single	High School	Government		1,000,000	1	Walking	15	1,000,000	1	Walking	15	1,000,000
VA/D/RT/K/N/K/B/001	SOC DART KN KKR 01B	M	son	M	35	Married	High School	Government		1,000,000	1	Walking	15	1,000,000	1	Walking	15	1,000,000
VA/D/RT/K/N/K/B/001	SOC DART KN KKR 01B	F	daughter	F	12	Single	High School	Government		1,000,000	1	Walking	15	1,000,000	1	Walking	15	1,000,000
VA/D/RT/K/N/K/B/001	SOC DART KN KKR 01C	N/A	Not applicable	N/A	27	Married	High School	Government		1,000,000	1	Walking	15	1,000,000	1	Walking	15	1,000,000
VA/D/RT/K/N/K/B/001	SOC DART KN KKR 01A	M	son	M	41	Married	High School	Government		1,000,000	1	Walking	15	1,000,000	1	Walking	15	1,000,000
VA/D/RT/K/N/K/B/001	SOC DART KN KKR 01A	F	daughter	F	7	Single	High School	Government		1,000,000	1	Walking	15	1,000,000	1	Walking	15	1,000,000
VA/D/RT/K/N/K/B/001	SOC DART KN KKR 01A	M	son	M	41	Married	High School	Government		1,000,000	1	Walking	15	1,000,000	1	Walking	15	1,000,000
VA/D/RT/K/N/K/B/001	SOC DART KN KKR 01A	F	daughter	F	4	Single	High School	Government		1,000,000	1	Walking	15	1,000,000	1	Walking	15	1,000,000
VA/D/RT/K/N/K/B/001	SOC DART KN KKR 01B	M	son	M	39	Married	High School	Government		1,000,000	1	Walking	15	1,000,000	1	Walking	15	1,000,000
VA/D/RT/K/N/K/B/001	SOC DART KN KKR 01B	F	daughter	F	10	Single	High School	Government		1,000,000	1	Walking	15	1,000,000	1	Walking	15	1,000,000
VA/D/RT/K/N/K/B/001	SOC DART KN KKR 01B	M	son	M	35	Married	High School	Government		1,000,000	1	Walking	15	1,000,000	1	Walking	15	1,000,000
VA/D/RT/K/N/K/B/001	SOC DART KN KKR 01B	F	daughter	F	12	Single	High School	Government		1,000,000	1	Walking	15	1,000,000	1	Walking	15	1,000,000
VA/D/RT/K/N/K/B/001	SOC DART KN KKR 01C	N/A	Not applicable	N/A	27	Married	High School	Government		1,000,000	1	Walking	15	1,000,000	1	Walking	15	1,000,000

Annex 03 - Part 2
Data Bank related to Section 04, 07 and the other Non-exception Survey Residential Reassessment Dwelling

Legal Corresponding Property Value	Legal of this Survey Form of Unit	No. of Dwellers	Relation to Family Head	%	Age	Marital Status	Level of Finishing	Type of Work Pattern	Other Income Sources	Average Monthly Income (T\$/M)	Level of Expenditure	Months of Expenditure on Work	Level of Expenditure on Work (T\$/M)	Average Weekly Expenditure on Work (T\$/W)	Period (Year/Year)	Months of Expenditure on School	Level of Expenditure on School	Average Weekly Expenditure on School (T\$/W)		
V.A/D.R/F/K/S/M/G2/005	SOC DARE KN M/2/ 005	Family Head	son/daughter	F	18	Single	Intermediate School								Monthly	12th		100/00		
V.A/D.R/F/K/S/M/G2/005	SOC DARE KN M/2/ 005	Family Head	Not applicable	F	44	Married	Basic School													
V.A/D.R/F/K/S/M/G2/005	SOC DARE KN M/2/ 005	Spouse	Not applicable	F	34	Married	Basic School													
V.A/D.R/F/K/S/M/G2/005	SOC DARE KN M/2/ 005	Spouse	Not applicable	F	24	Married	Basic School													
V.A/D.R/F/K/S/M/G2/005	SOC DARE KN M/2/ 005	Spouse	Not applicable	F	14	Married	Basic School													
V.A/D.R/F/K/S/M/G2/007	SOC DARE KN M/2/ 007	Family Head	son/daughter	F	11	Separated	Basic School													
V.A/D.R/F/K/S/M/G2/007	SOC DARE KN M/2/ 007	Family Head	son/daughter	F	18	Separated	Intermediate School									Monthly	12th		100/00	
V.A/D.R/F/K/S/M/G2/007	SOC DARE KN M/2/ 007	Family Head	son/daughter	F	17	Separated	Intermediate School													
V.A/D.R/F/K/S/M/G2/007	SOC DARE KN M/2/ 007	Family Head	son/daughter	F	16	Separated	Intermediate School													
V.A/D.R/F/K/S/M/G2/007	SOC DARE KN M/2/ 007	Family Head	son/daughter	F	15	Separated	Intermediate School													
V.A/D.R/F/K/S/M/G2/007	SOC DARE KN M/2/ 007	Family Head	son/daughter	F	14	Separated	Intermediate School													
V.A/D.R/F/K/S/M/G2/007	SOC DARE KN M/2/ 007	Family Head	son/daughter	F	13	Separated	Intermediate School													
V.A/D.R/F/K/S/M/G2/007	SOC DARE KN M/2/ 007	Family Head	son/daughter	F	12	Separated	Intermediate School													
V.A/D.R/F/K/S/M/G2/007	SOC DARE KN M/2/ 007	Family Head	son/daughter	F	11	Separated	Intermediate School													
V.A/D.R/F/K/S/M/G2/007	SOC DARE KN M/2/ 007	Family Head	son/daughter	F	10	Separated	Intermediate School													
V.A/D.R/F/K/S/M/G2/007	SOC DARE KN M/2/ 007	Family Head	son/daughter	F	9	Separated	Intermediate School													
V.A/D.R/F/K/S/M/G2/007	SOC DARE KN M/2/ 007	Family Head	son/daughter	F	8	Separated	Intermediate School													
V.A/D.R/F/K/S/M/G2/007	SOC DARE KN M/2/ 007	Family Head	son/daughter	F	7	Separated	Intermediate School													
V.A/D.R/F/K/S/M/G2/007	SOC DARE KN M/2/ 007	Family Head	son/daughter	F	6	Separated	Intermediate School													
V.A/D.R/F/K/S/M/G2/007	SOC DARE KN M/2/ 007	Family Head	son/daughter	F	5	Separated	Intermediate School													
V.A/D.R/F/K/S/M/G2/007	SOC DARE KN M/2/ 007	Family Head	son/daughter	F	4	Separated	Intermediate School													
V.A/D.R/F/K/S/M/G2/007	SOC DARE KN M/2/ 007	Family Head	son/daughter	F	3	Separated	Intermediate School													
V.A/D.R/F/K/S/M/G2/007	SOC DARE KN M/2/ 007	Family Head	son/daughter	F	2	Separated	Intermediate School													
V.A/D.R/F/K/S/M/G2/007	SOC DARE KN M/2/ 007	Family Head	son/daughter	F	1	Separated	Intermediate School													
V.A/D.R/F/K/S/M/G2/007	SOC DARE KN M/2/ 007	Family Head	son/daughter	F	0	Separated	Intermediate School													

Annex 03 - Part 2
Data Bank 1 (1) of 10 - Survey of the Socio-Economic Survey - Residential Resettlement Questionnaire

Scale of Corresponding Property Survey	Code of this Socio-Economic Unit	N° of Dweller	Relation to Family Head	Sex	Age	Marital Status	Level of Education	Type of Work (contract)	Other Income Sources	Average Monthly Income (TZS)	Main Occupation	Means of Transport to Work	Level of Transport to Work	Average Weekly Income (TZS)	Period (Study)	Means of Transport to School	Time of Travel to School	Average Weekly Transport to School Cost (TZS)
1	001	1	Head of Household	Male	35	Married	Primary School	Self-employed		15000	Farmer	Own	1	15000	1 year	Own	30 min	0
1	002	2	Spouse	Female	32	Married	Primary School	Self-employed		12000	Farmer	Own	1	12000	1 year	Own	30 min	0
1	003	3	Child	Male	15	Single	Primary School	Student		5000	Farmer	Own	1	5000	1 year	Own	30 min	0
1	004	4	Child	Female	12	Single	Primary School	Student		4000	Farmer	Own	1	4000	1 year	Own	30 min	0
1	005	5	Child	Male	10	Single	Primary School	Student		3000	Farmer	Own	1	3000	1 year	Own	30 min	0
1	006	6	Child	Female	8	Single	Primary School	Student		2000	Farmer	Own	1	2000	1 year	Own	30 min	0
1	007	7	Spouse	Male	45	Married	Primary School	Self-employed		18000	Farmer	Own	1	18000	1 year	Own	30 min	0
1	008	8	Spouse	Female	42	Married	Primary School	Self-employed		16000	Farmer	Own	1	16000	1 year	Own	30 min	0
1	009	9	Child	Male	18	Single	Primary School	Student		6000	Farmer	Own	1	6000	1 year	Own	30 min	0
1	010	10	Child	Female	16	Single	Primary School	Student		5000	Farmer	Own	1	5000	1 year	Own	30 min	0
1	011	11	Child	Male	14	Single	Primary School	Student		4000	Farmer	Own	1	4000	1 year	Own	30 min	0
1	012	12	Child	Female	11	Single	Primary School	Student		3000	Farmer	Own	1	3000	1 year	Own	30 min	0
1	013	13	Child	Male	9	Single	Primary School	Student		2000	Farmer	Own	1	2000	1 year	Own	30 min	0
1	014	14	Child	Female	7	Single	Primary School	Student		1500	Farmer	Own	1	1500	1 year	Own	30 min	0
1	015	15	Child	Male	5	Single	Primary School	Student		1000	Farmer	Own	1	1000	1 year	Own	30 min	0
1	016	16	Child	Female	3	Single	Primary School	Student		500	Farmer	Own	1	500	1 year	Own	30 min	0
1	017	17	Child	Male	1	Single	Primary School	Student		200	Farmer	Own	1	200	1 year	Own	30 min	0
1	018	18	Child	Female	0	Single	Primary School	Student		0	Farmer	Own	1	0	1 year	Own	30 min	0
1	019	19	Child	Male	2	Single	Primary School	Student		300	Farmer	Own	1	300	1 year	Own	30 min	0
1	020	20	Child	Female	4	Single	Primary School	Student		600	Farmer	Own	1	600	1 year	Own	30 min	0
1	021	21	Child	Male	6	Single	Primary School	Student		1000	Farmer	Own	1	1000	1 year	Own	30 min	0
1	022	22	Child	Female	8	Single	Primary School	Student		1500	Farmer	Own	1	1500	1 year	Own	30 min	0
1	023	23	Child	Male	10	Single	Primary School	Student		2000	Farmer	Own	1	2000	1 year	Own	30 min	0
1	024	24	Child	Female	12	Single	Primary School	Student		2500	Farmer	Own	1	2500	1 year	Own	30 min	0
1	025	25	Child	Male	14	Single	Primary School	Student		3000	Farmer	Own	1	3000	1 year	Own	30 min	0
1	026	26	Child	Female	16	Single	Primary School	Student		3500	Farmer	Own	1	3500	1 year	Own	30 min	0
1	027	27	Child	Male	18	Single	Primary School	Student		4000	Farmer	Own	1	4000	1 year	Own	30 min	0
1	028	28	Child	Female	20	Single	Primary School	Student		4500	Farmer	Own	1	4500	1 year	Own	30 min	0
1	029	29	Child	Male	22	Single	Primary School	Student		5000	Farmer	Own	1	5000	1 year	Own	30 min	0
1	030	30	Child	Female	24	Single	Primary School	Student		5500	Farmer	Own	1	5500	1 year	Own	30 min	0
1	031	31	Child	Male	26	Single	Primary School	Student		6000	Farmer	Own	1	6000	1 year	Own	30 min	0
1	032	32	Child	Female	28	Single	Primary School	Student		6500	Farmer	Own	1	6500	1 year	Own	30 min	0
1	033	33	Child	Male	30	Single	Primary School	Student		7000	Farmer	Own	1	7000	1 year	Own	30 min	0
1	034	34	Child	Female	32	Single	Primary School	Student		7500	Farmer	Own	1	7500	1 year	Own	30 min	0
1	035	35	Child	Male	34	Single	Primary School	Student		8000	Farmer	Own	1	8000	1 year	Own	30 min	0
1	036	36	Child	Female	36	Single	Primary School	Student		8500	Farmer	Own	1	8500	1 year	Own	30 min	0
1	037	37	Child	Male	38	Single	Primary School	Student		9000	Farmer	Own	1	9000	1 year	Own	30 min	0
1	038	38	Child	Female	40	Single	Primary School	Student		9500	Farmer	Own	1	9500	1 year	Own	30 min	0
1	039	39	Child	Male	42	Single	Primary School	Student		10000	Farmer	Own	1	10000	1 year	Own	30 min	0
1	040	40	Child	Female	44	Single	Primary School	Student		10500	Farmer	Own	1	10500	1 year	Own	30 min	0

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Data Bank related to Sections 01 and 02 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of Corresponding Property Survey	Code of this Socio-Economic Unit	Family Head	Other	Commercial Activity Type	Type of Occupancy	Property Type	Other
VAL/DART/IL/GRN/001	The interview was not done						
VAL/DART/IL/GRN/001	SOC/DART/IL/GRN/001	Male		Vehicle related services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/002	SOC/DART/IL/GRN/002	Male		Vehicle related services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/002	The interview was not done					Not interviewed	
VAL/DART/IL/GRN/002	SOC/DART/IL/GRN/002A	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/003	SOC/DART/IL/GRN/003	Male		Vehicle related services	Tenant	Stand-Alone Kiosk	
VAL/DART/IL/GRN/003	SOC/DART/IL/GRN/003A	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/003	SOC/DART/IL/GRN/003B	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/003	SOC/DART/IL/GRN/003C	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/004	SOC/DART/IL/GRN/004	Female		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/004	The interview was not done						
VAL/DART/IL/GRN/004	SOC/DART/IL/GRN/004A	Male		Other retail or services	Tenant	No answer	
VAL/DART/IL/GRN/005	SOC/DART/IL/GRN/005	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/005	The interview was not done				Tenant	Not interviewed	
VAL/DART/IL/GRN/005	SOC/DART/IL/GRN/005A	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/006	SOC/DART/IL/GRN/006	Female		Restaurant/ bar/ food&leisure/ hotel	Property Owner	Kiosk attached to building	
VAL/DART/IL/GRN/006	SOC/DART/IL/GRN/006A	Male		Vehicle related services	Tenant	Stand-Alone Kiosk	
VAL/DART/IL/GRN/006	SOC/DART/IL/GRN/006B	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/006	SOC/DART/IL/GRN/006C	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/008	SOC/DART/IL/GRN/008	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/008	SOC/DART/IL/GRN/008A	Female		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/008	SOC/DART/IL/GRN/008B	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/008	The interview was not done				Tenant	Not interviewed	
VAL/DART/IL/GRN/008	SOC/DART/IL/GRN/008C	Male		Other retail or services	Tenant	Kiosk attached to building	

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Data Bank related to Sections 01 and 02 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of Corresponding Property Survey	Code of the Socio-Economic Unit	Family Head	Other:	Commercial Activity Type	Type of occupancy	Property Type	Other:
VAL/DART/IL/GRN/009	SOC/DART/IL/GRN/009	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/009	SOC/DART/IL/GRN/009B	Male		Other retail or services	Tenant	No answer	
VAL/DART/IL/GRN/009	SOC/DART/IL/GRN/009C	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/009	SOC/DART/IL/GRN/009E	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/010	SOC/DART/IL/GRN/010	Female		Restaurant/ bar/ food&leisure/ hotel	Property Owner	Kiosk attached to building	
VAL/DART/IL/GRN/011	SOC/DART/IL/GRN/011	Female		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/011	SOC/DART/IL/GRN/011A	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/011	SOC/DART/IL/GRN/011B	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/012	SOC/DART/IL/GRN/012	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/012	SOC/DART/IL/GRN/012A	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/012	SOC/DART/IL/GRN/012B	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/013	SOC/DART/IL/GRN/013	Male		Other	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/013	SOC/DART/IL/GRN/013A	Male		Other	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/013	SOC/DART/IL/GRN/013B	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/013	The interviewee is not interviewed			Residential	Tenant	Not interviewed	
VAL/DART/IL/GRN/013	SOC/DART/IL/GRN/013C	Female		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/015	SOC/DART/IL/GRN/015	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/015	The interviewee is not interviewed			Residential	Tenant	Not interviewed	
VAL/DART/IL/GRN/015	SOC/DART/IL/GRN/015A	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/016	SOC/DART/IL/GRN/016	Female		Other retail or services	Tenant	No answer	
VAL/DART/IL/GRN/016	SOC/DART/IL/GRN/016A	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/016	SOC/DART/IL/GRN/016B	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/016	SOC/DART/IL/GRN/016C	Female		Restaurant/ bar/ food&leisure/ hotel	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/017	SOC/DART/IL/GRN/017	Male		Other retail or services	Tenant	Kiosk attached to building	

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Data Bank related to Sections 01 and 02 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of Corresponding Property Survey	Code of the Socio-Economic Unit	Family Head	Other:	Commercial Activity Type:	Type of occupancy	Property Type	Other:
VAL/DART/IL/GRN/017	SOC/DART/IL/GRN/017A	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/017	SOC/DART/IL/GRN/017B	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/017	SOC/DART/IL/GRN/017C	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/018	The interview was not done				Tenant	Not interviewed	
VAL/DART/IL/GRN/018	SOC/DART/IL/GRN/018	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/020	SOC/DART/IL/GRN/020	Female		Other retail or services	Property Owner	Kiosk attached to building	
VAL/DART/IL/GRN/022	SOC/DART/IL/GRN/022	Female		Other retail or services	Property Owner	Stand-Alone Kiosk	
VAL/DART/IL/GRN/025	SOC/DART/IL/GRN/025	Female		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/IL/GRN/027	SOC/DART/IL/GRN/027	Male		Other	Property Owner	Stand-Alone Kiosk	
VAL/DART/IL/GRN/031	SOC/DART/IL/GRN/031	Female		Other retail or services	Other	Kiosk attached to building	
VAL/DART/IL/GRN/031	SOC/DART/IL/GRN/031A	Female		Other retail or services	Other	Kiosk attached to building	
VAL/DART/IL/GRN/034	SOC/DART/IL/GRN/034	Female		Other retail or services	Property Owner	Kiosk attached to building	
VAL/DART/IL/GRN/036	SOC/DART/IL/GRN/036	Female		Other retail or services	Property Owner	Kiosk attached to building	
VAL/DART/IL/GRN/038	SOC/DART/IL/GRN/038	Female		Restaurant/ bar/ food&leisure/ hotel	Property Owner	Room in residential building	
VAL/DART/IL/GRN/047	SOC/DART/IL/GRN/047	Female		Vehicle related services	Property Owner	Independent Commercial Building	
VAL/DART/IL/GRN/063	SOC/DART/IL/GRN/063	Female		Other retail or services	Property Owner	Stand-Alone Kiosk	
VAL/DART/IL/GRN/068	SOC/DART/IL/GRN/068	Male		Other retail or services	Property Owner	Kiosk attached to building	
VAL/DART/IL/GRN/069	SOC/DART/IL/GRN/069	Female		Other retail or services	Property Owner	Kiosk attached to building	
VAL/DART/IL/GRN/074	SOC/DART/IL/GRN/074	Female		Other retail or services	Property Owner	Kiosk attached to building	
VAL/DART/IL/GRN/075	SOC/DART/IL/GRN/075	Male		Other retail or services	Property Owner	Kiosk attached to building	
VAL/DART/IL/GRN/077	SOC/DART/IL/GRN/077	Female		Other retail or services	Property Owner	Kiosk attached to building	
VAL/DART/IL/GRN/078	SOC/DART/IL/GRN/078	Male		Restaurant/ bar/ food&leisure/ hotel	Property Owner	Kiosk attached to building	
VAL/DART/IL/GRN/081	SOC/DART/IL/GRN/081	Male		Other retail or services	Property Owner	Kiosk attached to building	
VAL/DART/IL/GRN/085	SOC/DART/IL/GRN/085	Female		Other retail or services	Property Owner	Stand-Alone Kiosk	

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Code of Corresponding Property Survey	Code of the Socio-Economic Unit	Family Head	Other:	Commercial Activity Type	Type of occupancy	Property Type	Other
VAL/DART/IL/GRN/086	SOC/DART/IL/GRN/086	Male		Other retail or services	Property Owner	Room in residential building	
VAL/DART/IL/GRN/093	SOC/DART/IL/GRN/093	Female		Other retail or services	Property Owner	Stand-Alone Kiosk	
VAL/DART/IL/GRN/097	SOC/DART/IL/GRN/097	Female		Other retail or services	Other	Kiosk attached to building	
VAL/DART/IL/GRN/098	SOC/DART/IL/GRN/098	Male		Other retail or services	Property Owner	Stand-Alone Kiosk	
VAL/DART/IL/GRN/100	SOC/DART/IL/GRN/100	Male		Other retail or services	Property Owner	Kiosk attached to building	
VAL/DART/IL/GRN/103	SOC/DART/IL/GRN/103	Male		Other retail or services	Property Owner	Kiosk attached to building	
VAL/DART/KN/HNF/001	The interview was not done			Restaurant/ bar/ food&leisure/ hotel	Property Owner	Not interviewed	
VAL/DART/KN/HNF/002	The interview was not done			Other	Property Owner	Not interviewed	
VAL/DART/KN/HNF/003	SOC/DART/KN/HNF/003				Property Owner	No answer	
VAL/DART/KN/HNF/004	The interview was not done				Property Owner	Not interviewed	
VAL/DART/KN/HNF/006	The interview was not done			Other retail or services	Tenant	Not interviewed	
VAL/DART/KN/HNF/006	The interview was not done			Other retail or services	Tenant	Not interviewed	
VAL/DART/KN/HNF/007	SOC/DART/KN/HNF/007	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/KN/HNF/007	SOC/DART/KN/HNF/007A	Female		Industrial	Tenant	Stand-Alone Kiosk	
VAL/DART/KN/HNF/007	The interview was not done			Residential	Tenant	Not interviewed	
VAL/DART/KN/HNF/007	SOC/DART/KN/HNF/007B	Male		Restaurant/ bar/ food&leisure/ hotel	Tenant	Stand-Alone Kiosk	
VAL/DART/KN/HNF/008	SOC/DART/KN/HNF/008	Female		Restaurant/ bar/ food&leisure/ hotel	Property Owner	Kiosk attached to building	
VAL/DART/KN/HNF/009	SOC/DART/KN/HNF/009	Male		Restaurant/ bar/ food&leisure/ hotel	Property Owner	Kiosk attached to building	
VAL/DART/KN/HNF/010	SOC/DART/KN/HNF/010A	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/KN/HNF/010	SOC/DART/KN/HNF/010B	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/KN/HNF/010	SOC/DART/KN/HNF/010C	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/KN/HNF/010	SOC/DART/KN/HNF/010D	Female		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/KN/HNF/011	SOC/DART/KN/HNF/011	Female		Restaurant/ bar/ food&leisure/ hotel	Tenant	Kiosk attached to building	
VAL/DART/KN/HNF/011	SOC/DART/KN/HNF/011A	Female		Other	Tenant	Kiosk attached to building	

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Data Bank related to Sections 01 and 02 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of Corresponding Property Survey	Code of this Socio-Economic Unit	Family Head	Other	Commercial Activity Type	Type of occupancy	Property Type	Other
VAL/DART/KN/HNF/011	SOC/DART/KN/HNF/011B	Male		Other	Tenant	Kiosk attached to building	
VAL/DART/KN/HNF/011	SOC/DART/KN/HNF/011C	Male		Other	Tenant	Kiosk attached to building	
VAL/DART/KN/HNF/011	SOC/DART/KN/HNF/011D	Male		Other	Tenant	Stand-Alone Kiosk	
VAL/DART/KN/HNF/011	SOC/DART/KN/HNF/011E	Male		Other	Tenant	Kiosk attached to building	
VAL/DART/KN/HNF/011	SOC/DART/KN/HNF/011F	Male		Other	Tenant	Kiosk attached to building	
VAL/DART/KN/HNF/012	SOC/DART/KN/HNF/012	Male		Other retail or services	Tenant	Room in residential building	
VAL/DART/KN/HNF/012	SOC/DART/KN/HNF/012A	Female		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/KN/KMR/003	SOC/DART/KN/KMR/003	Male		Public/ Institutional buildings	Tenant	No answer	
VAL/DART/KN/KMR/004	SOC/DART/KN/KMR/004	Male		Vehicle related services	Property Owner	No answer	
VAL/DART/KN/KMR/004	SOC/DART/KN/KMR/004A	Male		Vehicle related services	Tenant	No answer	
VAL/DART/KN/KMR/005	SOC/DART/KN/KMR/005	Male		Industrial	Tenant	No answer	
VAL/DART/KN/KMR/005	SOC/DART/KN/KMR/005A	Male		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/005	The interview was not done				Tenant	Not interviewed	
VAL/DART/KN/KMR/005	The interview was not done				Tenant	Not interviewed	
VAL/DART/KN/KMR/005	SOC/DART/KN/KMR/005B	Female		Restaurant/ bar/ food&leisure/ hotel	Tenant	No answer	
VAL/DART/KN/KMR/006	SOC/DART/KN/KMR/006A	Female		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/006	SOC/DART/KN/KMR/006B	Female		Restaurant/ bar/ food&leisure/ hotel	Tenant	No answer	
VAL/DART/KN/KMR/007	SOC/DART/KN/KMR/007A	Male		Other retail or services	Property Owner	No answer	
VAL/DART/KN/KMR/007	SOC/DART/KN/KMR/007C	Female		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/007	SOC/DART/KN/KMR/007D	Female		Restaurant/ bar/ food&leisure/ hotel	Tenant	No answer	
VAL/DART/KN/KMR/008	SOC/DART/KN/KMR/008	Male		Vehicle related services	Tenant	No answer	
VAL/DART/KN/KMR/008	SOC/DART/KN/KMR/008A	Male		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/009	SOC/DART/KN/KMR/009A	Male		Other	Tenant	No answer	
VAL/DART/KN/KMR/009	SOC/DART/KN/KMR/009B	Female		Other	Tenant	No answer	

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Code of Corresponding Property Survey	Code of the Socio-Economic Unit	Family Head	Other	Commercial Activity Type	Type of occupancy	Property Type	Other
VAL/DART/KN/KMR/009	SOC/DART/KN/KMR/009C	Male		Other	Tenant	No answer	
VAL/DART/KN/KMR/009	SOC/DART/KN/KMR/009D	Female		Restaurant/ bar/ food&leisure/ hotel	Tenant	No answer	
VAL/DART/KN/KMR/009	SOC/DART/KN/KMR/009E	Female		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/009	SOC/DART/KN/KMR/009F	Female		Restaurant/ bar/ food&leisure/ hotel	Tenant	No answer	
VAL/DART/KN/KMR/009	SOC/DART/KN/KMR/009G	Male		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/011	SOC/DART/KN/KMR/011A	Female		Restaurant/ bar/ food&leisure/ hotel	Tenant	No answer	
VAL/DART/KN/KMR/012	SOC/DART/KN/KMR/012	Female		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/012	SOC/DART/KN/KMR/012A	Male		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/012	The interview was not done					Not interviewed	
VAL/DART/KN/KMR/012	The interview was not done					Not interviewed	
VAL/DART/KN/KMR/012	SOC/DART/KN/KMR/012B	Female		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/013	SOC/DART/KN/KMR/013	Male		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/013	SOC/DART/KN/KMR/013A	Male		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/013	SOC/DART/KN/KMR/013B	Male		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/013	SOC/DART/KN/KMR/013C	Male		Vehicle related services	Tenant	No answer	
VAL/DART/KN/KMR/013	SOC/DART/KN/KMR/013D	Female		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/014	SOC/DART/KN/KMR/014	Male		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/014	SOC/DART/KN/KMR/014A	Male		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/014	SOC/DART/KN/KMR/014B	Male		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/014	SOC/DART/KN/KMR/014C	Male		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/014	SOC/DART/KN/KMR/014D	Male		Restaurant/ bar/ food&leisure/ hotel	Tenant	No answer	
VAL/DART/KN/KMR/014	SOC/DART/KN/KMR/014E	Female		Other	Property Owner	No answer	
VAL/DART/KN/KMR/015	SOC/DART/KN/KMR/015	Male		Restaurant/ bar/ food&leisure/ hotel	Property Owner	No answer	
VAL/DART/KN/KMR/015	SOC/DART/KN/KMR/015A	Male		Other retail or services	Tenant	No answer	

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Data Bank related to Sections 01 and 02 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of Corresponding Property Survey	Code of this Socio-Economic Unit	Family Head	Other	Commercial Activity Type	Type of occupancy	Property Type	Other
VAL/DART/KN/KMR/017	SOC/DART/KN/KMR/017	Male		Industrial	Property Owner	No answer	
VAL/DART/KN/KMR/017	SOC/DART/KN/KMR/017B	Male		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/017	SOC/DART/KN/KMR/017C	Male		Restaurant/ bar/ food&leisure/ hotel	Tenant	No answer	
VAL/DART/KN/KMR/020	SOC/DART/KN/KMR/020	Male		Restaurant/ bar/ food&leisure/ hotel	Tenant	No answer	
VAL/DART/KN/KMR/020	SOC/DART/KN/KMR/020A	Male		Restaurant/ bar/ food&leisure/ hotel	Tenant	No answer	
VAL/DART/KN/UB2/021	The interview was not done					Not interviewed	
VAL/DART/KN/KMR/023	SOC/DART/KN/KMR/023	Female		Other	Property Owner	No answer	
VAL/DART/KN/KMR/025	SOC/DART/KN/KMR/025B	Female		Other retail or services	Property Owner	No answer	
VAL/DART/KN/KMR/026	SOC/DART/KN/KMR/026	Male		Restaurant/ bar/ food&leisure/ hotel	Tenant	No answer	
VAL/DART/KN/KMR/026	SOC/DART/KN/KMR/026A	Male		Vehicle related services	Tenant	No answer	
VAL/DART/KN/KMR/026	SOC/DART/KN/KMR/026B	Male		Other retail or services	Property Owner	No answer	
VAL/DART/KN/KMR/027	SOC/DART/KN/KMR/027	Male		Other retail or services	Property Owner	No answer	
VAL/DART/KN/KMR/028	SOC/DART/KN/KMR/028	Male		Other	Property Owner	No answer	
VAL/DART/KN/KMR/029	SOC/DART/KN/KMR/029	Female		Other retail or services	Property Owner	No answer	
VAL/DART/KN/KMR/029	SOC/DART/KN/KMR/029A	Female		Restaurant/ bar/ food&leisure/ hotel	Tenant	No answer	
VAL/DART/KN/KMR/029	SOC/DART/KN/KMR/029B	Female		Other retail or services	Tenant	No answer	
VAL/DART/KN/KMR/031	SOC/DART/KN/KMR/031	Female		Restaurant/ bar/ food&leisure/ hotel	Property Owner	No answer	
VAL/DART/KN/KMR/032	The interview was not done					Not interviewed	
VAL/DART/KN/KMR/032	The interview was not done					Not interviewed	
VAL/DART/KN/KND/002	SOC/DART/KN/KND/002A	Female		Other retail or services	Property Owner	Kiosk attached to building	
VAL/DART/KN/KND/004	SOC/DART/KN/KND/004			Other	Property Owner	No answer	
VAL/DART/KN/MG2/001	SOC/DART/KN/MG2/001	Female		Industrial	Tenant	No answer	
VAL/DART/KN/MG2/002	SOC/DART/KN/MG2/002	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/KN/MG2/002	SOC/DART/KN/MG2/002A	Male			Tenant	No answer	

Annex 03 - Part 3
Data Bank related to Sections 01 and 02 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of Corresponding Property Survey	Code of this Socio-Economic Unit	Family Head	Other	Commercial Activity Type	Type of occupancy	Property Type	Other:
VAL/DART/KN/MG2/002	SOC/DART/KN/MG2/002B	Male			Tenant	No answer	
VAL/DART/KN/MG2/002	SOC/DART/KN/MG2/002C	Male			Tenant	No answer	
VAL/DART/KN/MG2/002	SOC/DART/KN/MG2/002D	Female		Restaurant/ bar/ food&leisure/ hotel	Tenant	Stand-Alone Kiosk	
VAL/DART/KN/MG2/002	The interview was not done				Tenant	Not interviewed	
VAL/DART/KN/MG2/002	The interview was not done				Tenant	Not interviewed	
VAL/DART/KN/MG2/002	SOC/DART/KN/MG2/002E	Male			Tenant	No answer	
VAL/DART/KN/MG2/003	SOC/DART/KN/MG2/003	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/KN/MG2/003	SOC/DART/KN/MG2/003A	Female		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/KN/MG2/003	SOC/DART/KN/MG2/003B	Female		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/KN/MG2/003	SOC/DART/KN/MG2/003C	Male		Restaurant/ bar/ food&leisure/ hotel	Tenant	Stand-Alone Kiosk	
VAL/DART/KN/MG2/004	SOC/DART/KN/MG2/004A	Male		Restaurant/ bar/ food&leisure/ hotel	Tenant	Stand-Alone Kiosk	
VAL/DART/KN/MG2/005	SOC/DART/KN/MG2/005	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/KN/MG2/005	SOC/DART/KN/MG2/005A	Female		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/KN/MG2/005	SOC/DART/KN/MG2/005C	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/KN/MG2/005	The interview was not done						
VAL/DART/KN/MG2/005	SOC/DART/KN/MG2/005D	Male		Other retail or services	Tenant	Kiosk attached to building	
VAL/DART/KN/MG2/006	SOC/DART/KN/MG2/006A	Male			Property Owner	No answer	
VAL/DART/KN/MNY/001	SOC/DART/KN/MNY/001	Male		Petrol stations	Property Owner	No answer	
VAL/DART/KN/FR/001	The interview was not done				Tenant	Not interviewed	
VAL/DART/KN/UB2/001	SOC/DART/KN/UB2/001	Male		Other retail or services	Tenant	No answer	
VAL/DART/KN/UB/002	The interview was not done				Property Owner	Not interviewed	
VAL/DART/KN/UB/003	The interview was not done					Not interviewed	
VAL/DART/KN/UB2/003	SOC/DART/KN/UB2/003	Female		Other retail or services	Tenant	No answer	
VAL/DART/KN/UB2/003	SOC/DART/KN/UB2/003A	Female		Other retail or services	Property Owner	No answer	

Annex 03 - Part 3
Data Bank related to Sections 01 and 02 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of Corresponding Property Survey	Code of this Socio-Economic Unit	Family Head	Other:	Commercial Activity Type	Type of occupancy	Property Type	Other:
VAL/DART/KN/UB2/004	SOC/DART/KN/UB2/004	Male		Other retail or services	Tenant	No answer	
VAL/DART/KN/UB2/005	SOC/DART/KN/UB2/005	Female		Other retail or services	Property Owner	No answer	
VAL/DART/KN/UB2/006	SOC/DART/KN/UB2/006	Male		Other retail or services	Tenant	No answer	
VAL/DART/KN/UB2/007	SOC/DART/KN/UB2/007B	Female		Other retail or services	Property Owner	No answer	
VAL/DART/KN/UB2/008	SOC/DART/KN/UB2/008A	Male		Public/ Institutional buildings	Property Owner	No answer	
VAL/DART/KN/UB2/009	SOC/DART/KN/UB2/009	Female		Public/ Institutional buildings	Property Owner	No answer	
VAL/DART/KN/UB2/010	SOC/DART/KN/UB2/010	Female		Other retail or services	Property Owner	No answer	
VAL/DART/KN/UB2/011	SOC/DART/KN/UB2/011	Male		Other retail or services	Property Owner	No answer	
VAL/DART/KN/UB2/012	SOC/DART/KN/UB2/012A	Male		Restaurant/ bar/ food&leisure/ hotel	Property Owner	No answer	
VAL/DART/KN/UB2/013	SOC/DART/KN/UB2/013	Male		Other retail or services	Property Owner	No answer	
VAL/DART/KN/UB2/014	The interview was not done			Other retail or services	Tenant	Not interviewed	
VAL/DART/KN/UB2/014	SOC/DART/KN/UB2/014	Male		Other retail or services	Property Owner	No answer	
VAL/DART/KN/UB2/015	The interview was not done			Other retail or services	Property Owner	Not interviewed	
VAL/DART/KN/UB2/016	SOC/DART/KN/UB2/016	Male		Other retail or services	Property Owner	No answer	
VAL/DART/KN/UB2/017	SOC/DART/KN/UB2/017	Female		Other retail or services	Property Owner	No answer	
VAL/DART/KN/UB2/021	SOC/DART/KN/UB2/021	Male		Other	Property Owner	Stand-Alone Kiosk	
VAL/DART/KN/UB2/022	The interview was not done			Petrol stations	Property Owner	Not interviewed	
VAL/DART/KN/UB2/022	The interview was not done			Other retail or services	Property Owner	Not interviewed	
VAL/DART/KN/UB2/027	The interview was not done			Residential	Property Owner	Not interviewed	
VAL/DART/KN/UB2/027	The interview was not done			Residential	Tenant	Not interviewed	

Annex 03 - Part 4

Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of this Socio-Economic Unit	N° of Employee	Relation to Owner	Lives within 300m of corridor?	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
SOC/DART/IL/GRN/001	Owner	not applicable		not applicable	Full Time	1	daladala
SOC/DART/IL/GRN/001	Manager	not applicable		not applicable	Full Time	1	daladala
SOC/DART/IL/GRN/001	Other	no relation		Other	Full Time	1	daladala
SOC/DART/IL/GRN/002	Owner	not applicable		not applicable	Full Time	0	daladala
SOC/DART/IL/GRN/002A	Owner	not applicable		not applicable	Full Time	0	daladala
SOC/DART/IL/GRN/002A	Manager	not applicable		not applicable	Full Time	0	daladala
SOC/DART/IL/GRN/002A	Other	no relation		Other	Full Time	0	daladala
SOC/DART/IL/GRN/002A	Other	no relation		Other	Full Time	0	daladala
SOC/DART/IL/GRN/003	Owner	not applicable		not applicable	Full Time	0	daladala
SOC/DART/IL/GRN/003	Manager	no relation		Registered employee	Full Time	0	daladala
SOC/DART/IL/GRN/003	Other	no relation		Registered employee	Full Time	0	daladala
SOC/DART/IL/GRN/003	Other	no relation		Registered employee	Full Time	0	daladala
SOC/DART/IL/GRN/003A	Owner	not applicable		not applicable	Full Time	0	daladala
SOC/DART/IL/GRN/003A	Other	other relative		Registered employee	Full Time	0	daladala
SOC/DART/IL/GRN/003A	Other	no relation		Registered employee	Full Time	0	daladala
SOC/DART/IL/GRN/003A	Other	other relative		Registered employee	Full Time	0	daladala
SOC/DART/IL/GRN/003A	Other	other relative		Registered employee	Full Time	0	daladala
SOC/DART/IL/GRN/003B	Owner	not applicable		not applicable	Full Time	0	daladala
SOC/DART/IL/GRN/003B	Manager	other relative		Registered employee	Full Time	0	daladala
SOC/DART/IL/GRN/003C	Owner	not applicable		not applicable	Full Time	0	daladala
SOC/DART/IL/GRN/004	Owner	not applicable		not applicable	Full Time	2	daladala
SOC/DART/IL/GRN/004A	Owner	not applicable		not applicable	Full Time	0	daladala
SOC/DART/IL/GRN/005	Owner	not applicable		not applicable	Full Time	1	daladala

Annex 03 - Part 4

Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of this Socio-Economic Unit	N° of Employee	Relation to Owner	Lives within 300m of corridor?	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
SOC/DART/IL/GRN/005	Manager	other relative		Informal	Full Time	1	daladala
SOC/DART/IL/GRN/005A	Owner	not applicable		not applicable	Full Time	0	daladala
SOC/DART/IL/GRN/005A	Manager	no relation		Informal	Full Time	0	daladala
SOC/DART/IL/GRN/006	Owner	not applicable		not applicable	Full Time	0	daladala
SOC/DART/IL/GRN/006	Other	no relation		Informal	Full Time	4	daladala
SOC/DART/IL/GRN/006	Other	no relation		Informal	Full Time	1	daladala
SOC/DART/IL/GRN/006A	Other	other relative		Informal	Full Time	2	daladala
SOC/DART/IL/GRN/006A	Other	other relative		Informal	Full Time	2	daladala
SOC/DART/IL/GRN/006A	Owner	not applicable		not applicable	Full Time	2	daladala
SOC/DART/IL/GRN/006A	Manager	other relative		Informal	Full Time	2	car-motorbike
SOC/DART/IL/GRN/006A	Other	other relative		Informal	Full Time	2	daladala
SOC/DART/IL/GRN/006B	Owner	not applicable		not applicable	Full Time	0	daladala
SOC/DART/IL/GRN/006B	Manager	other relative		Informal	Full Time	0	daladala
SOC/DART/IL/GRN/006C	Owner	not applicable		not applicable	Full Time	8	daladala
SOC/DART/IL/GRN/006C	Manager	no relation		Informal	Full Time	8	daladala
SOC/DART/IL/GRN/008A	Owner	not applicable		not applicable	Full Time	3	daladala
SOC/DART/IL/GRN/008B	Owner	not applicable		not applicable	Full Time	5	daladala
SOC/DART/IL/GRN/008B	Other	other relative		Informal	Full Time	5	daladala
SOC/DART/IL/GRN/008B	Other	no relation		Informal	Full Time	5	daladala
SOC/DART/IL/GRN/008B	Other	no relation		Informal	Full Time	5	daladala
SOC/DART/IL/GRN/008C	Owner	not applicable		not applicable	Full Time	1	daladala
SOC/DART/IL/GRN/008C	Other	other relative		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/009	Owner	not applicable		not applicable	Full Time	2	car-motorbike

Legend
 Yes
 No/Not Informed

Annex 03 - Part 4

Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of this Socio-Economic Unit	N° of Employee	Relation to Owner	Lives within 300m of corridor?	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
SOC/DART/IL/GRN/009	Manager	other relative		Registered employee	Full Time	2	daladala
SOC/DART/IL/GRN/009	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/009	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/009	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/IL/GRN/009B	Owner	not applicable		not applicable	Full Time	1	daladala
SOC/DART/IL/GRN/009B	Manager	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/009C	Owner	not applicable		not applicable	Full Time	1	daladala
SOC/DART/IL/GRN/009C	Manager	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/009C	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/009E	Owner	not applicable		not applicable	Full Time	3	car-motorbike
SOC/DART/IL/GRN/009E	Other	no relation		Registered employee	Full Time	1	walking
SOC/DART/IL/GRN/009E	Other	no relation		Registered employee	Full Time	1	walking
SOC/DART/IL/GRN/009E	Other	no relation		Registered employee	Full Time	1	walking
SOC/DART/IL/GRN/010	Other	no relation		Registered employee	Full Time	6	daladala
SOC/DART/IL/GRN/010	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/IL/GRN/010	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/010	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/IL/GRN/010	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/010	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/010	Other	no relation		Registered employee	Full Time	11	daladala
SOC/DART/IL/GRN/011	Owner	not applicable		not applicable	Full Time	0	daladala
SOC/DART/IL/GRN/011	Other	other relative		Informal	Full Time	0	daladala

Annex 03 - Part 4

Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of this Socio-Economic Unit	N° of Employee	Relation to Owner	Lives within 300m of corridor?	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
SOC/DART/IL/GRN/011A	Owner	not applicable		not applicable	Full Time	0	daladala
SOC/DART/IL/GRN/011A	Other	no relation		Other	Full Time	0	daladala
SOC/DART/IL/GRN/011A	Other	no relation		Other	Full Time	0	daladala
SOC/DART/IL/GRN/011A	Other	no relation		Other	Full Time	0	daladala
SOC/DART/IL/GRN/011B	Owner	not applicable		not applicable	Full Time	1	daladala
SOC/DART/IL/GRN/011B	Other	no relation		Informal	Full Time	1	daladala
SOC/DART/IL/GRN/012	Owner	not applicable		not applicable	Full Time	1	daladala
SOC/DART/IL/GRN/012	Manager	other relative		Other	Full Time	1	daladala
SOC/DART/IL/GRN/012A	Owner	not applicable		not applicable	Full Time	1	car-motorbike
SOC/DART/IL/GRN/012A	Other	no relation		Registered employee	Full Time	1	car-motorbike
SOC/DART/IL/GRN/012B	Owner	not applicable		not applicable	Partial Time	0	daladala
SOC/DART/IL/GRN/012B	Manager	no relation		Other	Full Time	0	daladala
SOC/DART/IL/GRN/013	Owner	not applicable		not applicable	Full Time	2	daladala
SOC/DART/IL/GRN/013	Manager	other relative		Other	Partial Time	2	daladala
SOC/DART/IL/GRN/013A	Owner	not applicable		not applicable	Full Time	2	daladala
SOC/DART/IL/GRN/013A	Manager	other relative		Other	Full Time	2	daladala
SOC/DART/IL/GRN/013B	Owner	not applicable		not applicable	Full Time	1	daladala
SOC/DART/IL/GRN/013B	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/013C	Owner	not applicable		not applicable	Full Time		daladala
SOC/DART/IL/GRN/014	Owner	not applicable		not applicable	Full Time	1	daladala
SOC/DART/IL/GRN/015	Owner	not applicable		not applicable	Full Time	1	daladala
SOC/DART/IL/GRN/016	Owner	not applicable		not applicable	Partial Time	0	daladala
SOC/DART/IL/GRN/016A	Owner	not applicable		not applicable	Partial Time	4	daladala

Legend
 Yes
 Not Informed

Annex 03 - Part 4

Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of this Socio-Economic Unit	N° of Employee	Relation to Owner	Lives within 300m of corridor?	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
SOC/DART/IL/GRN/016A	Manager	not applicable			Partial Time	4	daladala
SOC/DART/IL/GRN/016A	Other	son/daughter		Informal	Full Time	4	daladala
SOC/DART/IL/GRN/016B	Owner	not applicable			Full Time	1	daladala
SOC/DART/IL/GRN/016B	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/016B	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/016B	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/016B	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/016C	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/IL/GRN/016C	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/IL/GRN/016C	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/IL/GRN/016C	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/016C	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/IL/GRN/016C	Other	no relation		Registered employee	Full Time	5	daladala
SOC/DART/IL/GRN/016C	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/016C	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/IL/GRN/016C	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/016C	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/016C	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/016C	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/017	Owner	not applicable		not applicable	Full Time	2	daladala
SOC/DART/IL/GRN/017	Manager	other relative		Informal	Full Time	2	daladala
SOC/DART/IL/GRN/017A	Owner	not applicable		not applicable	Full Time	1	daladala

Legend
 Yes
 Not informed

Annex 03 - Part 4

Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of this Socio-Economic Unit	N° of Employee	Relation to Owner	Lives within 300m of corridor?	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
SOC/DART/IL/GRN/017A	Other	aggregates		Informal	Full Time	1	daladala
SOC/DART/IL/GRN/017B	Owner	not applicable		not applicable	Full Time	0	daladala
SOC/DART/IL/GRN/017C	Owner	not applicable		not applicable	Full Time	2	daladala
SOC/DART/IL/GRN/017C	Manager	other relative		Registered employee	Full Time	2	daladala
SOC/DART/IL/GRN/018	Owner	not applicable		not applicable	Full Time	1	daladala
SOC/DART/IL/GRN/020	Other	no relation		Not informed	Partial Time	0	none
SOC/DART/IL/GRN/020	Other	no relation		Not informed	Full Time	0	daladala
SOC/DART/IL/GRN/022	Other	no relation		Registered employee	Full Time	3	car-motorbike
SOC/DART/IL/GRN/022	Other	no relation		Registered employee	Full Time	3	none
SOC/DART/IL/GRN/022	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/IL/GRN/025	Other	no relation		Registered employee	Full Time	12	none
SOC/DART/IL/GRN/025	Other	no relation		Registered employee	Full Time	10	daladala
SOC/DART/IL/GRN/025	Other	no relation		Registered employee	Full Time	4	daladala
SOC/DART/IL/GRN/025	Other	no relation		Registered employee	Full Time	5	daladala
SOC/DART/IL/GRN/027	Other	no relation		Registered employee	Partial Time	8	none
SOC/DART/IL/GRN/027	Other	no relation		Registered employee	Full Time	8	daladala
SOC/DART/IL/GRN/027	Other	no relation		Registered employee	Full Time	8	daladala
SOC/DART/IL/GRN/027	Other	no relation		Registered employee	Full Time	6	daladala
SOC/DART/IL/GRN/031	Other	no relation		Registered employee	Full Time	7	daladala
SOC/DART/IL/GRN/031	Other	no relation		Registered employee	Full Time	7	daladala
SOC/DART/IL/GRN/031	Other	no relation		Registered employee	Full Time	7	daladala
SOC/DART/IL/GRN/031A	Other	no relation		not applicable	Partial Time	7	walking
SOC/DART/IL/GRN/031A	Other	no relation		Registered employee	Full Time	7	daladala

Legend
 Yes
 Not Informed

Annex 03 - Part 4

Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of this Socio-Economic Unit	No. of Employee	Relation to Owner	Lives within 300m of corridor	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
SOC/DART/IL/GRN/031A	Other	no relation		Registered employee	Full Time	7	daladala
SOC/DART/IL/GRN/034	Owner	not applicable		not applicable	Partial Time	8	none
SOC/DART/IL/GRN/034	Manager	not applicable		formal	Full Time	8	none
SOC/DART/IL/GRN/034	Other	no relation		Informal	Full Time	3	daladala
SOC/DART/IL/GRN/034	Other	no relation		Informal	Full Time	3	daladala
SOC/DART/IL/GRN/034	Other	no relation		Informal	Full Time	3	daladala
SOC/DART/IL/GRN/034	Other	no relation		Informal	Full Time	3	daladala
SOC/DART/IL/GRN/034	Other	no relation		Informal	Full Time	3	daladala
SOC/DART/IL/GRN/034	Other	no relation		Informal	Full Time	3	daladala
SOC/DART/IL/GRN/036	Owner	not applicable		not applicable	Full Time	10	none
SOC/DART/IL/GRN/036	Other	no relation		Informal	Full Time	10	daladala
SOC/DART/IL/GRN/036	Other	no relation		Informal	Full Time	10	daladala
SOC/DART/IL/GRN/036	Other	no relation		Informal	Full Time	10	daladala
SOC/DART/IL/GRN/038	Other	not applicable		not applicable	Full Time	5	none
SOC/DART/IL/GRN/038	Other	not applicable		not applicable	Full Time	5	daladala
SOC/DART/IL/GRN/047	Owner	not applicable		not applicable	Full Time	3	none
SOC/DART/IL/GRN/047	Other	not applicable		not applicable	Full Time	3	none
SOC/DART/IL/GRN/047	Other	not applicable		Registered employee	Full Time	3	none
SOC/DART/IL/GRN/047	Other	not applicable		Registered employee	Full Time	3	none
SOC/DART/IL/GRN/047	Other	not applicable		Registered employee	Full Time	3	none
SOC/DART/IL/GRN/047	Other	not applicable		Registered employee	Full Time	3	none
SOC/DART/IL/GRN/063	Other	not applicable		not applicable	Full Time	17	none
SOC/DART/IL/GRN/063	Other	not applicable		Registered employee	Full Time	0	none

Legend
 Yes
 No/Not Informed

Annex 03 - Part 4

Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of this Socio-Economic Unit	N° of Employee	Relation to Owner	Lives within 300m of corridor?	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
SOC/DART/IL/GRN/068	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/IL/GRN/068	Other	no relation		Registered employee	Partial Time	5	none
SOC/DART/IL/GRN/068	Other	no relation		Registered employee	Full Time	5	none
SOC/DART/IL/GRN/068	Other	no relation		Registered employee	Full Time	13	none
SOC/DART/IL/GRN/068	Other	no relation		Registered employee	Full Time	1	none
SOC/DART/IL/GRN/068	Other	no relation		Registered employee	Full Time	5	none
SOC/DART/IL/GRN/075	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/IL/GRN/075	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/IL/GRN/077	Owner	owner		not applicable	Full Time	1	none
SOC/DART/IL/GRN/077	Other	no relation		Informal	Full Time	1	none
SOC/DART/IL/GRN/077	Other	no relation		Informal	Full Time	1	daladala
SOC/DART/IL/GRN/077	Other	no relation		Informal	Full Time	1	daladala
SOC/DART/IL/GRN/077	Other	no relation		Informal	Full Time	1	daladala
SOC/DART/IL/GRN/077	Other	no relation		Informal	Full Time	1	none
SOC/DART/IL/GRN/077	Other	no relation		Informal	Full Time	1	none
SOC/DART/IL/GRN/078	Other	no relation		Registered employee	Full Time	24	none
SOC/DART/IL/GRN/078	Other	no relation		Registered employee	Full Time	24	none
SOC/DART/IL/GRN/078	Other	no relation		Registered employee	Full Time	10	none
SOC/DART/IL/GRN/078	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/078	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/078	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/078	Other	no relation		Registered employee	Full Time	1	daladala

Legend
 Yes
 No/Not Informed

Annex 03 - Part 4

Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of this Socio-Economic Unit	N° of Employee	Relation to Owner	Lives within 300m of corridor?	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
SOC/DART/IL/GRN/078	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/IL/GRN/081	Owner	not applicable		not applicable	Partial Time	17	daladala
SOC/DART/IL/GRN/081	Manager	not applicable		Informal	Partial Time	17	daladala
SOC/DART/IL/GRN/081	Other	not applicable		Informal	Partial Time	7	none
SOC/DART/IL/GRN/081	Other	not applicable		Informal	Full Time	4	none
SOC/DART/IL/GRN/085	Other	no relation		Informal	Full Time	4	daladala
SOC/DART/IL/GRN/086	Owner	not applicable		Informal	Full Time	17	none
SOC/DART/IL/GRN/093	Owner	not applicable		Informal	Full Time	3	none
SOC/DART/IL/GRN/093	Other	agreggates		Registered employee	Full Time	3	daladala
SOC/DART/IL/GRN/093	Other	agreggates		Registered employee	Full Time	3	daladala
SOC/DART/IL/GRN/093	Other	father/mother		Registered employee	Full Time	6	daladala
SOC/DART/IL/GRN/093	Other	father/mother		Registered employee	Full Time	2	daladala
SOC/DART/IL/GRN/093	Other	father/mother		Registered employee	Full Time	3	daladala
SOC/DART/IL/GRN/093	Other	grandson/granddaughter		Registered employee	Full Time	2	daladala
SOC/DART/IL/GRN/093	Other	not applicable		Informal	Full Time	5	none
SOC/DART/IL/GRN/093	Other	not applicable		Informal	Partial Time	5	none
SOC/DART/IL/GRN/100	Owner	not applicable		not applicable	Partial Time	2	none
SOC/DART/IL/GRN/100	Other	grandson/granddaughter		Informal	Full Time	2	none
SOC/DART/IL/GRN/103	Other	not applicable		Informal	Full Time	13	none
SOC/DART/IL/GRN/103	Other	not applicable		Informal	Full Time	13	none
SOC/DART/KN/HNF/007	Owner	not applicable		not applicable	Full Time	4	daladala
SOC/DART/KN/HNF/007	Manager	not applicable		not applicable	Full Time	4	daladala
SOC/DART/KN/HNF/007	Other	no relation		Other	Full Time	3	daladala

Legend
 Yes
 Not informed

Annex 03 - Part 4

Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of this Socio-Economic Unit	N° of Employee	Relation to Owner	Lives within 300m of corridor?	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
SOC/DART/KN/HNF/007A	Manager	not applicable			Full Time	0	daladala
SOC/DART/KN/HNF/007A	Other	no relation		Other	Full Time	0	daladala
SOC/DART/KN/HNF/007A	Other	no relation		Other	Full Time	0	daladala
SOC/DART/KN/HNF/007A	Other	no relation		Other	Full Time	0	daladala
SOC/DART/KN/HNF/007A	Other	no relation		Other	Full Time	0	daladala
SOC/DART/KN/HNF/007B	Owner	not applicable		not applicable	Full Time	7	daladala
SOC/DART/KN/HNF/007B	Manager	not applicable			Full Time	7	daladala
SOC/DART/KN/HNF/007B	Other	other relative		Other	Full Time	5	daladala
SOC/DART/KN/HNF/007B	Other	no relation		Other	Full Time	4	daladala
SOC/DART/KN/HNF/008	Owner	not applicable		not applicable	Full Time	10	daladala
SOC/DART/KN/HNF/009	Owner	not applicable		not applicable	Full Time	12	daladala
SOC/DART/KN/HNF/009	Manager	other relative		Not informed	Full Time	12	daladala
SOC/DART/KN/HNF/009	Other	no relation		Not informed		12	daladala
SOC/DART/KN/HNF/009	Other	no relation		Not informed		12	daladala
SOC/DART/KN/HNF/009	Other	no relation		Not informed		12	daladala
SOC/DART/KN/HNF/009	Other	no relation		Not informed		12	daladala
SOC/DART/KN/HNF/009	Other	no relation		Not informed		12	daladala
SOC/DART/KN/HNF/009	Other	no relation		Not informed		8	daladala
SOC/DART/KN/HNF/009	Other	no relation		Not informed		8	daladala
SOC/DART/KN/HNF/009	Other	no relation		Not informed		8	daladala
SOC/DART/KN/HNF/009	Other	no relation		Not informed		6	daladala
SOC/DART/KN/HNF/009	Other	no relation		Not informed		4	daladala
SOC/DART/KN/HNF/009	Other	no relation		Not informed		5	daladala

Legend
 Yes
 Not informed

Annex 03 - Part 4

Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of this Socio-Economic Unit	N° of Employee	Relation to Owner	Lives within 300m of corridor?	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
SOC/DART/KN/HNF/009	Other	no relation		Not informed		4	daladala
SOC/DART/KN/HNF/010A	Owner	not applicable		not applicable	Full Time	17	daladala
SOC/DART/KN/HNF/010A	Other	no relation		Registered employee	Full Time	5	daladala
SOC/DART/KN/HNF/010B	Owner	not applicable		not applicable	Partial Time	16	car-motorbike
SOC/DART/KN/HNF/010B	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/KN/HNF/010B	Other	no relation		Registered employee	Full Time	12	daladala
SOC/DART/KN/HNF/010C	Owner	not applicable		not applicable	Partial Time	2	car-motorbike
SOC/DART/KN/HNF/010C	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/KN/HNF/010D	Owner	not applicable		not applicable	Full Time	2	daladala
SOC/DART/KN/HNF/010D	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/KN/HNF/010D	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/KN/HNF/010D	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/KN/HNF/011	Other	no relation		Registered employee	Full Time	5	walking
SOC/DART/KN/HNF/011	Other	no relation		Registered employee	Full Time	4	walking
SOC/DART/KN/HNF/011	Other	no relation		Registered employee	Full Time	4	walking
SOC/DART/KN/HNF/011	Other	no relation		Registered employee	Full Time	4	walking
SOC/DART/KN/HNF/011	Other	no relation		Registered employee	Full Time	2	walking
SOC/DART/KN/HNF/011	Other	no relation		Registered employee	Full Time	2	walking
SOC/DART/KN/HNF/011A	Owner	not applicable		not applicable	Full Time	12	car-motorbike
SOC/DART/KN/HNF/011A	Manager	no relation		Registered employee	Full Time	12	car-motorbike
SOC/DART/KN/HNF/011A	Other	no relation		Registered employee	Full Time	12	daladala
SOC/DART/KN/HNF/011A	Other	no relation		Registered employee	Full Time	12	daladala
SOC/DART/KN/HNF/011A	Other	no relation		Registered employee	Full Time	12	daladala

Legend
 Yes
 Not Informed

Annex 03 - Part 4

Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of this Socio-Economic Unit	N° of Employee	Relation to Owner	Lives within 100m of corridor?	Type of Work Contract	Working Period	How long does the person have this occupation*	Means of Transport to Work
SOC/DART/KN/HNF/011A	Other	no relation		Registered employee	Full Time	12	daladala
SOC/DART/KN/HNF/011A	Other	no relation		Registered employee	Full Time	12	daladala
SOC/DART/KN/HNF/011A	Other	no relation		Registered employee	Full Time	12	daladala
SOC/DART/KN/HNF/011A	Other	no relation		Registered employee	Full Time	12	car-motorbike
SOC/DART/KN/HNF/011A	Other	no relation		Registered employee	Full Time	12	car-motorbike
SOC/DART/KN/HNF/011A	Other	no relation		Registered employee	Full Time	12	daladala
SOC/DART/KN/HNF/011B	Owner	not applicable		not applicable	Partial Time	12	car-motorbike
SOC/DART/KN/HNF/011B	Other	no relation		Registered employee	Full Time	7	daladala
SOC/DART/KN/HNF/011B	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/KN/HNF/011C	Owner	not applicable		not applicable	Partial Time	6	car-motorbike
SOC/DART/KN/HNF/011C	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/KN/HNF/011C	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/KN/HNF/011D	Owner	not applicable		not applicable	Partial Time	6	car-motorbike
SOC/DART/KN/HNF/011D	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/KN/HNF/011D	Other	no relation		Registered employee	Full Time	4	daladala
SOC/DART/KN/HNF/011D	Other	no relation		Registered employee	Full Time	4	daladala
SOC/DART/KN/HNF/011D	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/KN/HNF/011D	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/KN/HNF/011D	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/KN/HNF/011D	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/KN/HNF/011E	Owner	not applicable		not applicable	Full Time	8	daladala
SOC/DART/KN/HNF/011E	Other	no relation		Registered employee	Full Time	5	daladala

Legend
 Yes
 Not Inform

Annex 03 - Part 4

Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of this Socio-Economic Unit	N° of Employee	Relation to Owner	Lives within 300m of corridor?	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
SOC/DART/KN/HNF/011F	Owner	not applicable		not applicable	Full Time	12	daladala
SOC/DART/KN/HNF/011F	Other	no relation		Informal	Full Time	3	daladala
SOC/DART/KN/HNF/011F	Other	no relation		Informal	Full Time	1	walking
SOC/DART/KN/HNF/011F	Other	no relation		Informal	Full Time	2	car-motorbike
SOC/DART/KN/HNF/011F	Other	no relation		Informal	Full Time	2	daladala
SOC/DART/KN/HNF/011F	Other	no relation		Informal	Full Time	2	daladala
SOC/DART/KN/HNF/011F	Other	no relation		Informal	Full Time	2	daladala
SOC/DART/KN/HNF/011F	Other	no relation		Informal	Full Time	2	daladala
SOC/DART/KN/HNF/011F	Other	no relation		Informal	Full Time	2	daladala
SOC/DART/KN/HNF/012A	Owner	not applicable		not applicable	Partial Time	1	car-motorbike
SOC/DART/KN/HNF/012A	Manager	no relation		Registered employee	Full Time	1	daladala
SOC/DART/KN/KMR/003	Manager	no relation		Other	Full Time	3	none
SOC/DART/KN/KMR/003	Other	no relation		Other	Full Time	3	none
SOC/DART/KN/KMR/003	Other	no relation		Other	Full Time	3	none
SOC/DART/KN/KMR/003	Other	no relation		Other	Full Time	3	none
SOC/DART/KN/KMR/003	Other	no relation		Other	Full Time	3	none
SOC/DART/KN/KMR/003	Other	no relation		Other	Full Time	3	none
SOC/DART/KN/KMR/003	Other	no relation		Other	Full Time	3	none
SOC/DART/KN/KMR/003	Other	no relation		Other	Full Time	3	none
SOC/DART/KN/KMR/003	Other	no relation		Other	Full Time	3	none
SOC/DART/KN/KMR/004	Other	no relation		Other	Full Time	1	none
SOC/DART/KN/KMR/004	Other	no relation		Other	Full Time	25	none
SOC/DART/KN/KMR/004	Other	no relation		Other	Full Time	10	none
SOC/DART/KN/KMR/004	Other	son/daughter		Other	Full Time	4	daladala
SOC/DART/KN/KMR/004	Other	no relation		Other	Full Time	5	daladala

Legend
 Yes
 Not Inform

Annex 03 - Part 4

Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of this Socio-Economic Unit	N° of Employee	Relation to Owner	Lives within 300m of corridor?	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
					Full Time	12	none
SOC/DART/KN/KMR/004A	Owner	not applicable		not applicable	Full Time	11	car-motorbike
SOC/DART/KN/KMR/004A	Other	other relative		Other	Full Time	6	daladala
SOC/DART/KN/KMR/004A	Other	no relation		Other	Full Time	6	daladala
SOC/DART/KN/KMR/004A	Other	no relation		Other	Full Time	6	daladala
SOC/DART/KN/KMR/004A	Other	no relation		Other	Full Time	6	walking
SOC/DART/KN/KMR/004A	Other	no relation		Other	Full Time	3	daladala
SOC/DART/KN/KMR/004A	Other	no relation		Other	Full Time	4	daladala
SOC/DART/KN/KMR/004A	Other	no relation		Other	Full Time	3	daladala
SOC/DART/KN/KMR/004A	Other	no relation		Other	Full Time	2	daladala
SOC/DART/KN/KMR/004A	Other	no relation		Other	Full Time	7	daladala
SOC/DART/KN/KMR/005	Manager	no relation			Full Time	2	walking
SOC/DART/KN/KMR/005	Other	no relation		Other	Full Time	1	daladala
SOC/DART/KN/KMR/005	Other	no relation		Other	Full Time	0	daladala
SOC/DART/KN/KMR/005A	Owner	not applicable		not applicable	Full Time	3	daladala
SOC/DART/KN/KMR/005A	Manager	not applicable		not applicable	Full Time	3	daladala
SOC/DART/KN/KMR/005A	Other	no relation		Other	Full Time	1	daladala
SOC/DART/KN/KMR/005B	Owner	not applicable		not applicable	Full Time	4	daladala
SOC/DART/KN/KMR/005B	Manager	no relation			Full Time	4	daladala
SOC/DART/KN/KMR/005B	Other	no relation		Other	Full Time	4	daladala
SOC/DART/KN/KMR/006A	Owner	not applicable		not applicable	Full Time	5	car-motorbike
SOC/DART/KN/KMR/006A	Other	no relation		Other	Full Time	5	walking
SOC/DART/KN/KMR/006A	Other	no relation		Other	Full Time	1	walking

Legend
 Yes
 No/Not Informed

Annex 03 - Part 4

Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of this Socio-Economic Unit	N° of Employee	Relation to Owner	Lives within 300m of corridor?	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
SOC/DART/KN/KMR/006A	Other	no relation		Informal	Full Time	5	daladala
SOC/DART/KN/KMR/006A	Other	no relation		Informal	Full Time	5	daladala
SOC/DART/KN/KMR/006A	Other	no relation		Informal	Full Time	5	daladala
SOC/DART/KN/KMR/006A	Other	no relation		Informal	Full Time	5	daladala
SOC/DART/KN/KMR/006A	Other	no relation		Informal	Full Time	5	walking
SOC/DART/KN/KMR/006A	Other	no relation		Informal	Full Time	5	walking
SOC/DART/KN/KMR/006B	Owner	not applicable		not applicable	Full Time	21	car-motorbike
SOC/DART/KN/KMR/006B	Other	no relation			Full Time	4	daladala
SOC/DART/KN/KMR/006B	Other	no relation			Full Time	13	none
SOC/DART/KN/KMR/006B	Other	no relation			Full Time	2	daladala
SOC/DART/KN/KMR/006B	Other	no relation			Full Time	14	none
SOC/DART/KN/KMR/006B	Other	no relation			Full Time	20	none
SOC/DART/KN/KMR/006B	Other	no relation			Full Time	7	daladala
SOC/DART/KN/KMR/006B	Other	no relation			Full Time	4	daladala
SOC/DART/KN/KMR/006B	Other	no relation			Full Time	21	none
SOC/DART/KN/KMR/006B	Other	no relation			Full Time	18	daladala
SOC/DART/KN/KMR/006B	Other	no relation			Full Time	17	daladala
SOC/DART/KN/KMR/007A	Owner	not applicable		not applicable	Full Time	22	car-motorbike
SOC/DART/KN/KMR/007C	Owner	not applicable		not applicable	Full Time	3	none
SOC/DART/KN/KMR/007C	Other	no relation		Registered employee	Full Time	3	none

Legend
 Yes
 Not/Not Informed

Annex 03 - Part 4

Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of this Socio-Economic Unit	N° of Employee	Relation to Owner	Lives within 300m of corridor?	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
SOC/DART/KN/KMR/007C	Other	no relation		Registered employee	Full Time	3	none
SOC/DART/KN/KMR/007D	Other	no relation		Registered employee	Partial Time	18	car-motorbike
SOC/DART/KN/KMR/007E	Other	no relation		Registered employee	Full Time	8	daladala
SOC/DART/KN/KMR/007F	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/KN/KMR/007G	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/KN/KMR/007H	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/KN/KMR/007I	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/KN/KMR/007J	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/KN/KMR/007K	Other	no relation		Registered employee	Full Time	6	daladala
SOC/DART/KN/KMR/007L	Other	no relation		Registered employee	Full Time	12	daladala
SOC/DART/KN/KMR/007M	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/KN/KMR/007N	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/KN/KMR/007O	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/KN/KMR/008	Owner	not applicable		not applicable	Full Time	1	daladala
SOC/DART/KN/KMR/008	Manager	agreggates		Other	Partial Time	1	car-motorbike
SOC/DART/KN/KMR/008	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/KN/KMR/008	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/KN/KMR/008	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/KN/KMR/008	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/KN/KMR/008	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/KN/KMR/008	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/KN/KMR/008	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/KN/KMR/008A	Owner	not applicable		not applicable	Full Time	1	car-motorbike

Legend
 Yes
 No/Not Informed

Annex 03 - Part 4

Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of this Socio-Economic Unit	N° of Employee	Relation to Owner	Lives within 500m of corridor?	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
SOC/DART/KN/KMR/008A	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/KN/KMR/008A	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/KN/KMR/009A	Owner	not applicable		not applicable	Partial Time	2	daladala
SOC/DART/KN/KMR/009A	Manager	no relation		Registered employee	Partial Time	1	walking
SOC/DART/KN/KMR/009A	Other	no relation		Registered employee	Partial Time	1	walking
SOC/DART/KN/KMR/009A	Other	no relation		Registered employee	Partial Time	1	walking
SOC/DART/KN/KMR/009A	Other	no relation		Registered employee	Partial Time	1	walking
SOC/DART/KN/KMR/009B	Other	no relation		Registered employee	Full Time	4	daladala
SOC/DART/KN/KMR/009B	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/KN/KMR/009C	Owner	not applicable		not applicable	Full Time	4	daladala
SOC/DART/KN/KMR/009D	Owner	no relation		Registered employee	Full Time	2	walking
SOC/DART/KN/KMR/009D	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/KN/KMR/009D	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/KN/KMR/009D	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/KN/KMR/009E	Owner	no relation		not applicable	Full Time	1	walking
SOC/DART/KN/KMR/009E	Other	no relation		Registered employee	Full Time	1	none
SOC/DART/KN/KMR/009F	Owner	not applicable		not applicable	Full Time	6	daladala
SOC/DART/KN/KMR/009F	Other	no relation		Registered employee	Full Time	2	none
SOC/DART/KN/KMR/009G	Owner	not applicable		not applicable	Full Time	2	daladala
SOC/DART/KN/KMR/009G	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/KN/KMR/011A	Owner	not applicable		not applicable	Full Time	1	daladala
SOC/DART/KN/KMR/011A	Other	no relation		Registered employee	Full Time	1	daladala

Legend
 Yes
 No/Not Informed

Annex 03 - Part 4

Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of the Socio-Economic Unit	N° of Employee	Relation to Owner	Lives within 300m of corridor?	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
SOC/DART/KN/KMR/012	Owner	not applicable		not applicable	Full Time	3	walking
SOC/DART/KN/KMR/012A	Owner	not applicable		not applicable	Partial Time	1	walking
SOC/DART/KN/KMR/012A	Other	agreggates		Other	Full Time	1	walking
SOC/DART/KN/KMR/012B	Owner	not applicable		not applicable	Partial Time	6	walking
SOC/DART/KN/KMR/012B	Other	agreggates		Other	Partial Time	3	walking
SOC/DART/KN/KMR/013	Owner	not applicable		not applicable	Partial Time	3	daladala
SOC/DART/KN/KMR/013A	Owner	not applicable		not applicable	Partial Time	2	walking
SOC/DART/KN/KMR/013B	Owner	not applicable		not applicable	Partial Time	2	daladala
SOC/DART/KN/KMR/013C	Owner	not applicable		not applicable	Partial Time	8	walking
SOC/DART/KN/KMR/013D	Owner	not applicable		not applicable	Partial Time	7	walking
SOC/DART/KN/KMR/013D	Other	no relation		Registered employee	Partial Time	3	daladala
SOC/DART/KN/KMR/014	Owner	not applicable		not applicable	Full Time	16	car-motorbike
SOC/DART/KN/KMR/014	Manager	no relation		Registered employee	Full Time	16	daladala
SOC/DART/KN/KMR/014	Other	other relative		Registered employee	Full Time	16	daladala
SOC/DART/KN/KMR/014	Other	no relation		Registered employee	Full Time	7	daladala
SOC/DART/KN/KMR/014	Other	no relation		Registered employee	Full Time	4	none
SOC/DART/KN/KMR/014	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/KN/KMR/014	Owner	not applicable		not applicable	Full Time	8	walking
SOC/DART/KN/KMR/014B	Owner	not applicable		not applicable	Full Time	6	car-motorbike
SOC/DART/KN/KMR/014B	Other	other relative		Registered employee	Full Time	4	daladala
SOC/DART/KN/KMR/014C	Owner	not applicable		not applicable	Full Time	5	walking
SOC/DART/KN/KMR/014C	Other	other relative		Registered employee	Full Time	2	walking
SOC/DART/KN/KMR/014C	Other	no relation		Registered employee	Full Time	2	walking

Annex 03 - Part 4

Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of this Socio-Economic Unit	N° of Employee	Relation to Owner	Lives within 300m of corridor?	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
SOC/DART/KN/KMR/014C	Owner	not applicable		Registered employee	Full Time	3	walking
SOC/DART/KN/KMR/014C	Owner	not applicable		Registered employee	Full Time	3	walking
SOC/DART/KN/KMR/014C	Owner	not applicable		Registered employee	Full Time	3	none
SOC/DART/KN/KMR/014E	Owner	not applicable		not applicable	Full Time	7	daladala
SOC/DART/KN/KMR/015	Owner	not applicable		not applicable	Full Time	25	car-motorbike
SOC/DART/KN/KMR/015	Manager	other relative		Registered employee	Full Time	20	walking
SOC/DART/KN/KMR/015	Other	no relation		Registered employee	Full Time	16	daladala
SOC/DART/KN/KMR/015	Other	no relation		Registered employee	Full Time	4	walking
SOC/DART/KN/KMR/015	Other	no relation		Registered employee	Full Time	6	walking
SOC/DART/KN/KMR/015	Other	no relation		Registered employee	Full Time	10	walking
SOC/DART/KN/KMR/015	Other	no relation		Registered employee	Full Time	3	walking
SOC/DART/KN/KMR/015A	Owner	not applicable		not applicable	Full Time	6	car-motorbike
SOC/DART/KN/KMR/015A	Manager	not applicable			Full Time	6	car-motorbike
SOC/DART/KN/KMR/015A	Other	no relation		Registered employee	Full Time	6	daladala
SOC/DART/KN/KMR/015A	Other	no relation		Registered employee	Full Time	6	daladala
SOC/DART/KN/KMR/015A	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/KN/KMR/017	Owner	not applicable		not applicable	Full Time	22	car-motorbike
SOC/DART/KN/KMR/017	Manager			Not informed	Full Time	22	car-motorbike
SOC/DART/KN/KMR/017	Other	no relation		Other	Full Time	18	walking
SOC/DART/KN/KMR/017	Other	no relation		Other	Full Time	10	walking
SOC/DART/KN/KMR/017	Other	no relation		Other	Full Time	10	walking
SOC/DART/KN/KMR/017	Other	other relative		Other	Full Time	5	walking
SOC/DART/KN/KMR/017B	Owner	not applicable		not applicable	Full Time	8	car-motorbike

Legend
 Yes
 No/Not Informed

Annex 03 - Part 4

Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of this Socio-Economic Unit	N° of Employee	Relation to Owner	Lives within 500m of corridor?	Type of Work Contract	Working Period	How long does the person have this occupation*	Means of Transport to Work
SOC/DART/KN/KMR/017B	Manager			Not informed	Full Time	8	car-motorbike
SOC/DART/KN/KMR/017B	Other	other relative		Other	Full Time	7	car-motorbike
SOC/DART/KN/KMR/017B	Other	no relation		Other	Full Time	3	daladala
SOC/DART/KN/KMR/017C	Owner	not applicable		not applicable	Full Time	10	daladala
SOC/DART/KN/KMR/017C	Other	other relative		Other	Full Time	4	daladala
SOC/DART/KN/KMR/017C	Other	agreggates		Other	Full Time	1	daladala
SOC/DART/KN/KMR/017C	Other	agreggates		Other	Full Time	1	daladala
SOC/DART/KN/KMR/020	Owner	not applicable		not applicable	Full Time	0	none
SOC/DART/KN/KMR/020	Owner	not applicable		not applicable	Full Time	0	daladala
SOC/DART/KN/KMR/020A	Owner	not applicable		not applicable	Full Time	3	none
SOC/DART/KN/KMR/020A	Manager	no relation		Other	Full Time	3	none
SOC/DART/KN/KMR/020A	Other	no relation		Other	Full Time	2	none
SOC/DART/KN/KMR/020A	Other	no relation		Other	Full Time	1	none
SOC/DART/KN/KMR/023	Owner	not applicable		not applicable	Full Time	28	daladala
SOC/DART/KN/KMR/023	Manager	no relation		Registered employee	Full Time		daladala
SOC/DART/KN/KMR/025B	Owner	not applicable		not applicable	Full Time	3	none
SOC/DART/KN/KMR/025B	Manager	no relation		Other	Full Time	3	none
SOC/DART/KN/KMR/025B	Other	no relation		Other	Full Time	3	none
SOC/DART/KN/KMR/026	Owner	not applicable		not applicable	Full Time	0	walking
SOC/DART/KN/KMR/026	Other	no relation		Registered employee	Full Time	0	walking
SOC/DART/KN/KMR/026	Other	no relation		Registered employee	Full Time	0	walking
SOC/DART/KN/KMR/026	Other	no relation		Registered employee	Full Time	0	walking
SOC/DART/KN/KMR/026B	Owner	not applicable		not applicable	Full Time	17	none

Legend
 Yes
 No/Not informed

Annex 03 - Part 4

Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of this Socio-Economic Unit	N° of Employee	Relation to Owner	Lives within 300m of corridor?	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
						13	none
							none
SOC/DART/KN/KMR/029	Owner	not applicable		not applicable	Full Time	3	none
SOC/DART/KN/KMR/029A	Owner	not applicable		not applicable	Partial Time	2	daladala
SOC/DART/KN/KMR/029A	Other	other relative		Informal	Full Time	2	daladala
SOC/DART/KN/KMR/029B	Owner	not applicable		not applicable	Full Time	10	daladala
SOC/DART/KN/KMR/029B						4	none
SOC/DART/KN/KMR/031	Owner	not applicable		not applicable	Full Time	9	daladala
SOC/DART/KN/KMR/031	Other	no relation		Other	Full Time	9	daladala
SOC/DART/KN/KMR/031	Other	no relation		Other	Full Time	4	daladala
SOC/DART/KN/KND/002A	Owner	not applicable		not applicable	Full Time	3	car-motorbike
SOC/DART/KN/KND/002A	Other	no relation		Registered employee	Full Time	1	car-motorbike
SOC/DART/KN/MG2/000						6	none
SOC/DART/KN/MG2/000						6	none
SOC/DART/KN/MG2/001	Other	no relation		Other	Full Time	5	daladala
SOC/DART/KN/MG2/001	Other	no relation		Other	Full Time	2	daladala
SOC/DART/KN/MG2/002	Owner	not applicable		not applicable	Partial Time	0	walking
SOC/DART/KN/MG2/002	Manager	other relative		Other	Partial Time	0	walking
SOC/DART/KN/MG2/002D						0	daladala
SOC/DART/KN/MG2/003	Owner	not applicable		not applicable	Full Time	6	daladala
SOC/DART/KN/MG2/003	Manager	other relative		Informal	Partial Time	3	daladala
SOC/DART/KN/MG2/003A	Owner	not applicable		not applicable	Full Time	7	car-motorbike
SOC/DART/KN/MG2/003A	Manager	no relation		Registered employee	Full Time	1	daladala

Annex 03 - Part 4

Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of this Socio-Economic Unit	N° of Employee	Relation to Owner	Lives within 100m of corridor?	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
SOC/DART/KN/MG2/003B	Owner	not applicable		not applicable	Full Time	5	daladala
SOC/DART/KN/MG2/003B	Manager	no relation			Full Time	5	daladala
SOC/DART/KN/MG2/003C	Other	no relation		Registered employee	Full Time	7	daladala
SOC/DART/KN/MG2/003C	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/KN/MG2/003C	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/KN/MG2/003C	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/KN/MG2/003C	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/KN/MG2/003C	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/KN/MG2/004A	Other	no relation		Registered employee	Full Time	5	daladala
SOC/DART/KN/MG2/004A	Other	no relation		Registered employee	Full Time	5	daladala
SOC/DART/KN/MG2/004A	Other	no relation		Registered employee	Full Time	5	daladala
SOC/DART/KN/MG2/004A	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/KN/MG2/004A	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/KN/MG2/004A	Other	no relation		Registered employee	Full Time	5	daladala
SOC/DART/KN/MG2/004A	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/KN/MG2/004A	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/KN/MG2/004A	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/KN/MG2/004A	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/KN/MG2/005	Other	other relative		Other	Full Time	17	daladala
SOC/DART/KN/MG2/005	Other	other relative		Other	Full Time	18	daladala
SOC/DART/KN/MG2/005	Other	other relative		Other	Full Time	1	daladala
SOC/DART/KN/MG2/005	Other	other relative		Other	Full Time	14	daladala

Legend
 Yes
 Not/Not Informed

Annex 03 - Part 4

Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of this Socio-Economic Unit	N° of Employee	Relation to Owner	Lives within 300m of corridor?	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
SOC/DART/KN/MG2/005	Other	other relative		Other	Full Time	18	daladala
SOC/DART/KN/MG2/005A	Manager	other relative		Other	Full Time	1	daladala
SOC/DART/KN/MG2/005A	Other	no relation		Other	Full Time	0	daladala
SOC/DART/KN/MG2/005C	Owner	not applicable		not applicable	Full Time		none
SOC/DART/KN/MG2/005C	Other	no relation		Other	Full Time	4	daladala
SOC/DART/KN/MG2/005C	Other	no relation		Other	Full Time	24	daladala
SOC/DART/KN/MG2/005D	Manager	other relative		Other	Full Time	1	daladala
SOC/DART/KN/MNY/001	Owner	not applicable		not applicable	Full Time	26	car-motorbike
SOC/DART/KN/MNY/001	Manager	not applicable		Registered employee	Full Time	9	car-motorbike
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	18	daladala
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	0	daladala
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	0	daladala
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	7	daladala
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	5	daladala
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	5	daladala
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	0	daladala
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	5	daladala
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	5	daladala

Annex 03 - Part 4

Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of this Socio-Economic Unit	N° of Employee	Relation to Owner	Lives within 300m of corridor?	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	7	daladala
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	6	daladala
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	6	daladala
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	6	daladala
SOC/DART/KN/MNY/001	Other	no relation		Registered employee	Full Time	2	daladala
SOC/DART/KN/UB2/001	Owner	not applicable		not applicable	Full Time	7	daladala
SOC/DART/KN/UB2/001	Manager	no relation			Full Time	1	daladala
SOC/DART/KN/UB2/001	Other	no relation		Registered employee	Full Time	8	daladala
SOC/DART/KN/UB2/001	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/KN/UB2/003	Owner	not applicable		not applicable	Full Time	17	car-motorbike
SOC/DART/KN/UB2/003	Other	no relation		Registered employee	Full Time	15	daladala
SOC/DART/KN/UB2/003	Other	no relation		Informal	Full Time	5	daladala
SOC/DART/KN/UB2/003	Other	no relation		Registered employee	Full Time	10	daladala
SOC/DART/KN/UB2/003	Other	no relation		Registered employee	Full Time	15	car-motorbike
SOC/DART/KN/UB2/003	Other	no relation		Registered employee	Full Time	15	car-motorbike
SOC/DART/KN/UB2/003A	Owner	not applicable		not applicable	Full Time	20	car-motorbike
SOC/DART/KN/UB2/003A	Other	son/daughter		Informal	Partial Time	6	car-motorbike
SOC/DART/KN/UB2/004	Owner	not applicable		not applicable	Full Time	17	car-motorbike
SOC/DART/KN/UB2/004	Other	no relation		Registered employee	Full Time	15	daladala
SOC/DART/KN/UB2/004	Other	no relation		Registered employee	Full Time	5	daladala
SOC/DART/KN/UB2/004	Other	son/daughter		Informal	Partial Time	7	daladala
SOC/DART/KN/UB2/004	Other	son/daughter		Registered employee	Partial Time	10	daladala

Legend
 Yes
 Not Inform

Annex 03 - Part 4

Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of this Socio-Economic Unit	N° of Employee	Relation to Owner	Lives within 300m of corridor?	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
SOC/DART/KN/UB2/005	Other	other relative		Registered employee	Full Time	2	daladala
SOC/DART/KN/UB2/006	Owner	not applicable		not applicable	Full Time	3	daladala
SOC/DART/KN/UB2/006	Other	no relation		Registered employee	Full Time	3	daladala
SOC/DART/KN/UB2/007B	Owner	not applicable		not applicable	Full Time	14	daladala
SOC/DART/KN/UB2/008A	Owner	not applicable		not applicable	Partial Time	25	daladala
SOC/DART/KN/UB2/009	Owner	not applicable		not applicable		1	daladala
SOC/DART/KN/UB2/010	Owner	not applicable		not applicable	Full Time	5	daladala
SOC/DART/KN/UB2/010	Other	aggregates			Full Time	4	daladala
SOC/DART/KN/UB2/011	Owner	not applicable		not applicable	Full Time	13	daladala
SOC/DART/KN/UB2/012A	Owner	not applicable		not applicable	Full Time	23	daladala
SOC/DART/KN/UB2/012A	Other	other relative		Registered employee	Full Time	12	daladala
SOC/DART/KN/UB2/013	Owner	not applicable		not applicable	Full Time	2	daladala
SOC/DART/KN/UB2/014	Owner	not applicable		not applicable	Full Time	9	daladala
SOC/DART/KN/UB2/014	Other	no relation		Informal	Full Time	3	daladala
SOC/DART/KN/UB2/014	Other	no relation		Informal	Full Time	5	daladala
SOC/DART/KN/UB2/016	Owner	not applicable		not applicable	Full Time	10	daladala
SOC/DART/KN/UB2/016	Manager	other relative		Registered employee	Full Time	8	daladala
SOC/DART/KN/UB2/021	Owner	not applicable		not applicable	Full Time	12	walking
SOC/DART/KN/UB2/021	Owner	not applicable		not applicable	Full Time	4	car-motorbike
SOC/DART/KN/UB2/021	Manager	no relation		Registered employee	Full Time	4	car-motorbike
SOC/DART/KN/UB2/021	Other	no relation		Registered employee	Full Time	0	daladala
SOC/DART/KN/UB2/021	Other	no relation		Registered employee	Full Time	4	daladala

Legend
 Yes
 Not/Not Informed

Annex 03 - Part 4

Data Bank related to Section 03 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of this Socio-Economic Unit	N° of Employee	Relation to Owner	Lives within 300m of corridor?	Type of Work Contract	Working Period	How long does the person have this occupation?	Means of Transport to Work
SOC/DART/KN/UB2/021	Other	no relation		Registered employee	Full Time	1	daladala
SOC/DART/KN/UB2/021	Other	no relation		Registered employee	Full Time	4	daladala
SOC/DART/KN/UB2/021	Other	no relation		Registered employee	Full Time	4	daladala
SOC/DART/KN/UB2/021	Other	no relation		Registered employee	Full Time	4	car-motorbike

Annex 03 - Part 5

Data Bank related to Section 04 and 05 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of Corresponding Property Survey	Code of this Socio-Economic Unit	Average monthly sales	Average net monthly profit	Available Documentation	Bank Loans	Mortgage Inform value	Residential area within the property
VAL/DART/IL/GRN/001	The interview was not done						
VAL/DART/IL/GRN/001	SOC/DART/IL/GRN/001	21 000 000	500 000				
VAL/DART/IL/GRN/002	SOC/DART/IL/GRN/002	4 500 000	900 000				
VAL/DART/IL/GRN/002	The interview was not done						
VAL/DART/IL/GRN/002	SOC/DART/IL/GRN/002A	2 000 000	200 000				
VAL/DART/IL/GRN/003	SOC/DART/IL/GRN/003	3 000 000	1 000 000				
VAL/DART/IL/GRN/003	SOC/DART/IL/GRN/003A	4 000 000	600 000				
VAL/DART/IL/GRN/003	SOC/DART/IL/GRN/003B						
VAL/DART/IL/GRN/003	SOC/DART/IL/GRN/003C	75 000 000	600 000				
VAL/DART/IL/GRN/004	SOC/DART/IL/GRN/004	1 800 000					
VAL/DART/IL/GRN/004	The interview was not done						
VAL/DART/IL/GRN/004	SOC/DART/IL/GRN/004A	1 500 000	450 000				
VAL/DART/IL/GRN/005	SOC/DART/IL/GRN/005	900 000	400 000				
VAL/DART/IL/GRN/005	The interview was not done						
VAL/DART/IL/GRN/005	SOC/DART/IL/GRN/005A	90 000	30 000				
VAL/DART/IL/GRN/006	SOC/DART/IL/GRN/006	1 000 000	100 000				
VAL/DART/IL/GRN/006	SOC/DART/IL/GRN/006A	1 000 000	4 000 000				
VAL/DART/IL/GRN/006	SOC/DART/IL/GRN/006B	400 000	150 000				
VAL/DART/IL/GRN/006	SOC/DART/IL/GRN/006C	1 000 000	100 000				
VAL/DART/IL/GRN/008	SOC/DART/IL/GRN/008	30 000 000					
VAL/DART/IL/GRN/008	SOC/DART/IL/GRN/008A	10 500 000	1 200 000				
VAL/DART/IL/GRN/008	SOC/DART/IL/GRN/008B	6 000 000	1 200 000				

Legend
 Yes
 Not Inform

Annex 03 - Part 5

Data Bank related to Sections 04 and 05 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of Corresponding Property Survey	Code of this Socio-Economic Unit	Average monthly sales	Average net monthly profit	Available Documentation	Bank Loans	Mortgage Inform value	Residential area within the property
VAL/DART/IL/GRN/008	SOC/DART/IL/GRN/008C	75 000 000	750 000				
VAL/DART/IL/GRN/009	SOC/DART/IL/GRN/009C	900 000	450 000				
VAL/DART/IL/GRN/009	SOC/DART/IL/GRN/009E						
VAL/DART/IL/GRN/010	SOC/DART/IL/GRN/010	1 800 000	600 000				
VAL/DART/IL/GRN/011	SOC/DART/IL/GRN/011A						
VAL/DART/IL/GRN/011	SOC/DART/IL/GRN/011B						
VAL/DART/IL/GRN/012	SOC/DART/IL/GRN/012	450 000	300 000				
VAL/DART/IL/GRN/012	SOC/DART/IL/GRN/012A	2 300 000	1 450 000				
VAL/DART/IL/GRN/012	SOC/DART/IL/GRN/012B						
VAL/DART/IL/GRN/013	SOC/DART/IL/GRN/013	900 000	180 000				
VAL/DART/IL/GRN/013	SOC/DART/IL/GRN/013A	1 200 000	360 000				
VAL/DART/IL/GRN/013	SOC/DART/IL/GRN/013B	2 700 000	150 000				
VAL/DART/IL/GRN/013	SOC/DART/IL/GRN/013C	1 200 000	180 000				
VAL/DART/IL/GRN/015	SOC/DART/IL/GRN/015	360 000	80 000				
VAL/DART/IL/GRN/015	SOC/DART/IL/GRN/015A	450 000	150 000				
VAL/DART/IL/GRN/016	SOC/DART/IL/GRN/016						

Legend
 Yes
 No/Not Informed

Annex 03 - Part 5

Data Bank related to Section 04 and 05 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of Corresponding Property Survey	Code of this Socio-Economic Unit:	Average monthly sales	Average net monthly profit	Available Documentation	Bank Loans	Mortgage Inform value	Residential area within the property
VAL/DART/IL/GRN/016	SOC/DART/IL/GRN/016A	1 000 000	500 000				
VAL/DART/IL/GRN/016	SOC/DART/IL/GRN/016B	600 000	250 000				
VAL/DART/IL/GRN/016	SOC/DART/IL/GRN/016C	250 000 000	80 000				
VAL/DART/IL/GRN/017	SOC/DART/IL/GRN/017	12 000 000	750 000				
VAL DART II GRN 017	SOC DART II GRN 017	1 000 000	100 000				
VAL DART II GRN 017	SOC DART II GRN 017B	1 000 000	100 000				
VAL DART II GRN 017	SOC DART II GRN 017C	100 000	100 000				
VAL DART II GRN 018	The interview was not done						
VAL/DART/IL/GRN 018	SOC DART II GRN 018	1 000 000					
VAL/DART/IL/GRN/020	SOC/DART/IL/GRN/020						
VAL/DART/IL/GRN 021	SOC DART II GRN 021	100 000	200 000				
VAL/DART/IL/GRN/025	SOC/DART/IL/GRN/025	400 000	100 000				
VAL DART II GRN 027	SOC DART II GRN 027	1 000 000	100 000				
VAL DART II GRN 031	SOC DART II GRN 031	1 000 000	100 000				
VAL DART II GRN 031	SOC DART II GRN 031	1 000 000	100 000				
VAL/DART/IL/GRN/034	SOC/DART/IL/GRN/034	6 000 000	450 000				
VAL/DART/IL/GRN/036	SOC/DART/IL/GRN/036	4 500 000	3 000 000				
VAL/DART/IL/GRN/038	SOC/DART/IL/GRN/038	600 000	100 000				
VAL/DART/IL/GRN/047	SOC/DART/IL/GRN/047	1 000 000	1 000 000				
VAL/DART/IL/GRN/063	SOC/DART/IL/GRN/063	1 000 000	1 000 000				
VAL/DART/IL/GRN/068	SOC/DART/IL/GRN/068	4 000 000	1 000 000				
VAL/DART/IL/GRN/069	SOC/DART/IL/GRN/069	100 000	1 000 000				

Legend
 Yes
 No/Not Informed

Annex 03 - Part 5

Data Bank related to Sections 04 and 05 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of Corresponding Property Survey	Code of this Socio-Economic Unit:	Average monthly sales	Average net monthly profit	Available Documentation	Bank Loans	Mortgage Inform value	Residential area within the property
VAL/DART/IL/GRN/074	SOC/DART/IL/GRN/074	690 000	360 000				
VAL/DART/IL/GRN/075	SOC/DART/IL/GRN/075	1 000 000	500 000				
VAL/DART/IL/GRN/077	SOC/DART/IL/GRN/077	1 000 000	500 000				
VAL/DART/IL/GRN/078	SOC/DART/IL/GRN/078	1 000 000	2 000 000				
VAL/DART/IL/GRN/081	SOC/DART/IL/GRN/081	150 000					
VAL/DART/IL/GRN/085	SOC/DART/IL/GRN/085	450 000	90 000				
VAL/DART/IL/GRN/091	SOC/DART/IL/GRN/091	1 000 000	500 000				
VAL/DART/IL/GRN/093	SOC/DART/IL/GRN/093	10 800 000	2 450 000				
VAL/DART/IL/GRN/097	SOC/DART/IL/GRN/097	300 000	150 000				
VAL/DART/IL/GRN/098	SOC/DART/IL/GRN/098	100 000					
VAL/DART/IL/GRN/100	SOC/DART/IL/GRN/100	1 000 000	500 000				
VAL/DART/IL/GRN/103	SOC/DART/IL/GRN/103	1 000 000	500 000				
VAL/DART/KN/HNF/001	The interview was not done						
VAL/DART/KN/HNF/002	The interview was not done						
VAL/DART/KN/HNF/003	SOC/DART/KN/HNF/003						
VAL/DART/KN/HNF/004	The interview was not done						
VAL/DART/KN/HNF/005	The interview was not done						
VAL/DART/KN/HNF/006	The interview was not done						
VAL/DART/KN/HNF/007	The interview was not done						
VAL/DART/KN/HNF/007	The interview was not done						

Legend
 Yes
 No/Not Informed

Annex 03 - Part 5
Data Bank related to Sections 04 and 05 of the Socio-Economic Survey - Commercial Activities Questionnaire

Code of Corresponding Property Survey	Code of this Socio-Economic Unit	Average monthly sales	Average net monthly profit	Available Documentation	Bank Loans	Mortgage Inform value	Residential area within the property
VAL DARI KN HNF 00X	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 00Y	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 00Z	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 001	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 002	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 003	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 004	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 005	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 006	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 007	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 008	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 009	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 010	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 011	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 012	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 013	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 014	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 015	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 016	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 017	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 018	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 019	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 020	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 021	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 022	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 023	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 024	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 025	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 026	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 027	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 028	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 029	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 030	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 031	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 032	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 033	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 034	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 035	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 036	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 037	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 038	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 039	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 040	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 041	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 042	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 043	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 044	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 045	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 046	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 047	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 048	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 049	000 DART KN HNF 000	1,000,000	1,000,000				
VAL DARI KN HNF 050	000 DART KN HNF 000	1,000,000	1,000,000				

Legend
 Yes
 No/Not Informed

Annex 03 - Part 5
Data Bank related to Sections 04 and 05 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of Corresponding Property Survey	Code of this Socio-Economic Unit	Average monthly sales	Average net monthly profit	Available Documentation	Bank Loans	Mortgage Inform value	Residential area within the property
VAL/DART/KN/KMR/005	SOC/DART/KN/KMR/005A	1 000 000	200 000				
VAL/DART/KN/KMR/006	SOC/DART/KN/KMR/006A	1 000 000	100 000				
VAL/DART/KN/KMR/006	SOC/DART/KN/KMR/006B	1 000 000	200 000				
VAL/DART/KN/KMR/007	SOC/DART/KN/KMR/007	1 000 000					
VAL/DART/KN/KMR/007	SOC/DART/KN/KMR/007A	1 000 000	100 000				
VAL/DART/KN/KMR/007	SOC/DART/KN/KMR/007B	1 000 000	400 000				
VAL/DART/KN/KMR/008	SOC/DART/KN/KMR/008	1 000 000	200 000				
VAL/DART/KN/KMR/008	SOC/DART/KN/KMR/008A	1 000 000	1 000 000				
VAL/DART/KN/KMR/009	SOC/DART/KN/KMR/009A	160 000	120 000				
VAL/DART/KN/KMR/009	SOC/DART/KN/KMR/009B	300 000	200 000				
VAL/DART/KN/KMR/009	SOC/DART/KN/KMR/009C	350 000	220 000				
VAL/DART/KN/KMR/009	SOC/DART/KN/KMR/009D	1 050 000	450 000				
VAL/DART/KN/KMR/009	SOC/DART/KN/KMR/009E	1 000 000	1 000 000				
VAL/DART/KN/KMR/009	SOC/DART/KN/KMR/009F	1 000 000	1 000 000				
VAL/DART/KN/KMR/009	SOC/DART/KN/KMR/009G	1 000 000	1 000 000				
VAL/DART/KN/KMR/011	SOC/DART/KN/KMR/011A	1 000 000	100 000				
VAL/DART/KN/KMR/012	SOC/DART/KN/KMR/012	1 000 000	1 000 000				
VAL/DART/KN/KMR/012	SOC/DART/KN/KMR/012A	1 000 000	100 000				
VAL/DART/KN/KMR/012	The interview was not done						
VAL/DART/KN/KMR/012	The interview was not done						
VAL/DART/KN/KMR/012	SOC/DART/KN/KMR/012B	1 000 000	100 000				
VAL/DART/KN/KMR/013	SOC/DART/KN/KMR/013	1 000 000	1 000 000				

Legend
 Yes
 No/Not Informed

Annex 03 - Part 5

Data Bank related to Section 04 and 05 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of Corresponding Property Survey	Code of this Socio-Economic Unit	Average monthly sales	Average net monthly profit	Available Documentation	Bank Loans	Mortgage Inform value	Residential area within the property
VAL/DART/KN/KMR/013	SOC/DART/KN/KMR/013	1 000 000	100 000				
VAL/DART/KN/KMR/013	SOC/DART/KN/KMR/013	1 000 000	200 000				
VAL/DART/KN/KMR/013	SOC/DART/KN/KMR/013	2 000 000	600 000				
VAL/DART/KN/KMR/013	SOC/DART/KN/KMR/013	1 000 000	70 000				
VAL/DART/KN/KMR/014	SOC/DART/KN/KMR/014						
VAL/DART/KN/KMR/014	SOC/DART/KN/KMR/014	1 000 000	200 000				
VAL/DART/KN/KMR/014	SOC/DART/KN/KMR/014	2 000 000	400 000				
VAL/DART/KN/KMR/014	SOC/DART/KN/KMR/014	1 000 000	100 000				
VAL/DART/KN/KMR/014	SOC/DART/KN/KMR/014	1 000 000	400 000				
VAL/DART/KN/KMR/014	SOC/DART/KN/KMR/014	10 000	10 000				
VAL/DART/KN/KMR/014	SOC/DART/KN/KMR/014	10 000 000	1 000 000				
VAL/DART/KN/KMR/015	SOC/DART/KN/KMR/015	4 000 000	600 000				
VAL/DART/KN/KMR/017	SOC/DART/KN/KMR/017	10 000 000	1 000 000				
VAL/DART/KN/KMR/017	SOC/DART/KN/KMR/017	1 000 000	400 000				
VAL/DART/KN/KMR/017	SOC/DART/KN/KMR/017	1 000 000	2 000 000				
VAL/DART/KN/KMR/020	SOC/DART/KN/KMR/020	120 000	40 000				
VAL/DART/KN/KMR/020	SOC/DART/KN/KMR/020A	1540000	704000				
VAL/DART/KN/UB2/021	The interview was not done						
VAL/DART/KN/KMR/023	SOC/DART/KN/KMR/023	1 000 000	2 000 000				
VAL/DART/KN/KMR/025	SOC/DART/KN/KMR/025	1 000 000	1 000 000				
VAL/DART/KN/KMR/026	SOC/DART/KN/KMR/026	1 000 000	2 000 000				
VAL/DART/KN/KMR/026	SOC/DART/KN/KMR/026	4 000 000	1 000 000				

Legend

<input type="checkbox"/>	Yes
<input type="checkbox"/>	Not/Not Informed

Annex 03 - Part 5

Data Bank related to Sections 04 and 05 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of Corresponding Property Survey	Code of this Socio-Economic Unit	Average monthly sales	Average net monthly profit	Available Documentation	Bank Loans	Mortgage Inform value	Residential area within the property
VAL/DART/KN/KMR/026	SOC/DART/KN/KMR/026	1 000 000	100 000				
VAL/DART/KN/KMR/027	SOC/DART/KN/KMR/027	1 000 000	100 000				
VAL/DART/KN/KMR/028	SOC/DART/KN/KMR/028	1 000 000	100 000				
VAL/DART/KN/KMR/029	SOC/DART/KN/KMR/029	2 400 000	300 000				
VAL/DART/KN/KMR/029	SOC/DART/KN/KMR/029A	450 000	270 000				
VAL/DART/KN/KMR/029	SOC/DART/KN/KMR/029B	1 000 000	100 000				
VAL/DART/KN/KMR/031	SOC/DART/KN/KMR/031	1 000 000	100 000				
VAL/DART/KN/KMR/033	The interview was not done						
VAL/DART/KN/KMR/032	The interview was not done						
VAL/DART/KN/KND/002	SOC/DART/KN/KND/002	1 000 000	100 000				
VAL/DART/KN/KND/004	SOC/DART/KN/KND/004						
VAL/DART/KN/MG2/001	SOC/DART/KN/MG2/001	1 000 000	100 000				
VAL/DART/KN/MG2/002	SOC/DART/KN/MG2/002	1 000 000	100 000				
VAL/DART/KN/MG2/002	SOC/DART/KN/MG2/002A						
VAL/DART/KN/MG2/002	SOC/DART/KN/MG2/002B						
VAL/DART/KN/MG2/002	SOC/DART/KN/MG2/002C						
VAL/DART/KN/MG2/002	SOC/DART/KN/MG2/002D	2 100 000	500 000				
VAL/DART/KN/MG2/002	The interview was not done						
VAL/DART/KN/MG2/002	The interview was not done						
VAL/DART/KN/MG2/002	SOC/DART/KN/MG2/002E						
VAL/DART/KN/MG2/003	SOC/DART/KN/MG2/003	160 000	100 000				
VAL/DART/KN/MG2/003	SOC/DART/KN/MG2/003	1 000 000	100 000				

Legend
 Yes
 Not Inform

Annex 03 - Part 5

Data Bank related to Sections 04 and 05 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of Corresponding Property Survey	Code of this Socio-Economic Unit.	Average monthly sales	Average net monthly profit	Available Documentation	Bank Loans	Mortgage Inform value	Residential area within the property
VAL/DART/KN/MG2/003	SOC/DART/KN/MG2/003B	1 000 000	1 000 000				
VAL/DART/KN/MG2/003	SOC/DART/KN/MG2/003A	4 000 000	0 000 000				
VAL/DART/KN/MG2/004	SOC/DART/KN/MG2/004A	11 000 000	2 000 000				
VAL/DART/KN/MG2/005	SOC/DART/KN/MG2/005	350 000	150 000				
VAL/DART/KN/MG2/005	SOC/DART/KN/MG2/005C	1 110 000	420 000				
VAL/DART/KN/MG2/005	The interview was not done						
VAL/DART/KN/MG2/005	SOC/DART/KN/MG2/005A	1 000 000	1 000 000				
VAL/DART/KN/MG2/006	SOC/DART/KN/MG2/006A		150 000				
VAL/DART/KN/MNY/001	SOC/DART/KN/MNY/001	2 13 000 000	10 000 000				
VAL/DART/KN/UB2/001	The interview was not done						
VAL/DART/KN/UB2/001	SOC/DART/KN/UB2/001	1 000 000	1 000 000				
VAL/DART/KN/UB2/002	The interview was not done						
VAL/DART/KN/UB2/003	The interview was not done						
VAL/DART/KN/UB2/003	SOC/DART/KN/UB2/003	7 000 000	0 000 000				
VAL/DART/KN/UB2/003	SOC/DART/KN/UB2/003A	7 000 000					
VAL/DART/KN/UB2/004	SOC/DART/KN/UB2/004	1 000 000	0 000 000				
VAL/DART/KN/UB2/005	SOC/DART/KN/UB2/005	1 000 000	0 000 000				
VAL/DART/KN/UB2/006	SOC/DART/KN/UB2/006	1 000 000	200 000				
VAL/DART/KN/UB2/007	SOC/DART/KN/UB2/007B	300 000					
VAL/DART/KN/UB2/008	SOC/DART/KN/UB2/008	4 000 000	400 000				
VAL/DART/KN/UB2/009	SOC/DART/KN/UB2/009	0 000 000	1 000 000				

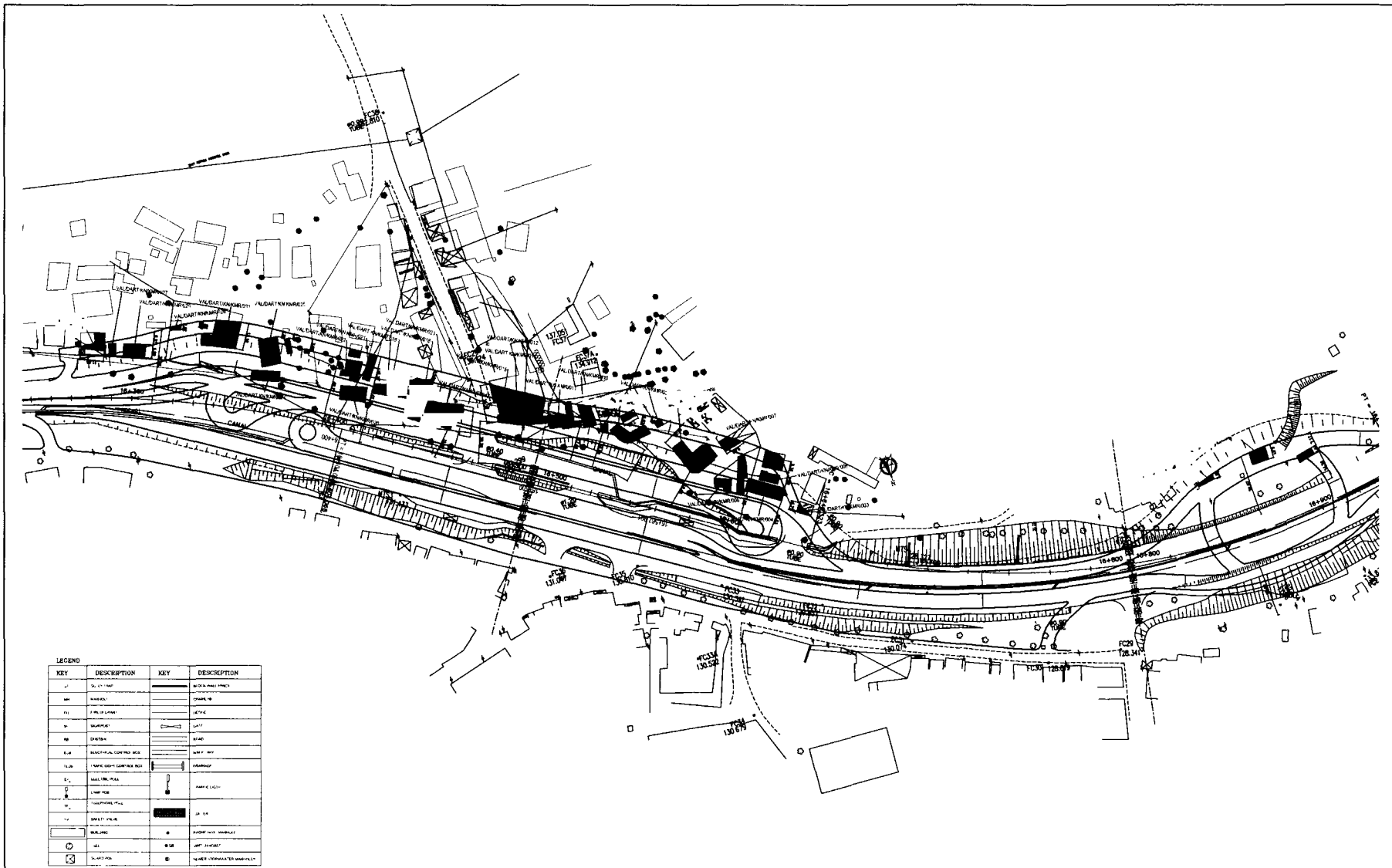
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 Not/Not Informed

Annex 03 - Part 5

Data Bank related to Sections 04 and 05 of the Socio-economic Survey - Commercial Activities Questionnaire

Code of Corresponding Property Survey	Code of this Socio-Economic Unit:	Average monthly sales	Average net monthly profit	Available Documentation	Bank Loans	Mortgage Inform value	Residential area within the property
VAL/DART/KN/UB2/010	SOC/DART/KN/UB2/010	1 000 000	500 000				
VAL/DART/KN/UB2/011	SOC/DART/KN/UB2/011	30 000	20 000				
VAL/DART/KN/UB2/012	SOC/DART/KN/UB2/012A	1 800 000	50 000				
VAL/DART/KN/UB2/013	SOC/DART/KN/UB2/013						
VAL/DART/KN/UB2/014	The interview was not done						
VAL/DART/KN/UB2/014	SOC/DART/KN/UB2/014	6 600 000	1 000 000				
VAL/DART/KN/UB2/015	The interview was not done						
VAL/DART/KN/UB2/016	SOC/DART/KN/UB2/016	1 000 000	100 000				
VAL/DART/KN/UB2/017	SOC/DART/KN/UB2/017	1 000 000	100 000				
VAL/DART/KN/UB2/021	SOC/DART/KN/UB2/021	2 000 000	200 000				
VAL/DART/KN/UB2/001	The interview was not done						
VAL/DART/KN/UB2/002	The interview was not done						
VAL/DART/KN/UB2/003	The interview was not done						
VAL/DART/KN/UB2/004	The interview was not done						

Legend
 Yes
 No/Not Informed



KEY	DESCRIPTION	KEY	DESCRIPTION
1	EXISTING WALL	10	EXISTING WALL
2	EXISTING FENCE	11	EXISTING FENCE
3	EXISTING ROAD	12	EXISTING ROAD
4	EXISTING CANAL	13	EXISTING CANAL
5	EXISTING DRAINAGE	14	EXISTING DRAINAGE
6	EXISTING UTILITY	15	EXISTING UTILITY
7	EXISTING POWER LINE	16	EXISTING POWER LINE
8	EXISTING TELEPHONE LINE	17	EXISTING TELEPHONE LINE
9	EXISTING WATER MAIN	18	EXISTING WATER MAIN
10	EXISTING SEWER MAIN	19	EXISTING SEWER MAIN
20	EXISTING ELECTRICAL	21	EXISTING ELECTRICAL
22	EXISTING TELEPHONE	23	EXISTING TELEPHONE
24	EXISTING WATER	25	EXISTING WATER
26	EXISTING SEWER	27	EXISTING SEWER
28	EXISTING GAS	29	EXISTING GAS
30	EXISTING CABLE	31	EXISTING CABLE

Client: **DAR ES SALAAM CITY COUNCIL**
 P O Box 9084, Dar es Salaam, Tanzania
 Tel: + 255-22-2123551/6
 Email: cd@dcc.go.tz

Consultant: **LOGIT**
 Logit Engineering Consultants Ltd
 P.O. Box 2222, Dar es Salaam, Tanzania
 Tel: +255 22 2123551/6
 Email: info@logit.co.tz

Inter-consult Ltd
 P.O. Box 1122, Dar es Salaam, Tanzania
 Tel: +255 22 2123551/6
 Email: info@interconsult.co.tz

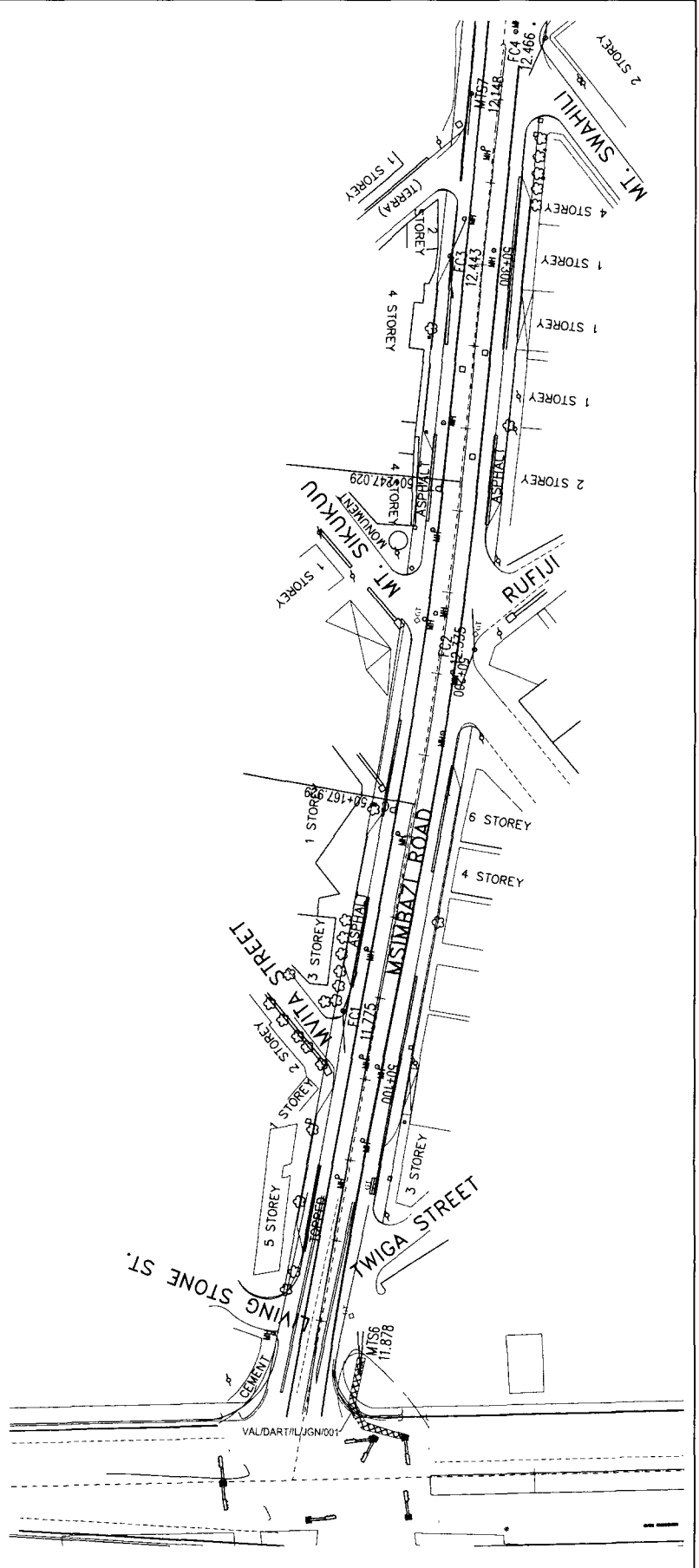
Client's approval

SENIOR ENGINEER

Revision Reference			
No.	Date	Description	Rev by

located by
 Surveyed by: G. Maliki
 Designed by: H. Shoo
 Drawn by: J. Uko
 Checked by: Z.T. Marege
 Approved by:
 Scale: 1:100

Project: **DAR RAPID TRANSIT (DART)**
 Subject: **LAND ACQUISITION IDENTIFICATION PLAN
 KIMARA TERMINAL**
 Date: June 2007
 Drg No: T301-CE-RD-LAP-17



KEY	DESCRIPTION	KEY	DESCRIPTION
1	Asphalt Road	1	1 STOREY
2	Cement Road	2	2 STOREY
3	Drainage	3	3 STOREY
4	Footpath	4	4 STOREY
5	Green Area	5	5 STOREY
6	Water Feature	6	6 STOREY
7	Boundary Wall	7	7 STOREY
8	Electricity	8	8 STOREY
9	Water	9	9 STOREY
10	Other	10	10 STOREY

DARES SALAAM CITY COUNCIL
 P O Box 9084, Dar es Salaam, Tanzania
 Tel: + 255-22-2123551/6
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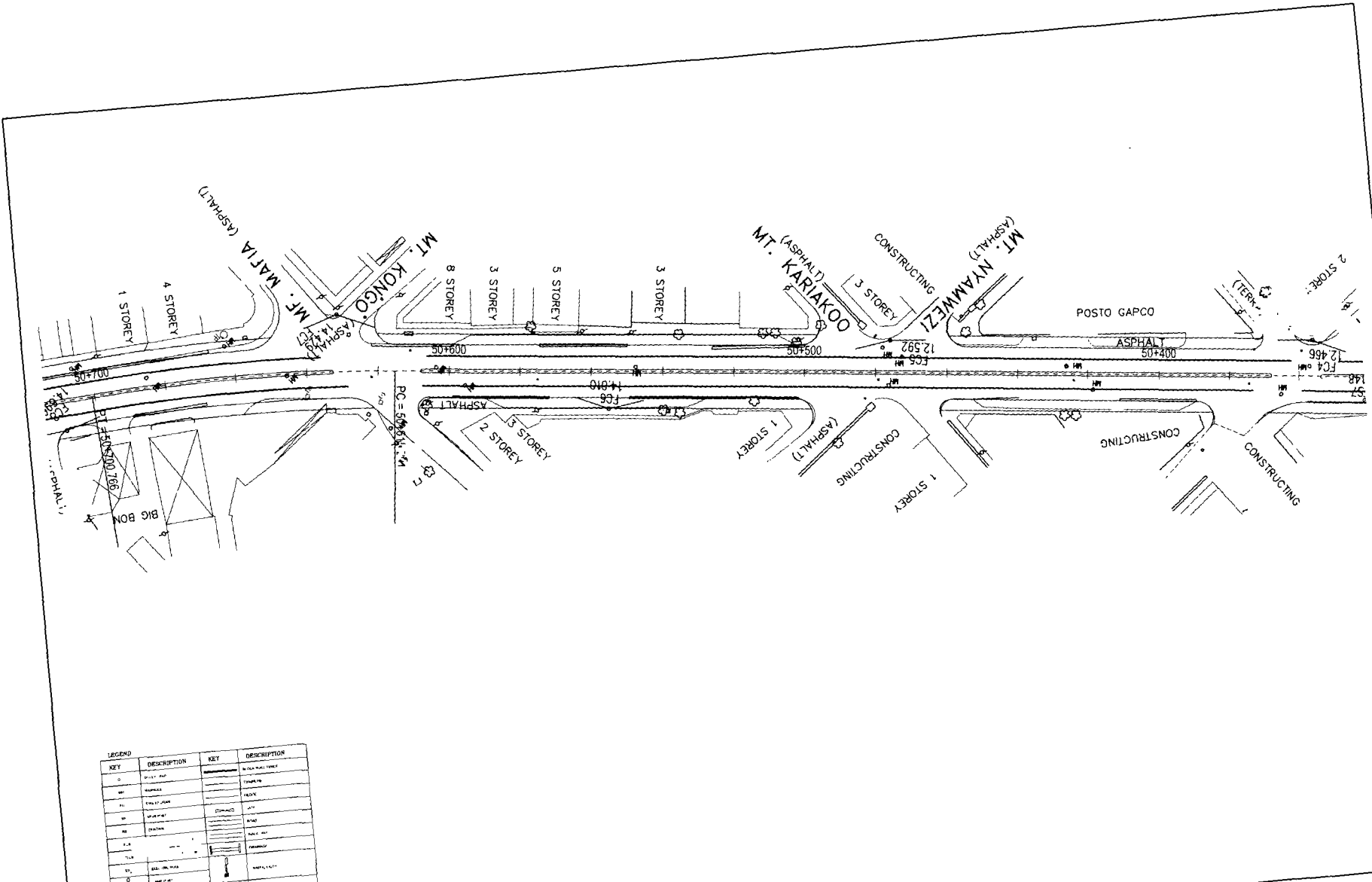
Consultant
LOGIT ... Rapid Consulting Ltd
Mt. Sikuu Consultants Ltd
 P.O. Box 1111, Dar es Salaam, Tanzania
 Tel: +255 22 2123551/6
 Email: cd@ddcc.go.tz

Client's approval
 SENIOR ENGINEER
 No Date

Revision Reference		Rev. by
No	Description	

Issued by: G. Makali
 Drawn by: H. Siro
 Checked by: Z. T. Mwangi
 Approved by: _____
 Scale: 1:100

Project: **DAR RAPID TRANSIT (DART)**
 Subject: LAND ACQUISITION IDENTIFICATION MAP
 Date: June 2007
 Dwg No: 1001/25-00/UP/20



LEGEND

KEY	DESCRIPTION	KEY	DESCRIPTION
○	BOUNDARY	□	ASPHALT
○	WATER	□	CONCRETE
○	ROAD	□	GRAVEL
○	RAILWAY	□	BRICK
○	POWER	□	STONE
○	SEWER	□	WOOD
○	TELEPHONE	□	GLASS
○	WALL	□	ROOF
○	DOOR	□	CEILING
○	WINDOW	□	FLOOR
○	STAIR	□	BASEMENT
○	PLANT	□	FOUNDATION
○	POLE	□	PILE
○	POST	□	ANCHOR
○	MARKER	□	CONCRETE PILE
○	WATER POINT	□	STEEL PILE
○	WATER TOWER	□	WOODEN PILE
○	WATER TANK	□	STONE PILE
○	WATER PIPE	□	BRICK PILE
○	WATER VALVE	□	GLASS PILE
○	WATER METER	□	WOODEN PILE
○	WATER PUMP	□	STONE PILE
○	WATER TREATMENT	□	BRICK PILE
○	WATER DISTRIBUTION	□	GLASS PILE
○	WATER COLLECTION	□	WOODEN PILE
○	WATER STORAGE	□	STONE PILE
○	WATER CONDUIT	□	BRICK PILE
○	WATER MAIN	□	GLASS PILE
○	WATER BRANCH	□	WOODEN PILE
○	WATER SERVICE	□	STONE PILE
○	WATER CONNECTION	□	BRICK PILE
○	WATER METER BOX	□	GLASS PILE
○	WATER VALVE BOX	□	WOODEN PILE
○	WATER PUMP BOX	□	STONE PILE
○	WATER TREATMENT BOX	□	BRICK PILE
○	WATER DISTRIBUTION BOX	□	GLASS PILE
○	WATER COLLECTION BOX	□	WOODEN PILE
○	WATER STORAGE BOX	□	STONE PILE
○	WATER CONDUIT BOX	□	BRICK PILE
○	WATER MAIN BOX	□	GLASS PILE
○	WATER BRANCH BOX	□	WOODEN PILE
○	WATER SERVICE BOX	□	STONE PILE
○	WATER CONNECTION BOX	□	BRICK PILE
○	WATER METER BOX	□	GLASS PILE
○	WATER VALVE BOX	□	WOODEN PILE
○	WATER PUMP BOX	□	STONE PILE
○	WATER TREATMENT BOX	□	BRICK PILE
○	WATER DISTRIBUTION BOX	□	GLASS PILE
○	WATER COLLECTION BOX	□	WOODEN PILE
○	WATER STORAGE BOX	□	STONE PILE
○	WATER CONDUIT BOX	□	BRICK PILE
○	WATER MAIN BOX	□	GLASS PILE
○	WATER BRANCH BOX	□	WOODEN PILE
○	WATER SERVICE BOX	□	STONE PILE
○	WATER CONNECTION BOX	□	BRICK PILE

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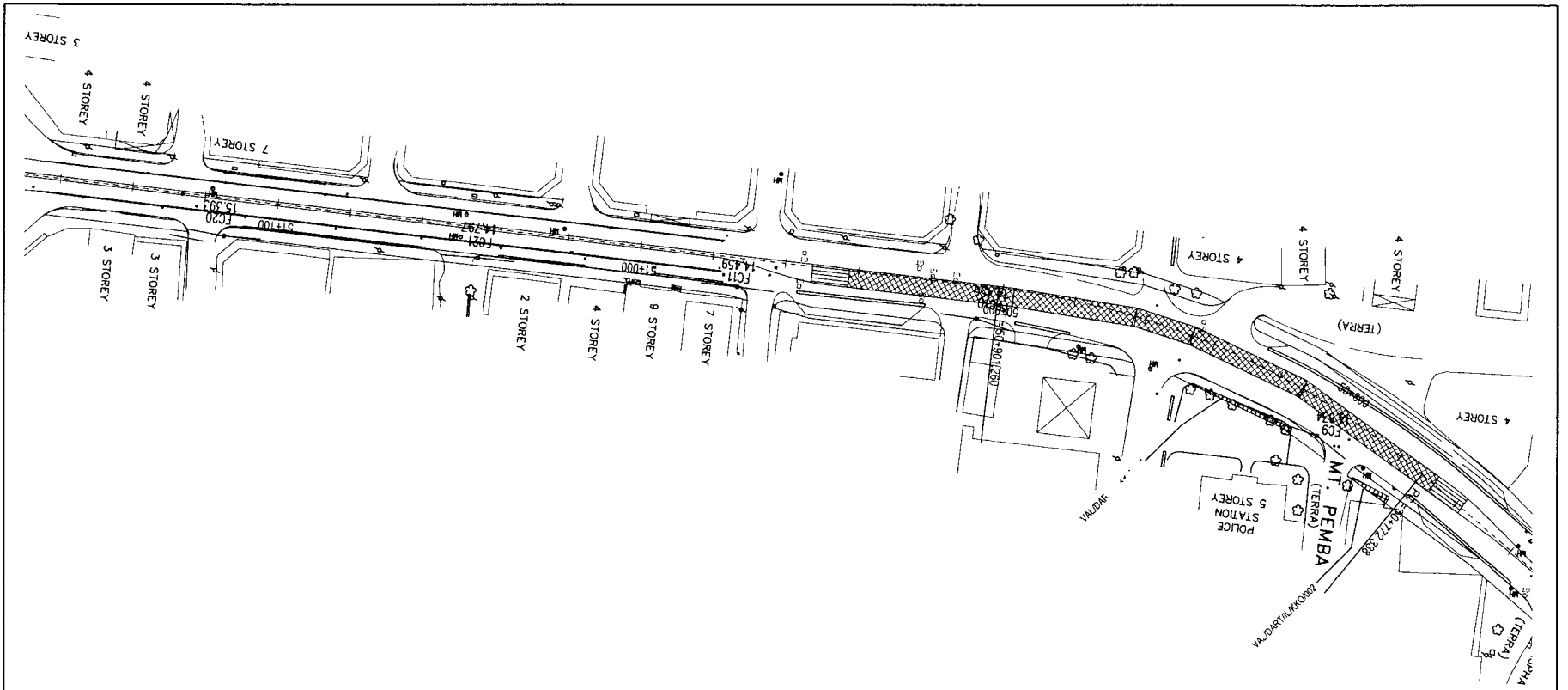


Client's approval
 SENIOR ENGINEER

Revision Reference:			
No.	Date	Description	Rev by

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Designed	H. Shoo
Drawn by	J. Gno
Checked by	Z.T. Mwangi
Approved by	
Scale	1:500

Project	DAR RAPID TRANSIT (DART)
Subject	LAND ACQUISITION IDENTIFICATION PLAN MSIMBAZI ROAD
Date	
Drng No	



LEGEND

KEY	DESCRIPTION	KEY	DESCRIPTION
0	EMPTY MAP		BOUNDARY
01	ROAD		ROAD
02	RAILWAY		RAILWAY
03	RAILWAY		RAILWAY
04	RAILWAY		RAILWAY
05	RAILWAY		RAILWAY
06	RAILWAY		RAILWAY
07	RAILWAY		RAILWAY
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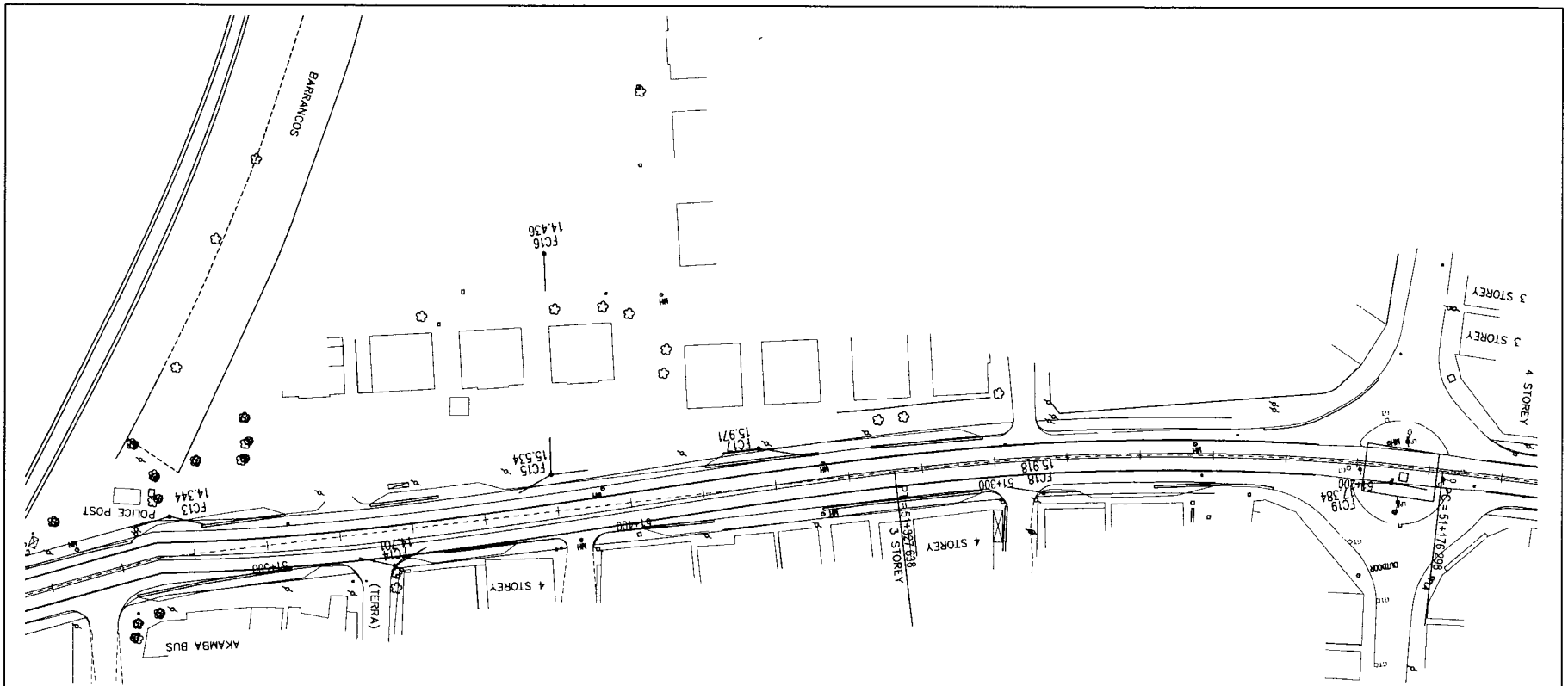
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Client's approval	Revision Reference			
	No	Date	Description	Rev by.
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Project		Date	
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Designed by	H. Shoo	Scale	1:1000
Drawn by	J. Uno		
Checked by	Z.T. Marege		
Approved by			



LEGEND

KEY	DESCRIPTION	KEY	DESCRIPTION
ST	STREET	—	BUILDING FOOTPRINT
WH	WALL	—	CHANGING
PL	PAVED AREA	—	ROAD
GR	GRAVEL	—	RAILWAY
BR	BURIED	—	RAILWAY
FC	FOOTCANDLE	—	RAILWAY
TR	TRANSFORMER	—	RAILWAY
EP	ELECTRIC PILE	—	RAILWAY
CP	CONCRETE PILE	—	RAILWAY
TP	TERRAZZO PILE	—	RAILWAY
TR	TERRAZZO	—	RAILWAY
—	RAILWAY	—	RAILWAY
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—	RAILWAY	—	RAILWAY

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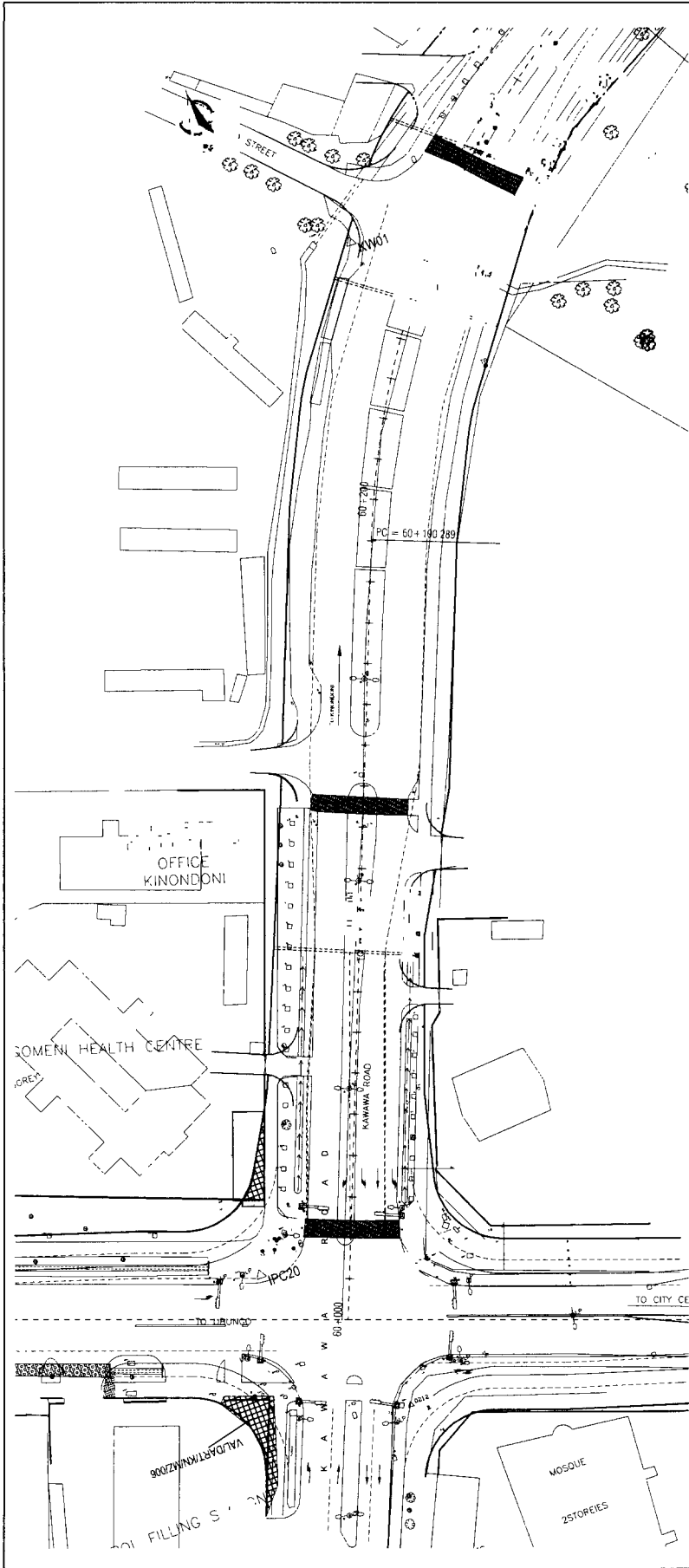
Client's approval
 SENIOR ENGINEER

Revision Reference	
No.	Date

Issued by:
 Surveyed by: G. Mafuku
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Project:
DAR RAPID TRANSIT (DART)
 Subject:
 LAND ACQUISITION IDENTIFICATION PLAN
 MSIMBAZI ROAD
 Date: June 2007
 Drg No.: T301-CE-RD-LAIP-23

Scale: 1:1000



NO	KEY	DESCRIPTION
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Client: DAR ES SALAAM CITY COUNCIL
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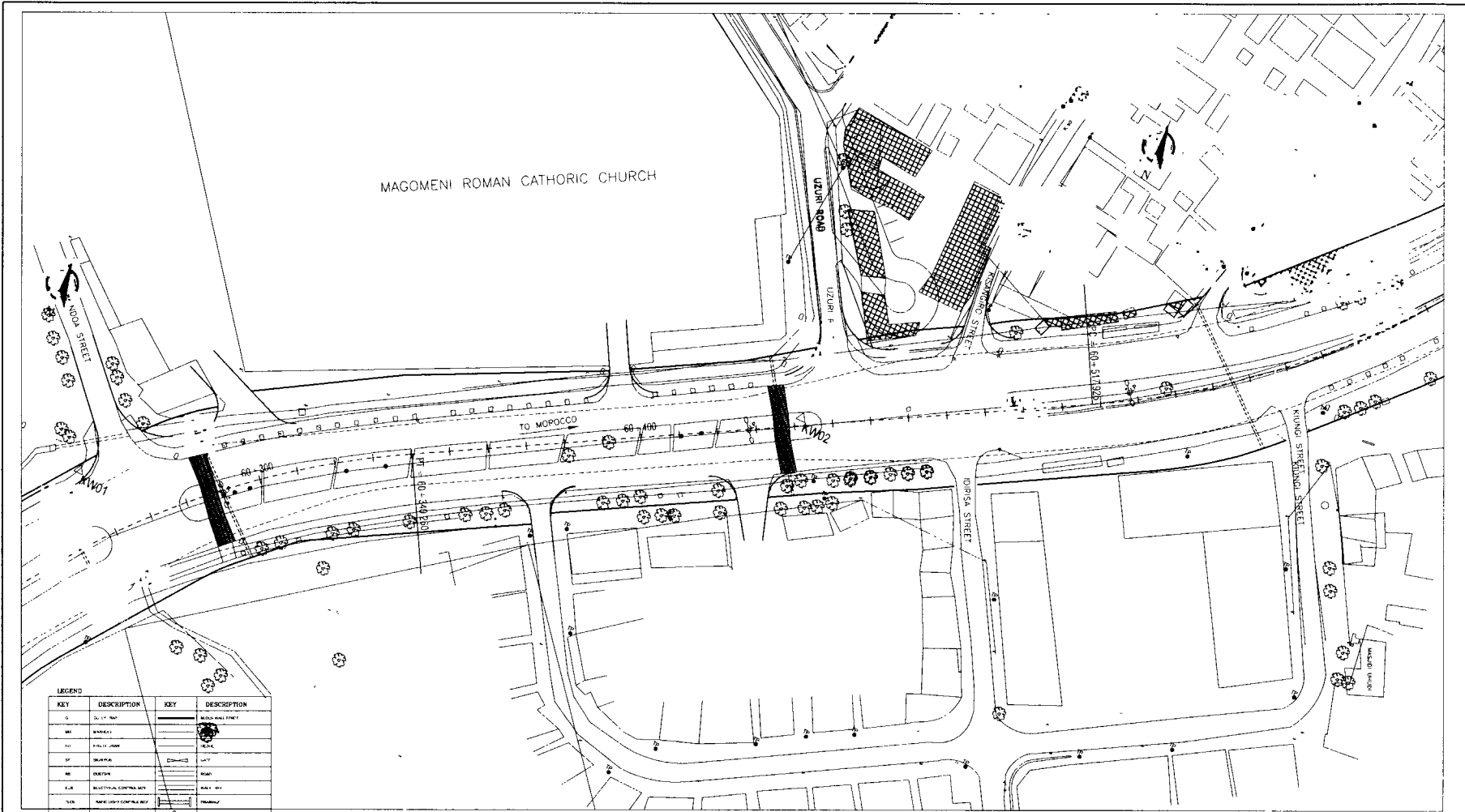
Consultant: LOGIT CONSULTANTS LTD
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Client's approval:

No	Date	Description

Revised by: G. Mshah, H. Siro, J. Lira, Z.T. Mwanjiru
Checked by: Z.T. Mwanjiru
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Project: DAR RAPID TRANSIT (DART)
Subject: LAND ACQUISITION IDENTIFICATION PLAN
KAWAWA ROAD
Date: June 2007
Dwg No: TSP/C2-RD/LAF/25



LEGEND

KEY	DESCRIPTION	KEY	DESCRIPTION
0	4.5M X 6M	---	ROAD BOUNDARY
100	WATER	---	RAIL
101	POWER	---	RAIL
102	SEWER	---	RAIL
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119	TELEPHONE	---	RAIL
120	TELEPHONE	---	RAIL

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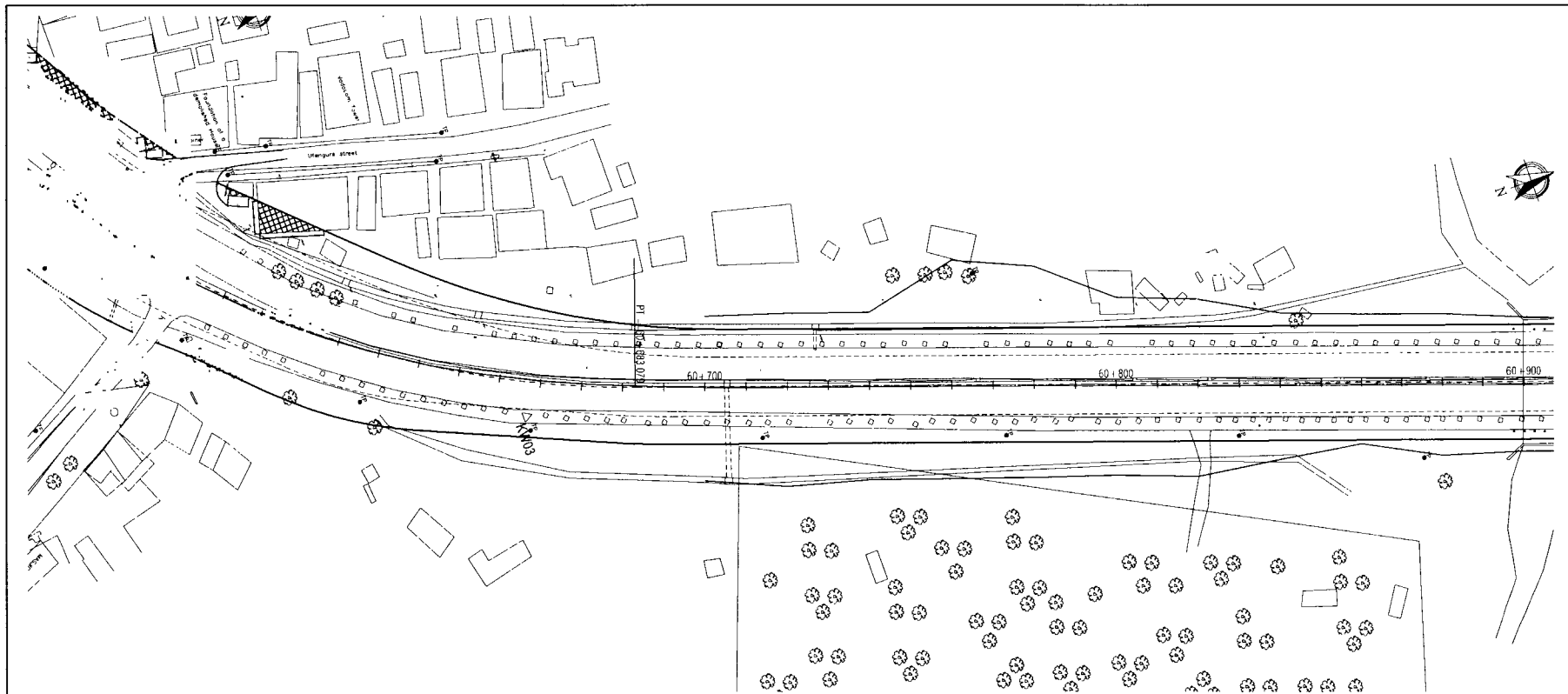
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Revision Reference:

No	Date	Description	Rev by

located by
 Surveyed by: G. Makini
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Project: **DAR RAPID TRANSIT (DART)**
 Subject: **LAND ACQUISITION IDENTIFICATION PLAN
 KAWAWA ROAD**
 Date: June 2007
 Drg No: T301-GE-RD-LAP-26 REV



KEY	DESCRIPTION	KEY	DESCRIPTION
1	TOILET	1	RECURRING TRAFFIC
2	WATER	2	TRAFFIC
3	PAVED ROAD	3	FIELD
4	UNPAVED ROAD	4	LAKE
5	GRAVEL	5	ROAD
6	RECURRING TRAFFIC	6	RAILWAY
7	TRAFFIC	7	STATION
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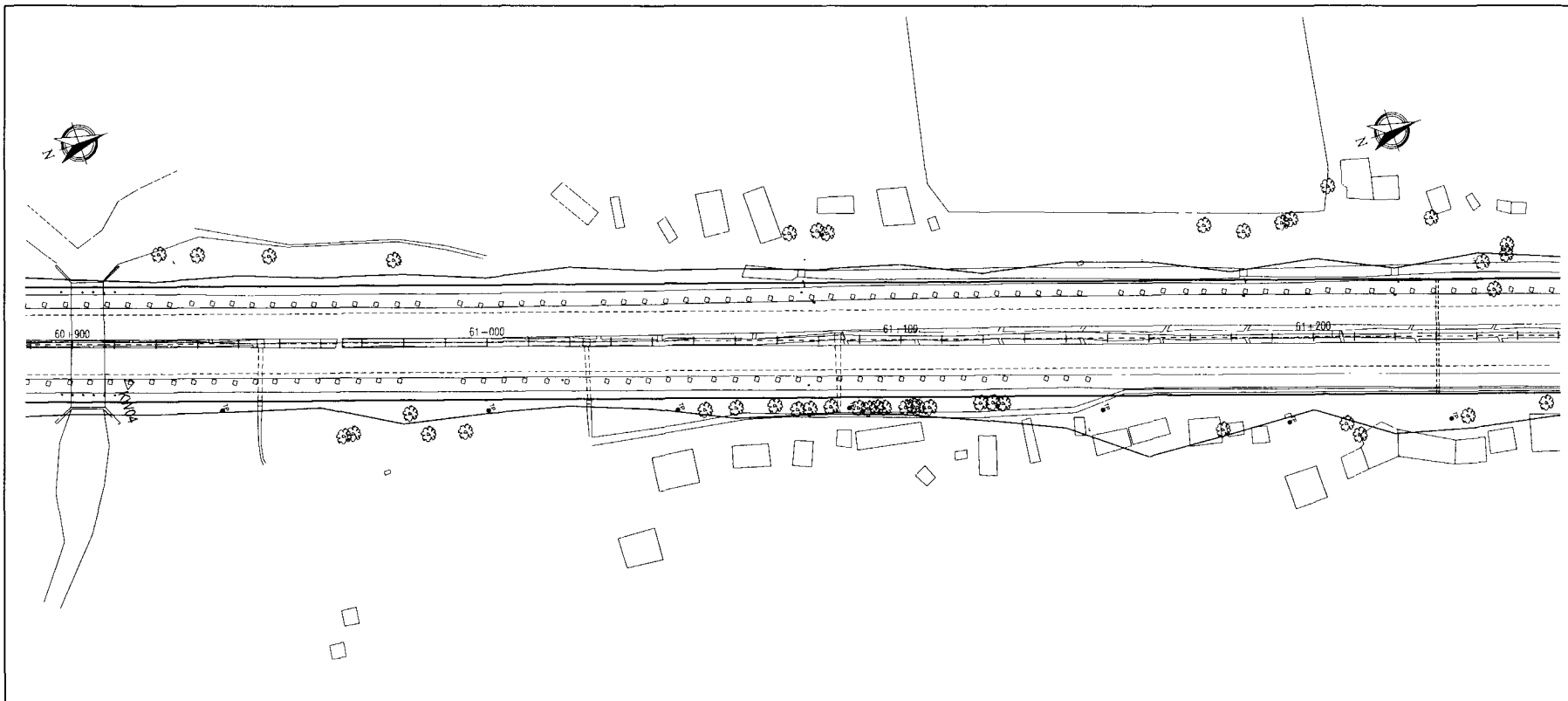
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Client's approval	Revision Reference.			Issued by	Project
SENIOR ENGINEER	No	Date	Description	Rev by	DAR RAPID TRANSIT (DART)
					Subject
					LAND ACQUISITION IDENTIFICATION PLAN KAWAWA ROAD
					Date
					June 2007
					Scale
					1:1000
					Drng No
					T301-GE-RD-LAP-27
					REV



LEGEND

KEY	DESCRIPTION	KEY	DESCRIPTION
1	GRAVEL PAD	10	RAILWAY TRACK
2	ROADSIDE	11	RAILWAY TRACK
3	PAVED SURFACE	12	RAILWAY TRACK
4	CONCRETE	13	RAILWAY TRACK
5	ASPHALT	14	RAILWAY TRACK
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31	RAILWAY TRACK	40	RAILWAY TRACK

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Client's approval	Revision Reference			
SENIOR ENGINEER	No.	Date	Description	Rev by

located by		Project	
Surveyed by	G. Mafuku	DAR RAPID TRANSIT (DART)	
Designed	H. Siroo	Subject	
Drawn by	J. Uko	LAND ACQUISITION IDENTIFICATION PLAN KAWAHA ROAD	
Checked by	Z.T. Marege	Date	
Approved by		June 2007	
Scale	1:100	Orig No	T304-CE-RD-LAP-28

ANNEX 05

Social Impact Assessment included in the ESIA

ANNEX 05

Socio-Economic Impacts Identified in the Environmental and Social Impact Assessment

Potential Impacts During Construction:

I.7 Potential impacts on road infrastructure and traffic:

7.01 General increase in traffic congestions during construction

All of Phase 1 of the BRT system will be implemented simultaneously, within a two year construction schedule. This will imply in significant limitation of capacity along the alignment, requiring provisional rearrangement of traffic and associated disturbances to pedestrians and adjacent land use. Due to limited extension of Dar Es Salaam's road network, provisional detours are not feasible. Thus, traffic management procedures during construction will have to be restrained to the project's Directly Affected Area (DAA).

Given the importance of the selected corridors, it is likely that impacts on traffic congestion will be widespread and will affect a significant part of Dar Es Salaam's main avenues during most of the construction period.

Though intense, this is a provisional impact that will cease once the project is in operation.

7.02 Temporary alterations in the local patterns of traffic circulation

The implementation process of DART (Phase 1) will require a series of provisional changes in the local standard of traffic distribution.

The change and/or strengthening of pavements will imply a partial interruption of traffic on the roads directly affected, compelling the temporary relocation of traffic within the DAA limits. In addition to the activities of demolition and paving on the BRT roads, the cross street traffic will be affected by the adjustments specified for the intersections. The project implementation will also compel traffic relocation on the Central Business District (CBD) road network, as per described in the "Programme for Traffic Management During Construction" (Section 6.5, P.05) and detailed in Annex 10.

As with the previous modifications, despite the intensity of changes in the patterns of road traffic, these changes shall be temporary, ceasing after the end of works. The exception is the CBD, where a new permanent scheme of traffic will be implemented at the operational phase (see Section 2.6).

7.03 Use of local street network by construction vehicles

During construction, there will be heavy duty vehicle traffic along the directly affected roads and in the rest of the network. It will be more intense on DART trunk roads because the local network lacks sufficient roads or expressways. Consequently, the intensity of impact 7.01 (increase in traffic congestions) will tend to rise.

This will also affect minor roads which connect the working faces to the support areas (send-off, borrow pit areas) and to the asphalt and concrete mixing plants. Nonetheless, in such cases, the impact should be less intense due to the lower traffic volume on those roads.

The usage of main roads and secondary routes by heavy duty vehicles will tend to decrease as demolition and paving works come to an end.

7.04 Local street pavement deterioration due to construction related traffic

The traffic of heavy vehicles on duty at the construction site will cause the wear and tear of the road pavements, especially those assigned to the implementation of DART Phase 1.

Such impact, also of a temporary nature, will be reverted after the end of works, given that new concrete pavements will be built on the segregated bus lanes, and the asphalt pavements will be rehabilitated / resurfaced. Nevertheless, the traffic of heavy vehicles on local roads may increase the deficiencies currently observed, since most part of the local road network, with the exception of the CBD roads and the major city links, is not paved and presents no drainage system.

7.05 Temporary disturbance of pedestrian circulation patterns

During the construction phase, there will be temporary interferences in the pedestrian circulation patterns, mostly on the roads directly affected and at the crossroads with the main local roads. Both the cross and parallel routes will be altered due to lane interruption, relocation of bus stops and the implementation of physical barriers (boarding, etc), as well as the implementation of provisional footbridges.

I.8 Potential impacts on population's quality of life:

8.01 Interference with local traffic during construction

During the construction works it will not be possible to divert the traffic on the avenues to other road sections, given the scarcity of alternative routes. Hence, in order to enable the occupation of part of the right-of-way by the construction teams without transferring the traffic to other sections, it will be necessary to stagger the relocation of traffic lanes.

This process, needless to say, shall cause a disruption to the users of the affected avenues, including freight vehicles, passenger vehicles and daladalas.

In the case of the Msimbazi Street branch, the disruption shall be particularly strong as a function of the relatively narrow right-of-way and of the continuous commercial activity along the link. Alternative roads will be inevitably used, affecting the conditions of traffic and noise therein.

A similar situation is expected along the Morogoro Road section within the CBD. In contrast, the problems expected on Kivukoni Road and Sokoine Drive, also in the central area, will be smaller, due to less intense traffic along these links.

Along the remaining parts of Morogoro Road the right-of-way is wider and the relocation should be possible with reduced impact on the road capacity during construction. On the other hand, on the Kawawa Road branch the available spaces are a little smaller.

8.02 Project induced social unrest during planning and construction

During the pre-construction stage the project disclosure may generate a substantial mobilization of the population employed or living in the areas directly affected by the roadworks.

Indeed, there will be concern and enquiries by the owners of properties to be expropriated, people living in zones prone to direct effect, owners and employees of shops and stores located along the corridor and other people whose activities and/or daily trips will be directly affected.

The mobilization of all these stakeholders, both for and against, shall extend until the construction stages, particularly the period of negotiation of indemnities and other complementary measures included in the Resettlement Action Plan for the present project.

8.03 Direct and indirect employment generation

It is expected that the works will last for twenty-four months with a workforce of roughly 450 during the peak season. This peak period is estimated to last approximately eight months. Out of the total staff, 70% will be direct and 30% indirect workforce. It is also estimated that up to 20% of the job positions may be filled by expatriate workers, coming from the country of origin of the company that wins the bid for the works.

In spite of the fact that the positive impact will cease after the end of works, it will provide a direct benefit to the families of all engaged workers.

8.04 Loss of employment in adjacent commercial activity during construction

The businesses facing the corridor shall be negatively affected during the construction, especially when the roadworks impose access difficulties.

If the reduction in their income level is significant, it may produce the dismissal of some workers. In the case interferences produced by the roadworks compel the temporary closure of a business, the dismissal of all workers will be a possibility.

The Socio-economic Census developed during the elaboration of the Resettlement Action Plan (RAP) Part A has identified a total of 70 to 75 workers in the business organizations to be partially or fully expropriated. It is estimated, based on the current status of the Socio-economic Census, Part B, that the number of employees in the businesses directly affected in this section of the alignment will be around 40 to 50 (preliminary research).

It must be emphasized that this number does not include workers of businesses provisionally affected during the construction and therefore not subject to expropriation (that is, not covered by the Socio-economic Census). In fact, the number of workers that may be affected by this impact may be considerably higher.

8.05 Risk of construction related accidents

At the construction phase the activities that represent accident risks for workers at the contractors' service (occupational risks) are as follows:

- Work at high traffic volume areas (risk of pedestrian casualty);
- Vehicle driving (collision risk);
- Transportation, movement and handling of materials and inputs;

- Passenger transport;
- Transportation, loading and unloading of explosives;
- Transportation, loading and unloading of dangerous products (fuel, oil, etc.);
- Warehousing and handling of fuel and flammable materials;
- Operation of heavy machines and equipment;
- Demolition of pavements and structures;
- Excavation works;
- Concreting Works (shaping, reinforcement, concreting, form stripping);
- Works at high places (relocation of air utilities, construction of bridges);
- Tree cutting;
- Works with the risk of electric shock (operation of electrical appliances and equipment, relocation of power distribution posts).

Apart from the occupational risks, the construction will also bring about accident risks for third parties, namely drivers and pedestrians on the move at DAA, such as: pedestrian casualties, vehicle collisions, falling into pavement holes, electric shock and other associated risks.

8.06 Increased noise and vibration levels along construction fronts and routes to support areas

The population who lives or works in places next to areas directly affected by the project will experience an increase in the levels of noise and vibration during the construction.

This impact will be stronger during the initial stages of works, when the activities of demolition of pavements and structures, relocation of interferences and transportation of rubble as far as the send-off area will require the use of heavy equipment and vehicles. It must be noted, though, that the roads subject to the implementation of DART at phase 1 already present high traffic volume, so that the current levels of noise and vibration at DAA are already significant.

The intensity of this impact shall vary according to the degree of severity or sensitivity of those affected. In general terms, the most sensitive recipients to noise are schools, hospitals and residential areas.

Along Kawawa Road the existing schools are not located next to the road alignment, so that the impact shall not be significant. Otherwise, in the Morogoro Road section between Kawawa Road and Kagera Street there are around half a dozen schools and one hospital bordering on the road which may be affected by such activity. There is also a hospital very close to the area planned for the location of Ubungo Terminal.

The increase in the noise and vibration levels also tends to occur in the case of sections far from the urban area, which will be used by trucks for the transportation of materials between the support areas and the working faces. On the other hand, the amount of critical recipients will be, in such cases, substantially smaller.

8.07 Public utility service interruptions during construction

During the construction phase, some disturbances to the supply of some public services may occur, sometimes due to the need to completely relocate the services, and in other cases because of pavement improvement work or similar direct intervention where the services are located.

The companies whose utilities supply may be affected are: TTCL (public telephone), TANESCO (power supply), DAWASCO (water supply) and DAWASA (sewage).

Though services interruption cannot be avoided in some cases, the duration of interruptions is expected to be minimal since any relocation work will be completed before removal of the existing utility lines.

8.08 Expropriation

The necessities of expropriation along the corridor and complementary roadworks are distributed unevenly along the alignment. On the whole, the geometry of the corridor design requires expropriation, mainly at the stations, where the cross section needs to be wider.

The following table synthesizes the information on the need of expropriating for Parts A and B of DART Phase 1, specifying the cases of partial and full expropriation.

Table 6.4.1.a
Number of Affected Properties

Types of Property	Phase I – Part A (Final)	Phase I – Part B (Estimate)
Residences to be totally affected	9	69
Residences to be partially affected	5	0
Commercial establishments affected within residences	11	21
Totally affected independent commercial establishments	7	21
Partially affected independent commercial establishments	68	2
Other affected buildings	12	1
TOTAL AFFECTED PROPERTIES	112	114

It is expected that a total of 226 properties will be affected, out of which 75 will be partially affected. Nevertheless, the most affected types of use will be the businesses which develop in front of dwellings, facing the right-of-way

Further details on the necessary expropriations are found in the Resettlement Action Plan, Parts A and B.

8.09 Resettlement of families

As described in the previous impact (8.09), from the 226 properties to be expropriated, 138 will be thoroughly affected. Out of these, 110 present residential use.

In the case of the residential properties to be partially affected, it will be possible to relocate them within their same building lots, through indemnities for the construction of additional rooms in compensation for the areas to be demolished.

Concerning the incident legislation (Section 5.2), it is inferred that the dwellers to be resettled shall fit basically into the following types of situation:

- Owners-dwellers who occupy the land regularly, and who will receive a compensation which amounts to the market value of the lot and to the restitution cost of the construction, with the possibility to use it in order to acquire an alternative property.
- Dwellers on land irregularly occupied, whose compensation will be limited to the reconstruction costs only, subject to special assistance in the respective Resettlement Action Plan.
- Tenant-dwellers who shall be resettled and compensated for the loss of accommodation as provided by the Resettlement Action Plan.

1.9 Potential impacts on economical activities:

9.01 Displacement of economic activities along the corridor

The project implementation shall result in the displacement of some commercial activities to alternative places. The activities to be displaced may be divided in two major groups:

- Activities displaced owing to expropriation
- Activities displaced as a function of the indirect effects of the project

In the first case, based on the Census of properties for Part A (concluded) and Part B (in progress) it is estimated that the number of businesses to be fully expropriated will be around 60, with 18 businesses in Part A and 42 in Part B.

It should be considered, nonetheless, that some business owners whose useful area is reduced by the expropriation are likely to choose the relocation to more spacious places.

The displacement of economic activities by the indirect effects of the corridor implementation will happen in many different ways.

As observed in the land use survey of the Directly Affected Area (Section 4.4.1), many businesses bordering the right-of-way use the inner space of this latter. Among these, pubs and restaurants place tables and chairs on the pavement or in lateral streets, especially in shaded areas, as well as other establishments that use public space for merchandise exhibition or even as a product stock area. In this group are included furniture stores, carpentry workshops, building material stores, used car (on-street parking) shops, amongst others.

With corridor implementation, which will occupy most of the right of way, eliminating the possibility of informal occupation, it is probable that most of these establishment owners choose to move out to other road links.

Other economic activities that use the right of way include businesses that rely on kiosks or other permanent facilities which remain on the streets at night.

In this group are mostly sellers of tree seedlings and ornamental plants, which are concentrated in Kimara and in the surroundings of Ubungo Terminal. In addition to these, there are many street vendors scattered around commercial concentration spots along the corridor alignment. Their number is presently smaller since the municipality has decided to relocate this kind of seller from the streets of Dar es Salaam to specially designated markets.

Moreover, along the corridor there are many taxi ranks and daladala parking spaces spread over the empty spaces in the right of way. There are also some parking spaces for construction lorries where the population goes to hire transport services..

All these economic activities shall be displaced as a consequence of the physical occupation of the project right of way.

Lastly, it must be considered that, in some sections of the corridor, where the available road capacity for general traffic will be reduced and/or the on-street parking will be removed, the commercial establishments shall experience a reduction in the business turnover, and this may, in some cases, bring about the decision to move out. It is estimated that this risk is concentrated mainly along the Msimbazi Street branch.

9.02 Reduction of the intensity of commercial activity along the DART during construction

Owing to traffic interruption on DART network, the shops and stores facing the alignments may be affected by the project. In the most critical situations, difficulties in the accessibility may compromise the turnover of clients, implying a reduction of sales volumes and / or services, including a reduction in the staff of such business organizations.

Despite being a temporary impact, it will demand monitoring and specific programmes aimed at the support for the local business, during the construction phase.

I.10 Potential impacts on physical and social infrastructure:

10.01 Disturbance and/or relocation of public utilities

As outlined in the description of impact 8.08, there may be interferences on air and underground utility networks during the construction phase.

All interferences shall be identified and listed in a physical inventory during the executive project phase. The entrepreneur will contact the concessionaire companies responsible for planning the technical procedures related to the relocation and the interruption of supply to public services. The relocation should be adjusted to the construction schedule.

I.11 Potential impacts on public finances:

11.01 Appropriation of part of the public sector's investment capacity

The DART implementation will be funded as follows: 70% of the total investment will be financed by the World Bank, in the form of grants, and 30% by the government of the United Republic of Tanzania.

As a result of such sums of investment, the government will pledge part of its revenue to the public transport sector, reducing, therefore, the availability of resources for investment in other sectors, as for example, health, education, etc.

Although significant, the amount to be invested by the government will be small as compared to the total resources necessary for the implementation of Phase 1. It should be considered, still, the direct and indirect fiscal benefits which will derive, respectively, from the construction and operation of the BRT system.

11.02 Increase in tax revenue during corridor construction

The implementation of DART Phase 1 will demand an investment in the order of US\$ 110 million. Tax revenues will be generated from the construction contracts won by the building contractors.

Complementary tax revenues will come out of the local acquisition of building materials, goods and services.

I.12 Potential impacts on architectural monuments and cultural property:

12.01 Disturbance of architectonic landmarks and other cultural property

The construction phase will directly affect the access to some important buildings and cultural landmarks of Dar Es Salaam. The main buildings to be affected are the many religious buildings located along the entire corridor, but some institutional buildings that will also be directly affected during this period.

The significant areas and/or buildings to be affected during the construction phase are listed in the Section 6.3 (Impact Receptors - Component C.3.08).

Potential Impacts during Operation:

I.16 Potential impacts on road infrastructure and traffic:

16.01 Permanent alteration of street capacities and local patterns of traffic distribution

With the implementation of exclusive rapid bus lanes along the DART corridors and the rationalization of the public transport system, there will be changes in the street capacities and local patterns of traffic distribution in the DAI.

The ideal standard cross sections as per defined in the project result in total width of 46.5 m along the corridor and 55 m at bus stations. However, as mentioned before in Section 2.1.1, this space is not always available and several design compromises have been adopted along the corridor alignment.

At present, it is observed that in the section of Morogoro Road from Kawawa Road to Ubungo, where the implementation of the ideal standard section of DART is forecasted, the carriageway lacks no physical capacity. The main cause of congestion on this road are the traffic lights at the crossroads. As they operate with four phases, they work as bottlenecks, compromising the traffic flow. In order to solve this problem, the project recommends the readjustment of intersections through the replacement with two-phase traffic lights and the ban on right turns. In addition, it will be made geometrical adjustments at the intersections, and some barriers will be placed at the end of a few local streets, in a way to drive the traffic flow to U-turns schemes. The segregation of bus lanes will also benefit the traffic reorganization as a whole along this road.

On the other hand, the central section of Morogoro Road, between City Council Station and Bibititi Road, presents a very restricted right-of-way. For this reason, the traffic will be limited exclusively to buses, cyclists and pedestrians. In Section 2 of Morogoro Road, between Bibititi and the United Nations, despite being wider than in Section 1, the right-of-way is still narrow, resulting in a less generous standard-section.

The most critical situation in terms of road capacity is found on Msimbazi Street, a narrow road downtown, with intense business activity. In this case, parking along the kerb will be forbidden, with a view to freeing one lane for the BRT and another for the general traffic.

In other important roads, currently used by many daladala lines, the substitution for micro-bus feeder services and the readjustment of intersections, among other road improvements, will provide capacity gains.

Specifically at the CBD, there is a plan for the reorganization of traffic by means of the assignment of one-way traffic throughout the road network, presently used in both directions.

16.02 Reduction in the availability of external parking spaces

Implementation of the corridor will displace various types of informal uses of the selected rights-of-way. One of the most significant uses to be displaced are parking spaces.

On more densely occupied segments of the Central Business District, implementation of the BRT will reduce already scarce street parking spaces. This will be particularly critical along Msimbazi Street branch where the customers of many of the existing commercial establishments make intensive use of available front-of-shop parking spots.

Along Morogoro and Kawawa Road where the right-of-way is significantly wider, left-over spaces are frequently occupied by taxi spots, daladala resting areas or construction truck parking spots which are known by the population that hires these trucks for construction, moving or other services (see Section 4.4.1). These uses of the right-of-way will no longer be feasible and new parking locations will need to be found.

This impact will affect mostly commercial establishments that depend on availability of parking, taxi drivers, daladala operators and truck owners to be displaced, and customers / users of the services provided by them or by the affected commercial establishments. However, only in the case of affected commercial establishments can this impact be considered significant and long lasting. In the other cases it is mostly temporary and will cease as soon as adequate alternative locations are defined.

16.03 Permanent restriction to transversal flows of pedestrians

The implementation of the corridor will entail some changes to the pedestrians' transversal flow, mainly because of the streets cross-sections reconfiguration.

The construction of the segregated lanes, plus large stations and terminals along the road can create a visual and physical block, restricting pedestrian crossing in some areas. This can produce impacts mainly in areas of intense circulation of people such as Msimbazi Street, and some parts of the city center.

Along the Morogoro Road, this impact will not be major, since along the first stretch, from City Council to Bibititi Rd, the road will be one-level, and there will be no mixed traffic, and along the second stretch, from Bibititi Rd up to Kimara, the corridor already has the characteristics of a wide road.

Along Kawawa Rd, the existing pedestrian crossing will be reorganized, according to the new signalling and intersections design, and these impacts will occur mainly at stations and the terminal.

Another feature of pedestrian flow that will change is that, today, in many points of the Morogoro Road, the drainage channels are located so as to discipline street crossing. After the implementation of the project, many of these channels will be closed, in order to accommodate the cycling paths and larger sidewalks, and, although crossing will not be allowed, the possibility of crossing out of the recommended areas might increase.

At the main point of circulation, the project proposes the construction of three pedestrian overpasses, providing access to the terminals and feeder stations and also allowing crossing of the road. These overpasses will be accessed by ramps, complying with all the requisites of universal accessibility. The proposed overpasses are:

Ubungo – at the proposed Ubungo Terminal
Kimara – at the proposed Kimara Terminal
Morocco - at the proposed Morocco Terminal

At the other Terminals and Stations, the project calls for bumps, signalling and zebra crossings in order to discipline pedestrian flows.

16.04 Changes in traffic level of streets affected by “daladala” route rationalization

With the beginning of the BRT system operation, many of the existing daladala lines will become extinct, while others will be maintained or relocated, as per the proposed scheme for the rationalization of public transport. Such initiative will promote changes in the traffic volumes on the affected roads.

On the roads where the daladalas operate with considerable demand for passengers, the lines will be replaced by micro-bus feeders, vehicles whose capacity is larger than that of the former. On these roads, the impact on the traffic volumes will be positive, because there will be a reduction in the number of vehicles, and subsequently, less demand for the respective road capacity.

At the Central Business District there is a plan for a complete relocation of daladala lines presently in operation, with the extinction of some and route changes in others. In this case, some roads may experience an increase in the traffic volume due to the concentration of lines. Yet, this will happen only on roads whose capacity can hold such increase, without any damage to traffic flow.

On the remaining road network in town where daladala lines move around with low passenger demand, these shall be preserved.

16.05 Changes in traffic level of streets affected by permanent re-routing of passenger vehicles

The reorganization of general traffic in the CBD streets shall affect the levels of road capacity. In this context, the major impact will happen on Morogoro Road between Sokoine Drive and Bibititi Road, where the traffic of passenger vehicles will be removed.

Notwithstanding, in most of the downtown road network there is a plan for the conversion of lanes into one-way traffic, completely changing the patterns of access and exit. Whilst this shall bring about an increase in the flow of some roads and decrease in others, it is a minor impact, given that the traffic in the central region is already quite heavy and this condition shall not be changed in any case.

16.06 Increase in the level of bicycle use

One of the objectives of the DART system project is to promote non-motorized transport (bicycles and pedestrians).

Almost all standard sections specified in the project will have exclusive cycle lanes, excepting Msimbazi Street and the section of Morogoro Road between the City Council Station and Bibititi Road, where cyclists will be able to move freely on bus lanes.

The promotion of NMTs will constitute a positive impact of the Project, bringing indirect benefits to the general traffic.

16.07 Increase of the barrier effect along the corridor

With the implementation of DART, drivers and pedestrians will face difficulties in crossing the roads where segregated bus lanes are fitted.

The main crossroads will be subject to geometric and traffic light adjustments, in order to eliminate bottlenecks. Some barriers will be put at the end of a few local streets parallel to the corridors, in a way to drive the traffic flow to U-turns and enable crossings only at signalized intersections.

From the viewpoint of pedestrian safety on casualties, the restrictions on crossing and accessibility to BRT from some specific stops with high demand represent a positive impact in relation to the current situation. Alternatively, from the point of view of car accessibility they represent a negative impact, whose intensity, nevertheless, tends to decrease as the motorized population familiarizes itself with the new traffic schemes.

I.17 Potential impacts on public transportation:

17.01 Improvement in the quality and capacity of public mass transport serving the IAI population

This is, concurrently, the main objective and main positive impact of the DART operation.

The implementation of a special physical infrastructure, formed by corridors, stations and terminals aimed at the segregation of many different types of vehicles and to transport hierarchy, together with the concession of the DART operation, which includes the replacement of the current "daladalas" by a brand new circulating fleet (articulated buses on trunk lines and micro-buses in feeder services), will result in system rationalization and will increase the capacity of passenger transport.

The final results expected with the implementation and operation of such a system are the increase in users' mobility, comfort and safety.

17.02 Re-routing and rationalization of "daladala" lines

The rationalization and the change of the remaining daladala routes will not imply a smaller coverage of the public transport service. As analysed in Section 2.4, the level of service of public transport will not be reduced in any of the city's regions; much to the contrary, it will be expanded in many cases.

Without any service loss, the rationalization of routes shall imply an increase in some users' trip to the nearest line. Likewise, in many cases the rationalization will result in a reduction in the users' walking distance.

Another effect of the rationalization of the remaining daladala lines will be the improvement in the reliability and quality of services, given that, with a smaller amount of lines, frequencies can be reduced.

17.03 Reduction of total bus / kilometers in the IAI

The implementation of the first phase of DART Project will result in a significant reduction in the total number of buses / kilometre covered in the city of Dar es Salaam. On average, the articulated buses on the corridor will cover 44,532 km / day, with a passenger-kilometre index (PKI) of 9,4. Moreover, the micro-buses of the feeder service will cover 51,211 km / day on average, with a PKI of 5,5.

The articulated buses will travel at an average speed of 23 km/h, and the micro-buses in feeder lines at 17 km/h, which make up a total of 4,750 operating hours / day for the whole fleet of the concessionaire operator.

On the other hand, 33 daladala lines will be shut down. In these lines, nearly 460,000 passengers are transported per day at an average speed of 17 km / hour with a passenger / km index (PKI) of 1.2 Therefore, the elimination of these 33 lines will result in a reduction of 26,600hours / day of daladalas in operation.

Apart from that, in the remaining daladala lines, the total number of passengers / day to be transported will drop to 1,170,000. Assuming that the current average speeds will maintain at 17 km/h with an improvement near of 28% in the PKI as a consequence of the rationalization of such service, it may be estimated that the operation of these lines will result in 56,300 hours / day of daladalas in operation.

From the above it is possible to gather that with the operation of DART Phase 1, the total number of bus hours / day in operation (including articulated buses, feeder micro-buses, daladals and the remaining modes) will be of 61,100 hours. Comparatively, it is estimated that the present fleet of 5,800 daladals operates on average 82,900 hours / day.

Consequently, there will be a significant reduction, of nearly 22,000 in the total amount of hours / bus / day in operation in Dar es Salaam.

Similarly, the total amount of buses / km covered per day shall drop from 1,408,000 km/day (exclusively daladals) to 1,133,000 km / day (considering articulated buses, feeder micro-buses and daladals, altogether).

The reductions in time and kilometers covered by buses in town will bring about benefits proportional to the traffic flow.

17.04 Reduction of operational costs per passenger / kilometer in public transport along the DART corridor

The overall reorganization of Dar es Salaam's public transport system will provide a substantial increase in the passenger transported / kilometer index (PKI), in addition to the use of modern vehicles with lower operating costs and the rationalization of routes and itineraries.

Such improvements will result in a large reduction in the costs of public transport services per passenger / kilometer in town, assuring, in the medium and long terms, more economical fares to users.

I.18 Potential impacts on urban structure:

18.01 Induction to changes in land use patterns within DAI

In general terms, the mass transport system offers potential for increasing the population density in the area within the influence range of stations (from 300 to 500 m). The increase in accessibility may cause changes in the patterns of land use and occupation in the areas bordering on or next to the corridors.

However, based on the analysis of the urban structure along the DART corridors at Phase 1, the areas at DAI where the project would have the greatest potential for promoting changes in the patterns of land use and occupation are those next to the stations planned for the Morogoro Road section along Ubungo e Kimara, where the use, predominantly residential, sprawls in a disperse pattern, among empty spaces. In this section the DART operation may

cause the reduction of empty spaces for residential purposes and an increase in the density of built-up areas.

The section between Ubungo and Urafiki is defined by the existence of industries and popular dwellings. It is unlikely that the BRT implementation will cause changes in these already consolidated occupation patterns.

In the section between Morogoro Road between Magomeni and Urafiki (entrance to Mabibo), the project implementation may cause the eviction of people that use the ROW for business and services. The shops and stores existing along this section also use Row for the display of their products, and shall lose this space. There is a potential for vertical development in this section, since the buildings lack the structures or slabs that enable easy building of one or more floors. Quite likely the business owners will promote improvements in the buildings, or possibly sell their outlets. At any rate, there would be no significant impacts on the urban structure.

In the Morogoro Road section between the Msimbazi River and Magomeni there is a steep grade separation between the road and the nearby residential areas, located at a higher level. Even if the carriageway is widened, this configuration will not be changed by the project, so that the gain of accessibility in this section will be most unlikely to cause changes in land use and occupation of the neighboring areas. In addition to the barrier effect, these areas are quite close to the city centre, with consolidated residential use and medium standard constructions in planned plots, different from the residential occupation in the section between Magomeni and Urafiki (entrance to Mabibo), with spontaneous and irregular plots and low standard construction dwellings.

On Kawawa Road between River Sinza and the area neighbouring Kinondoni Road, the border use is predominantly commercial, and the residential occupation along the DAÍ is similar to that in the section of Morogoro Road between River Msimbazi and Magomeni. Between Kinondoni and the Ali Hassam Road neighbourhood there is a vague definition as to the bordering uses, owing to the fact that in recent times Kawawa Road was subject to improvements which resulted in some "open wounds" in the urban fabric, causing land occupation with some institutional uses. It is possible that the DART implementation may promote the consolidation of business and institutional uses, without changing; nonetheless, the residential pattern of DAÍ's remaining areas.

In the central area it is unlikely that the project will affect the patterns of land use and occupation, since DART will pass through consolidated, chiefly mixed use areas with vertical development. At most there might be some marginal changes in Kaukauna Front area, with the removal of small outlets along the waterfront. The adjacent areas, featuring institutional, commercial and port uses (ferry-boat), will remain as they are. Notwithstanding, it must be observed that the project implementation may serve as an inducement factor in the urban and landscaping improvements to this section.

18.02 Increase in real estate values

The most important contributing factor in the definition of real estate values in cities with market economy is location.

Well located areas are those near tertiary sector centers or sub-centers, where jobs and urban facilities are concentrated (infrastructure and services in general). These areas are occupied by upper class districts. On the other hand, those areas far from these attractive sites and which are badly served by urban infrastructure are the least valued. As a rule, the further and less supplied an area is, the smaller the property prices are, and therefore, the smaller that resident population's income is.

The implementation of road improvements or the creation of mass transport lines tends, in general, to induce an increase in the property value in the adjacent or nearby areas, owing to an increase in accessibility. This may happen not only in nearby areas but also far from the attractive sites. Road projects that favor individual transport tend to encourage elitism or gentrification, that is, the substitution of families traditionally resident in the areas adjacent to the road works for families with higher incomes or automobile users.

However, in the case of mass transport projects based on buses, the possibility of two different types of increase in property value must be considered:

- The increase in the value of properties immediately adjacent to bus ways, for non-sophisticated, tertiary sector uses (traditional street trading); the shops, stores and services which depend on the turnover of customers, are likewise, less sensitive to the environmental impacts resulting from the changes implemented.
- The increase in the value of properties located in the area directly influenced by the corridor, for residential uses of low and medium standards, as a result of the availability of easy access of users to the transport system.

It should be highlighted that the implementation of bus ways takes place generally on roads that already have commercial corridor features. In such cases, the impact on the value will always be limited, in comparison to the creation of a new bus line for a distant, low-value area, where unused land still exists. Such event may cause a real "leap" in the property values of the affected area.

In the case of the current project, an increase in the property values along all the road links, as per the described trends, is expected. However, in the central area the potential for an increase shall be rather limited, owing to the consolidated occupation. The DART section where the increase in value will be more substantial is on Morogoro Road between Ubungo and Kimara, further from the city center, and presently less accessible.

18.03 Risk of localized real estate depreciation

This potential impact may be local, in those areas the environmental impact of the BRT system operation are noticed with greater intensity.

The increase in noise levels and the emission of atmospheric pollutants at the terminal and station areas, where a concentration of vehicles and people is expected, may cause a depreciation of some adjacent residential properties.

Aside from that, the implementation of road and landscape improvements and new architectural structures (terminals, stations) along the corridors will tend to minimize or even reverse property depreciation.

18.04 Potential for increased urban density within the DAI

Among the likely results of an increase in population density due to the operation of DART Phase 1 is the occupation of fields and empty lots, on the one hand, and vertical development, on the other. These are both forms of intensification of land use value; nonetheless, whereas the first process implies a concentration of the constructed area, whereas, the second implies the intensification of land occupation potential.

In theory, the increase in density, either through the concentration of the constructed area or through vertical development will have a positive impact. Both provide maximum utilization or social usufruct of the infrastructure and available services along the transport links, contributing to the economy of public resources. The reverse of this process is the centrifugal urban sprawl which promotes an expansion of the urbanized area and subsequently, the necessity to expand the network of public services (see impact 18.04).

In the specific case of the current project, the areas with a stronger probability of an increase in vertical development density are the business and residential areas bordering on Morogoro (as far as Ubungo Terminal) and Kawawa (as far as Ali Hassam Road) Roads. The currently empty areas along Morogoro Road between Kawawa Road and Kimara may experience an increase in land value and land fragmentation.

18.05 Increased potential for radial expansion of the urbanized area

Another likely effect of roadworks and the implementation of the BRT systems is an increase in the potential for centrifugal urban sprawl. This inducement potential is a natural consequence of an increase in accessibility to certain areas in town. With the reduction of travel times in the downtown-uptown radial trips, the relative distances decrease, in a way that new areas, formerly relegated to a second plan by the market, become part of processes such as land fragmentation and increase in property value.

In large cities of third world countries, where historical urban structures have taken on an essentially radial organizational configuration of the downtown-outskirts type, the implementation of the BRT systems has no power to change the urban reality defined by market rules. This is determined by the fact that the poorest population lives, as a rule, in districts where real estate and rents are cheaper, far from the central business district. The so-called 'infinite growth of urbanized area' model, although capable of providing the poor with conditions of access to housing (regardless of the quality of human settlements), has proven to be costly to public funds, because it requires the continuous expansion of networks and public services and demands high maintenance costs.

Differential increases in accessibility may worsen this trend, encouraging a rise in property prices and rents in some of the districts now served by more efficient transport systems. This could result in the eviction of those families who cannot afford the new housing costs, to places even farther or more peripheral to the city centre.

As already mentioned, with the implementation of DART Phase 1, such impact along Morogoro Road link, between Kawawa and Kimara, where there are still many empty spaces may occur.

Such effects can only be compensated by social and urban policies complementary to transport sector policies which allow families to stay put, or else policies that offer low cost dwelling units in locations, fairly served by transport, services and jobs.

In this context, the development of new tertiary sector sub-centers is a policy recommended to reduce the population's functional dependence on the jobs offered in the traditional CBD. Such a measure, though, cannot be exempt from the previously mentioned policies, since the development of sub-centers can also trigger an increase in property values and the eviction of lower income families.

18.06 Development of new urban sub-centers or expansion of existing ones

The corridor implementation can encourage significant urban and economic development along Morogoro and Kawawa Roads and Downtown, creating new hubs and triggering the development of other areas. The urban landscape can be improved by the application of other concepts of urban space use, since the corridor will promote changes predominantly in its area of influence.

The implementation of Phase I of the corridor makes possible the development of certain areas, inducing the creation of leisure and social activity facilities. Seven (07) important urban hubs along the proposed First Phase DART System that can be enhanced by its implementation can be identified:

Hub 1 – Kivukoni Front, including the Fish Market and the Water Front. The impact of the implementation of the Kivukoni Terminal, near the Fish Market, and the new landscape and reconfiguration of sidewalks along the waterfront, can induce new investments along this area, which has great potential for leisure and tourist activities, due to the existence of major hotels and the proximity of the port.

Hub 2 – Central Business District, including City Council, Samora Avenue, and Libya Square – The implementation of DART can bring new investments to these areas and the reorganization of its business characteristics will require also the reorganization of public space.

Hub 3 – Kariakoo - Kariakoo area is a market activity area. The Terminal will be located on the extreme south of this area and can serve to organize public areas, integrating the activities that were displaced by the removal of the street vendors that were occupying the area.

Hub 4 – Magomeni - The intersection of Kawawa and Morogoro is one of the major road network nodes in the City. With the implementation and expansion of the System, this importance will increase, and the area will become a significant hub for the whole city. This is also true of Magomeni Market, which can be enhanced thanks to its proximity to the Magomeni Mapipa Station.

Hub 5 – Morocco - The Morocco Terminal is an major transfer point of the DART System. For this reason, it may become an attractive location for businesses, especially for services and small shops.

Hub 6 – Ubungo – This is an important point for the transportation system in Dar Es Salaam. It is strategically located near the junction with Nelson Mandela/Sam Nujoma Rd and it encompasses the present Upcountry Terminal. This is a potential area for the development of mixed activities, and can be converted into a focal point of the city.

Hub 7 – Kimara – Due to the implementation of a Terminal, this area can be enhanced by the creation of new businesses and the development of existing ones.

I.19 Potential quality of life impacts:

19.01 Reduced travel time for corridor users

One of the main benefits of the implementation of DART system will be the reduction in travel times. This will arise mostly from:

Implementation of a suitable road infrastructure, which will enable the segregation of different types of traffic (including cycle lanes), and intersection adjustments, mostly for the reduction in waiting time at traffic lights;

Construction of terminals and stations for transfers, and the implementation of an integrated fare system;

The valorization of a public transport system, with the use of high capacity buses in the trunk lines and medium capacity micro-buses in the feeder services (feeders), as well as the organization of the daladala lines;

An incentive to non-motorized transport systems (NMTs), such as bicycles.

The first two improvements will enable an increase in the mean speed of buses on the project roads to an average of 23 km/h. The second two aim at facilitating / speeding up transfers in the trips between different transport modes (walking, bicycles, daladalas, micro-buses and articulated buses).

As compared to the present situation, characterized by the low capacity of the daladala systems to respond to the rising demand, the implementation and operation of the DART system will provide a significant reduction in the downtown-uptown travel times, benefiting not only public transport users, but also the motorized population due to traffic segregation and the bus stops.

Simulations show that the current daladala travel time amounts to 4,269,917 minutes. The DART system operating at the morning peak hour, by articulated bus will be 3,910,685 minutes, which is equivalent to time savings of 2.6 minutes per trip, or 98,840,119 minutes per month. As to the overall time, which includes in-vehicle, fare payment, walking to the station and waiting time, the average economy per trip will amount to 1.3 minutes per trip, equivalent to 49,242,000 minutes per month, for the whole system.

19.02 Alteration of total cost of travel for corridor and feeder line users

The management of the public transport system in Dar es Salaam will be granted as a concession to a private company, whose revenue shall derive from the collection of fares.

It is therefore expected that the operation of a modern and more efficient transport system will necessitate ticket prices higher than the fares currently charged by the daladala operators. Nevertheless, this price will have to be based on the average users' income possibilities, so that the running of the service becomes economically feasible.

Moreover, as described above, there is plan for the adoption of an integrated system between the trunk lines and feeder services in order to establish a unified system of fares at terminals and stations. The purpose is to provide a reduction passenger time at transfers and an overall economy of total travel time.

The beginning, the DART operation shall gradually introduce the population to the gains in the cost-benefit relationship, reducing the likely impact of the initial rise in transport fares. The overall consideration being that the increase in users' mobility represents, in theory, more available time for the practice of other profitable activities.

19.03 Reduction of total direct employment necessary for operation of the public transportation system of the IAI

The modernization and reorganization of the public transport system in Dar es Salaam will constitute a social negative impact in the reduction of operational people employed.

The current demand data shows that the daladala fleet in operation amounts to 5,800 vehicles, with a total of 11,600 employees (drivers and conductors) per shift. Taking into account a two-shift operation, with one driver and one conductor per shift, the total number of employees amounts to 23,200.

After the elimination and relocation of lines, the remaining daladala fleet will drop to 4,380 (including the spare fleet). Considering a two-shift operation, with one driver and one conductor per shift, the following shall prevail:

- 1,420 vehicles will be taken out of operation;
- 5,680 drivers and conductors will loose their jobs.

According to data shown in Section 2.7, it is estimated that DART Phase I operation will generate a total of approximately 2.100 jobs. A significant part of these jobs may be filled by daladala drivers and fare collectors. However, many will need to seek other employment as a result of DART Phase I implementation. A specific program will seek to mitigate this impact ("Programme for the Compensation of People Involved in Daladala Operation" - Section 6.5, P.13) by assisting affected drivers and fare collectors with training and other income restoration strategies.

19.04 Increase in the potential for employment generation in the DAI as a result of increased density

The increase of land occupation density, caused by the operation of the DART system, may bring about a rise in the potential for job generation. This is a potential indirect impact arising out of impact 8.04, as the population increase at DAI tends to boost business and services, benefiting the urban economy as a whole. The simple fact that there are more people living or moving around DAI represents a potential economic advantage and rise in salaries, which can contribute, accordingly, to the improvement in the quality of life of the population directly affected.

19.05 Disturbances to local population as a result of commercial activity displacement

Local consumption patterns will have to be altered as a result of the displacement of a significant amount of commercial activities, particularly along certain segments of the DART Phase I alignments. Consumers who will have to alter their consumption patterns will find new alternative suppliers of goods and services and will need to adapt to a significantly altered distribution of commercial activity within some neighborhoods.

This impact will be more intense during construction and initial phases of project operation. However, it can be considered an impact of lesser importance in the global context, since alternative suppliers of goods and services will be rapidly selected by consumers in most cases.

19.06 Local alteration of noise level during operation

During the operation of the DART system there will be changes in noise levels along the corridors and on DAI local roads.

On the corridors and at the intersections the traffic-light adjustments (decrease in the number of phases) will reduce congestions, so that vehicles will travel at mean speeds higher than the current ones. As motors turn for longer periods at higher rotation bands, the levels of noise emission will tend to decrease.

In some sections of DART, such as on Msimbazi Street, there will be no significant change in the noise levels, since there are several daladala lines circulating there. Despite the substitution of such services by high capacity buses, with less frequency and fewer stops, the latter will continue to produce higher noise levels.

On the remaining DAI roads changes in the noise levels will result from the relocation of general traffic and daladala lines. Local residential streets, presently with little traffic, may be affected by an increase in vehicles, and in turn, the noise levels. On the other hand, roads with currently intense daladala traffic may benefit from the reduction or extinction of lines, as well as from the introduction of micro-bus lines (feeder services).

Broadly speaking, the most sensitive recipients to vehicle noise are schools, hospitals and residential areas quoted in the description of impact 8.07. As the ROW along Morogoro Road is wide, the landscape project shall include green areas aimed at reducing the level of noise. A similar measure may be applied to the area surrounding the Ubungo Terminal and the existing hospital.

19.07 Landscape alteration and urban requalification of areas adjacent to the corridor

After the end of the execution of DART Phase 1 and the implementation of the landscape project, the configuration of road links directly affected will have their landscape positively changed to the betterment of the general urban aspect of the area.

Such modifications will include the restoration of pavements, fitting of new structures (stations, terminals), cycle lanes and new urban fixtures, as well as an increase in trees and urban gardens. These improvements tend, in turn, to encourage the revalorization of public areas and buildings, currently run-down or degraded by the intense vehicle and pedestrian traffic.

On the other hand, it is possible that some open spaces planted with trees and currently used by the population as leisure areas may be eliminated or partially affected by the widening of the carriage sections.

A project for a high capacity transport system cannot be seen as a sector project. Indeed, urban plans and schemes shall be ordered by the City Council so as to intensify the project's positive aspects and to avoid negative ones.

19.08 Alteration in the number of traffic accidents

Some previous experiences of corridor implementation have shown that there is the possibility of an increase in the number of accidents involving pedestrians, buses and mixed traffic, at least in the first months after implementation. This is due to the fact that both pedestrians and drivers have to get used to the new configuration of traffic speed and signalling, especially in the cases of buses, which will operate in segregated lanes.

Such an impact, however, can be considered to be provisional and it is expected that after an initial adjustment period the level of accidents will decrease below the present rate.

I.20 Potential impacts on economic activities:

20.01 Induced change in commercial and service activities along the corridor

The start of the operation of the DART system tends, in theory, to increase the value of the areas bordering on the roads affected, for tertiary sector uses. The infrastructure, road and landscape improvements may constitute incentives, for the owners of business facilities along the corridor, to renovate and/or expand their buildings, or to transfer their outlets to other parties interested in the investment.

20.02 Increased potential for retail shops and service providers in the areas surrounding stations and terminals

Due to the concentration of people, the areas within the range of accessibility to stations and terminals (from 300 to 500 m) may become particularly interesting for tertiary sector uses.

Notwithstanding, the stations and terminals specified in the project are already located in places meant for public transport users. They contain daladala stops and concentration of people, business and service activities. Furthermore, as the project specifies the construction of business and service outlets within the terminals, part of the potential for the development of new business facilities will be absorbed by this supply.

As a result of these aspects, major changes, such as significant changes to the commercial pattern or changes to the types of activities existing in the surroundings of stations and terminals are not expected. The possibility of substitution of residential uses by commercial uses in some real estate units should not, however be excluded.

20.03 Potential income loss for daladala operators

The implementation of the DART system is concerned with the rationalization of the whole public transport system currently under operation, including the elimination of several daladala lines. One potential negative impact of the project is the loss of jobs and sources of income for drivers and conductors.

Presently the daladalas perform 41,100,000 trips per month. Once the DART system begins to operate, the articulated buses and feeders will perform 11,200,000 trips, reducing the amount of daladala trips to 30,000,000 per month (reduction around 28%). This reduction will cause the elimination of approximately 5,680 jobs, as per described in Impact 19.03 description.

I.21 Potential impacts on physical and social infrastructure:

21.01 Increase in the serviced area of key components of local social infrastructure

The operation of the BRT system will increase the users' mobility and there will be an indirect expansion of the area covered by some social services existing at DAI. In other words, it will be easier and faster for people living in areas previously very far from social services to now have easy access to such facilities.

Most of the social services of interest for the system users (schools, religious buildings and hospitals) are located in the central area, in the surroundings of Kivukoni Front and in two sections of Morogoro Road: between Bibititi Road and River Msimbazi, and between Kawawa Road and Kagera Street. There are also many schools and some religious buildings located at DAI along Kawawa Road.

I.22 Potential impacts on public finances:

22.01 Indirect increases in property tax revenue as a result of real estate value increases and intensification of economic activity along the corridor

An indirect effect of the implementation of the DART system, is the expected increase in property values and the boost to economic activities at DAI, mostly in the areas surrounding the terminals and stations. This process tends to promote a gradual increase in the tax revenues on real estate, supported by the Dar es Salaam City Council.

ANNEX 06

Social Impact Assessment included in the ESIA

ANNEX 06

Rap Disclosure and Consultation Meetings

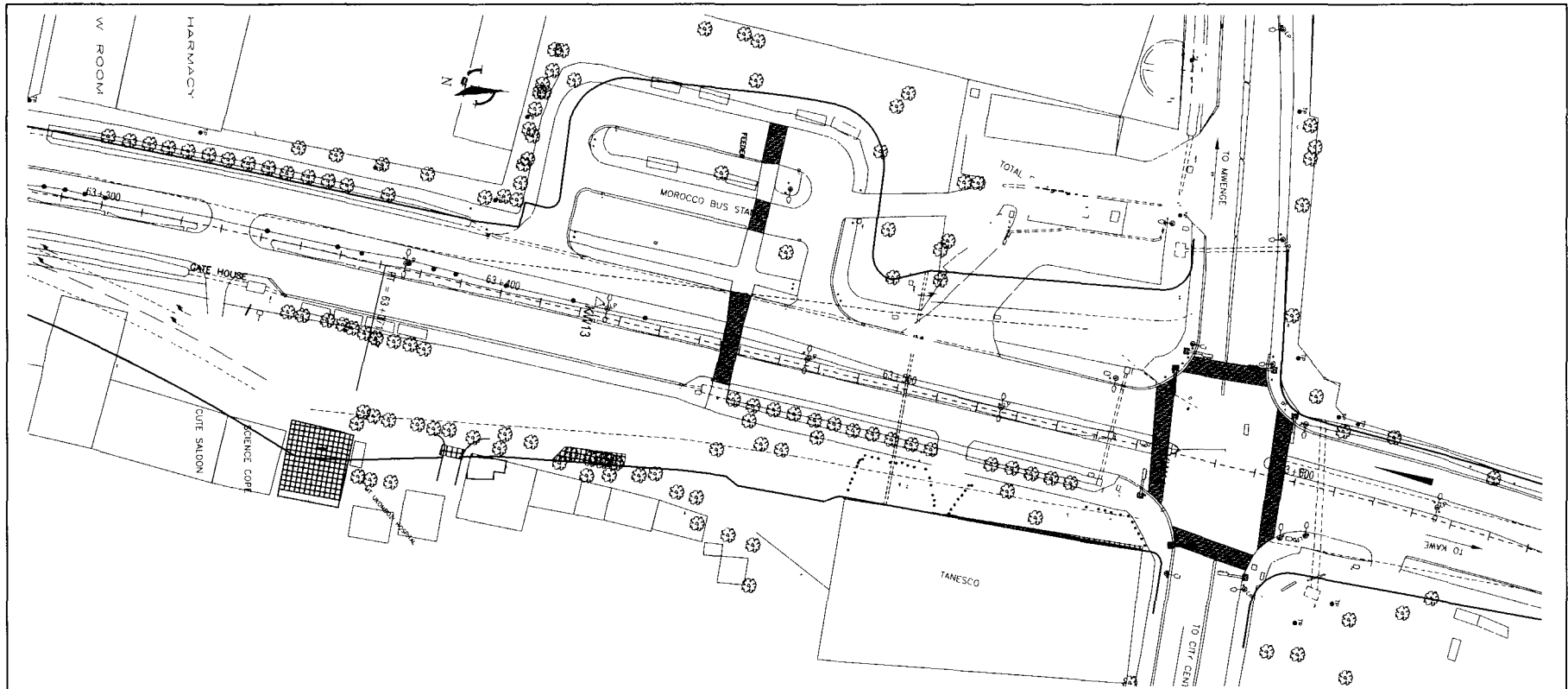
Date	Event
31/08/2007	<p data-bbox="522 704 905 736">RAP disclosure in News papers</p> <ul data-bbox="530 736 1349 895" style="list-style-type: none"><li data-bbox="530 736 1349 800">▪ Compensation Tables have been displayed at the DART Office and the two Municipalities (Ilala and Kinondoni)<li data-bbox="530 832 1285 895">▪ RAP Reports are accessible to the general public in the DART Office, TANROADS and the two Municipalities.
18/10/2007	<ul data-bbox="530 917 1349 1491" style="list-style-type: none"><li data-bbox="530 917 1349 1140">▪ A meeting was held with the Authorized Land Officers, Land Valuers, Engineers, Social Workers from the three Municipalities and Ward Executive Officers (WEO) from 17 Wards with affected properties (12 wards - Kinondoni and 5 Wards - Ilala) to discuss emerging issues after disclosure. Although there are no affected properties in the Municipality, Temeke Municipal officials were invited for awareness and capacity building.<li data-bbox="530 1172 1349 1289">▪ It was reported that affected persons with an exception of the Kariakoo Terminal site are very cooperative but requests the government to pay their compensation early so that they can start establishment in new areas.<li data-bbox="530 1332 1349 1491">▪ Meeting deliberations necessitated the formation of two task forces to address pressing issues and report back within seven days. The two task forces are:<ul data-bbox="596 1427 1063 1491" style="list-style-type: none"><li data-bbox="596 1427 1014 1459">- Gerezani resettlement procedure<li data-bbox="596 1459 1063 1491">- Procedures for paying compensation
24/10/2007	<ul data-bbox="530 1513 1349 1791" style="list-style-type: none"><li data-bbox="530 1513 1349 1704">▪ Authorized Land Officers, Land Valuers, Engineers, Social Workers from the three Municipalities and Ward Executive Officers (WEO) from 17 Wards with affected properties (12 wards - Kinondoni and 5 Wards - Ilala) met to receive "Task Force Reports". Temeke Municipal officials were invited again for capacity building.<li data-bbox="530 1736 1349 1791">▪ The two Task Forces formed on October 18th, 2007 reported on the findings. Payment procedures were presented and adopted.

Date	Event
29/10/2007	<ul style="list-style-type: none"> ▪ For security reasons payment will be based on bank transfers, an approach which is also favored by the majority of the affected. ▪ With regard to Kariakoo it was confirmed that all houses at Kariakoo Terminal site are built on government land which changed use from Residential to Terminal in 2002 after a consultative process of preparing "The Kariakoo New Redevelopment Scheme". ▪ The Title Deed holder is Tanzania Building Agency (TBA) which inherited the properties from Tanzania railways authority and Tanzania Harbors Authority which have changed mode of operation (privatized). ▪ Procedures are advanced to transfer the title deed from TBA to the DART Agency. DART Agency will pay compensation of properties and all other benefits as per World Bank Guidelines to TBA who will thereafter pay to their tenants who purchased the house after deducting the less the amount paid through tenant purchase scheme. ▪ TBA pledged cooperation on the timely removal of the properties in a smooth operation. ▪ After the meeting, a Grievance Committee was composed of the following core team members: Authorized Land Officer, Municipal Valuer, Municipal Engineer, Municipal Social Worker and DART Resettlement Specialist. Engineers from DART, Ward Executive Officers and other relevant officials will be co-opted depending on the issues raised by the affected persons. <ul style="list-style-type: none"> ➤ <i>Taking into consideration that DART is a project with indirect benefits compared to "CIUP", Grievances Committee are formed at Municipal level in the two Municipalities for efficiency and effectiveness.</i> <p>Under the leadership of the Ilala District Commissioner (a presidential appointee in charge of the Ilala District) a meeting was held to discuss how the TBA will administer compensations in a fair and transparent way. The TBA representative explained clearly on how property rights will be accorded to the occupiers. The meeting was well represented, including the Councilor Mayor, Authorized Land Officer, Municipal Valuer and an assistant representing the Municipal Director.</p>

Date	Event
30/10/2007	<ul style="list-style-type: none">▪ A Meeting with the Kariakoo terminal site residents chaired by the District Commissioner was held. Occupiers of TBA houses were informed on the mode of payment of resettlement values.▪ The DART agency will transfer compensation money to TBA to enable them to pay the occupiers immediately after the transfer of certificates is concluded.
03/11/2007	<ul style="list-style-type: none">▪ A Meeting to be attended by all affected persons to get clarification on compensation procedures and payment procedures. The Kinondoni District Commissioner will head the deliberations. The Municipal Mayor, Councilors from the 12 affected wards and Ward executive Officers will attend.▪ The MEDIA people have also been invited to attend. Through the meeting and media coverage we expect people will understand their rights and procure.
10/11/2007	<ul style="list-style-type: none">▪ A similar meeting to the above meeting of 03/11 will be held on Saturday 10th November.

NB:

1. The last two meetings intends to explain the procedures but also prepare the affected persons to cope with the changes.
2. After the two meetings MEDIA communication will continue to clear doubts. If necessary another meeting will be held if it is very necessary to do so.



LEGEND

KEY	DESCRIPTION	KEY	DESCRIPTION
1	BOUNDARY	11	EXISTING WALL
2	PROPOSED WALL	12	EXISTING FENCE
3	PROPOSED FENCE	13	EXISTING ROAD
4	PROPOSED ROAD	14	EXISTING DRIVEWAY
5	PROPOSED DRIVEWAY	15	EXISTING SIDEWALK
6	PROPOSED SIDEWALK	16	EXISTING CURB
7	PROPOSED CURB	17	EXISTING PAVEMENT
8	PROPOSED PAVEMENT	18	EXISTING ASPHALT
9	PROPOSED ASPHALT	19	EXISTING GRAVEL
10	PROPOSED GRAVEL	20	EXISTING SAND
21	PROPOSED SAND	22	EXISTING GRAVEL
23	PROPOSED GRAVEL	24	EXISTING SAND
25	PROPOSED SAND	26	EXISTING GRAVEL
27	PROPOSED GRAVEL	28	EXISTING SAND
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31	PROPOSED SAND	31	PROPOSED SAND
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49	PROPOSED SAND	49	PROPOSED SAND
50	PROPOSED GRAVEL	50	PROPOSED GRAVEL

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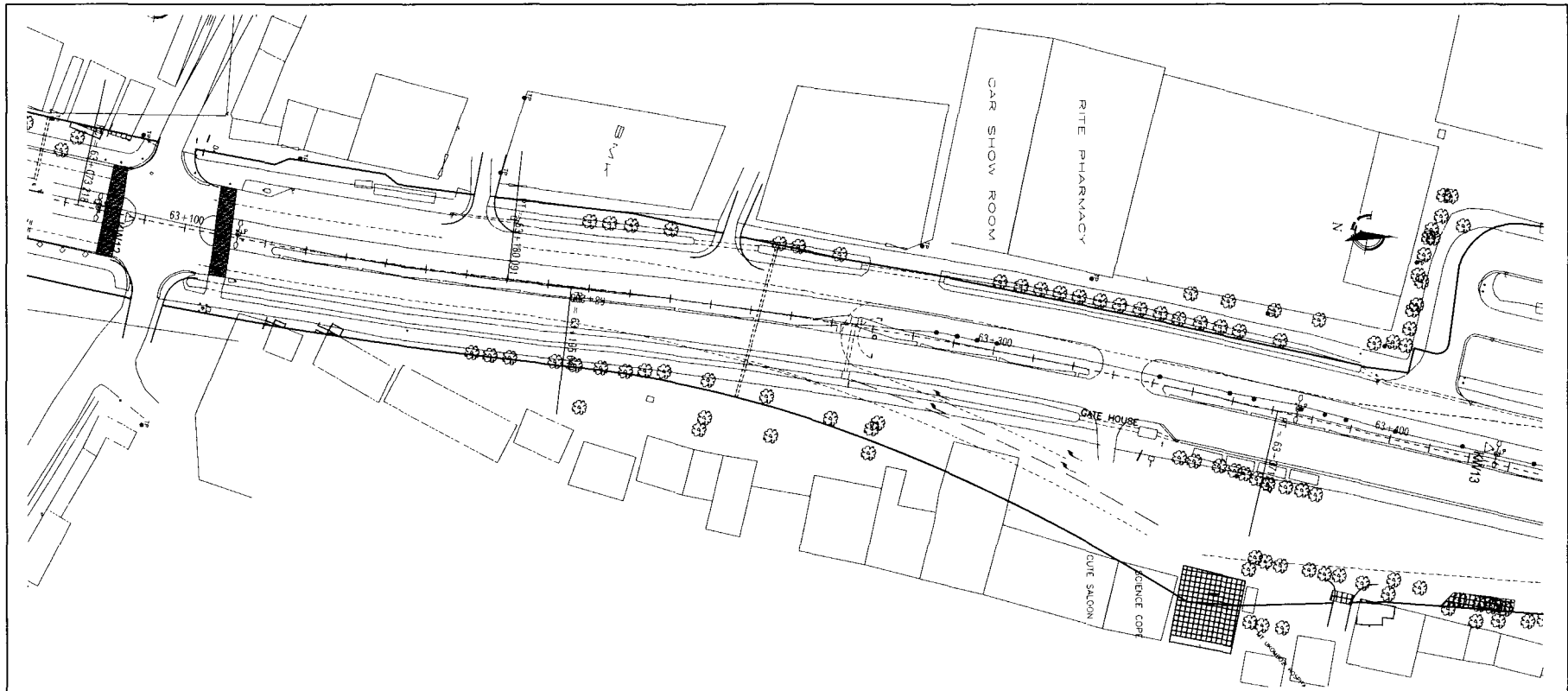
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Client's approval
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Revision Reference			
No.	Date	Description	Rev by

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 Checked by Z.T. Marengi
 Approved by
 Scale 1:1000

Project: **DAR RAPID TRANSIT (DART)**
 Subject: **LAND ACQUISITION IDENTIFICATION PLAN
 KAWAHA ROAD**
 Date: May 2007
 Drg No: T301-CR-RD-LAP-35



LEGEND

KEY	DESCRIPTION	KEY	DESCRIPTION
1	10' FT. WALL	1	WALK WAY
2	CONCRETE	2	CONCRETE
3	ASPHALT	3	ASPHALT
4	GRAVEL	4	GRAVEL
5	GRAVEL	5	GRAVEL
6	GRAVEL	6	GRAVEL
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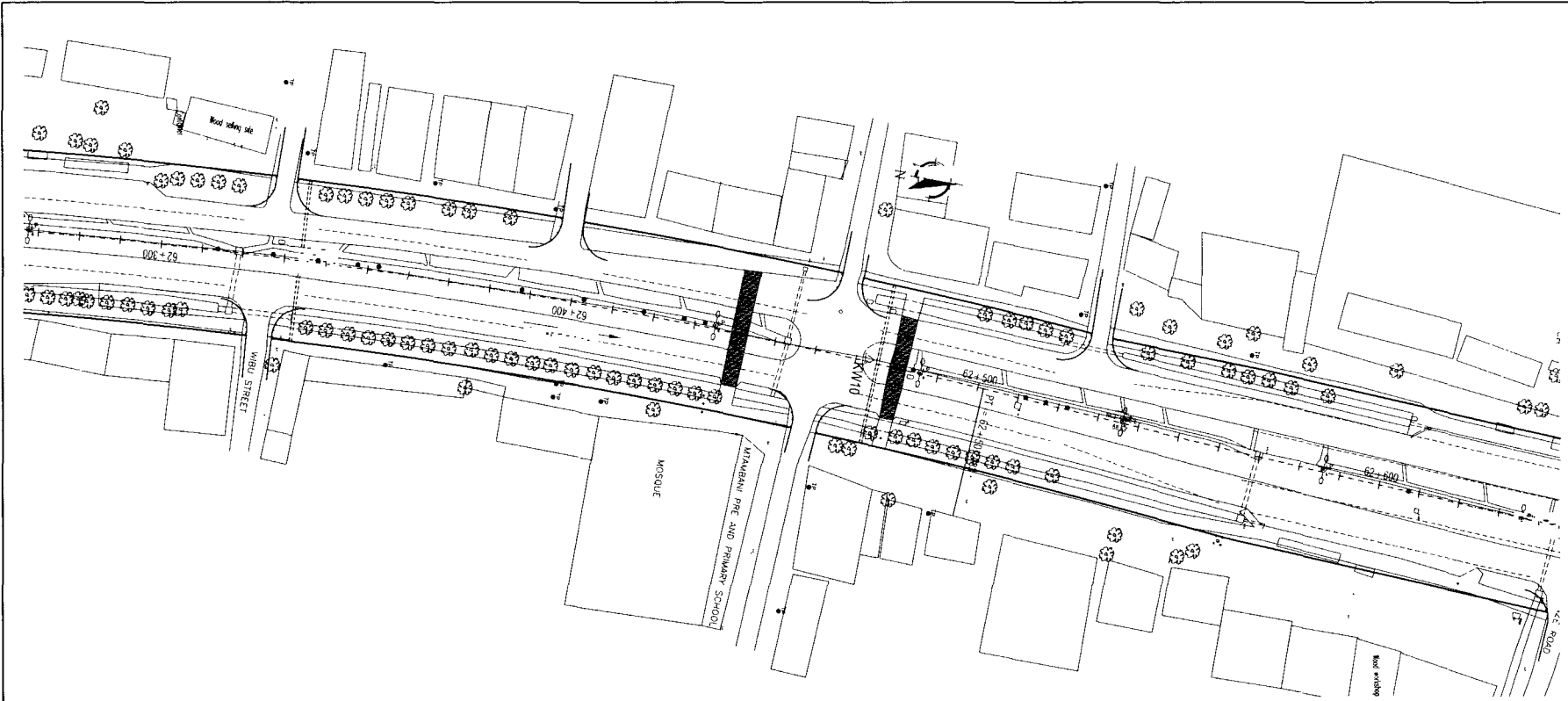
Client's approval
 SENIOR ENGINEER

Revision Reference			
No.	Date	Description	Rev by

located by
 Surveyed by: G. Mshali
 Designed by: H. Shoo
 Drawn by: J. Uno
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 Approved by:
 Scale: 1:100

Project: **DAR RAPID TRANSIT (DART)**
 Subject: **LAND ACQUISITION IDENTIFICATION PLAN
 KAWAWA ROAD**
 Date: May 2007
 Drg No: I301-CE-RD-LAP-34

REV



LEGEND

KEY	DESCRIPTION	KEY	DESCRIPTION
1	BOUNDARY	10	PROPOSED ROAD
2	EXISTING ROAD	11	PROPOSED SIDEWALK
3	PROPOSED ROAD	12	PROPOSED SIDEWALK
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39	PROPOSED ROAD	48	PROPOSED SIDEWALK
40	PROPOSED ROAD	49	PROPOSED SIDEWALK
41	PROPOSED ROAD	50	PROPOSED SIDEWALK

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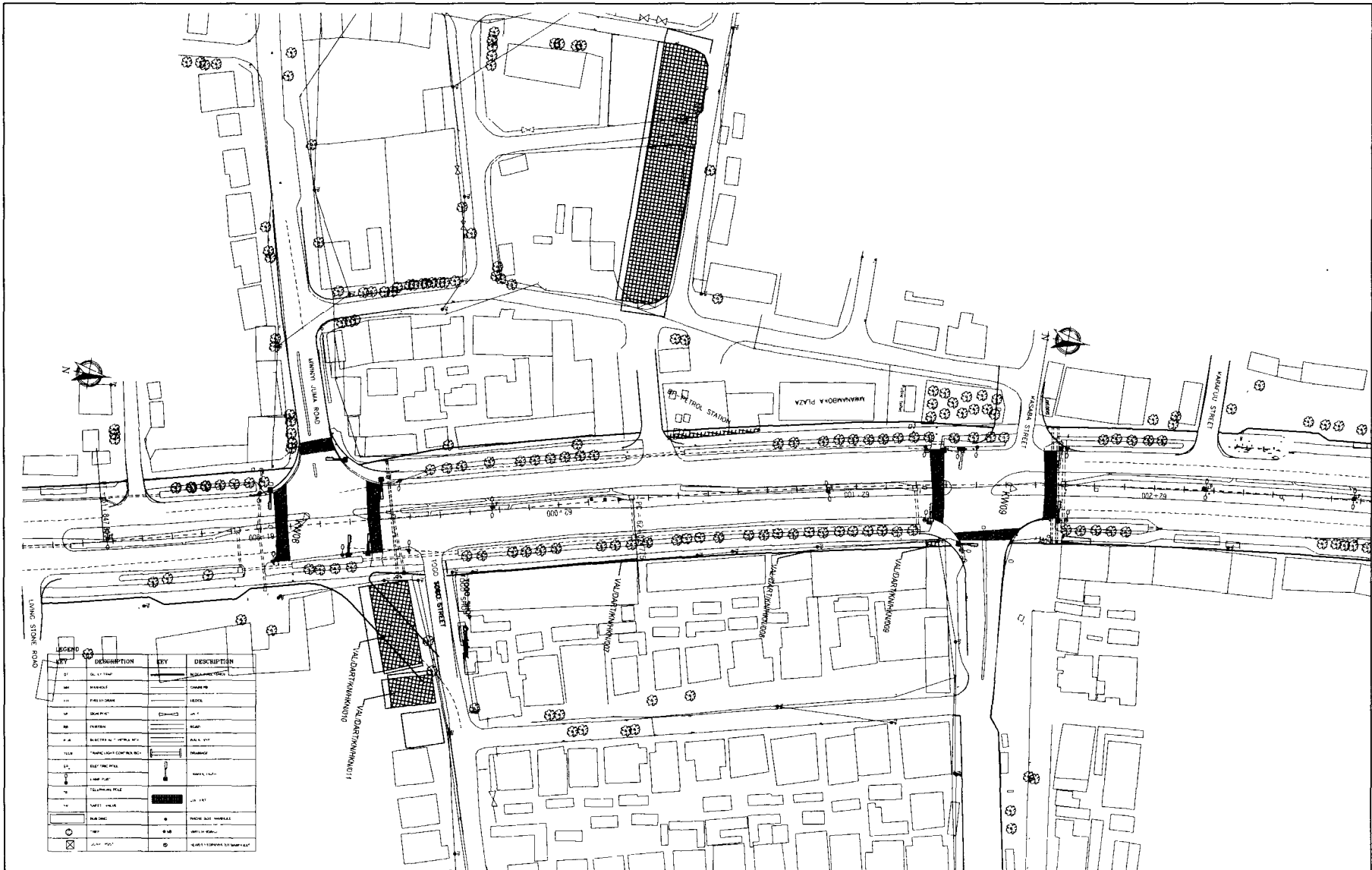
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
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No.	Date	Description	Rev by

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Designed by H. Shoo	Subject
Drawn by J. Uno	LAND ACQUISITION IDENTIFICATION PLAN KAWAWA ROAD
Checked by Z.T Marenga	Date
Approved by	June 2007
Scale 1:100	Drng No. T301-CE-RO-LAP-32



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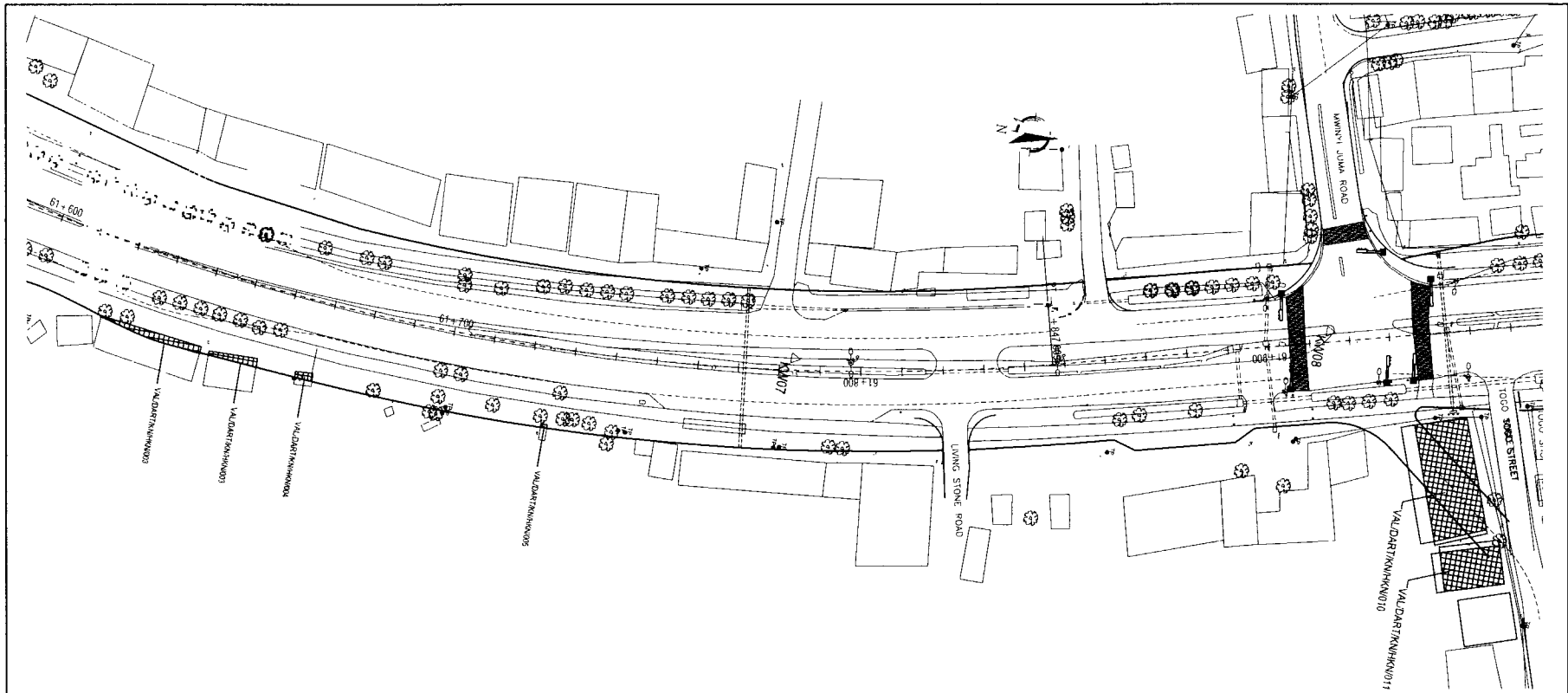
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located by	Project	
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Designed: H. Shoo	Subject	
Drawn by: J. Uno	LAND ACQUISITION IDENTIFICATION PLAN	
Checked by: Z.T. Marenga	KAWAWA ROAD	
Approved by:	Date	June 2007
Scale: 1:100	Drg No	T301-CE-RD-LAP-31



LEGEND

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78	GRAVEL	—	GRAVEL
79	GRAVEL	—	GRAVEL
80	GRAVEL	—	GRAVEL
81	GRAVEL	—	GRAVEL
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99	GRAVEL	—	GRAVEL
100	GRAVEL	—	GRAVEL

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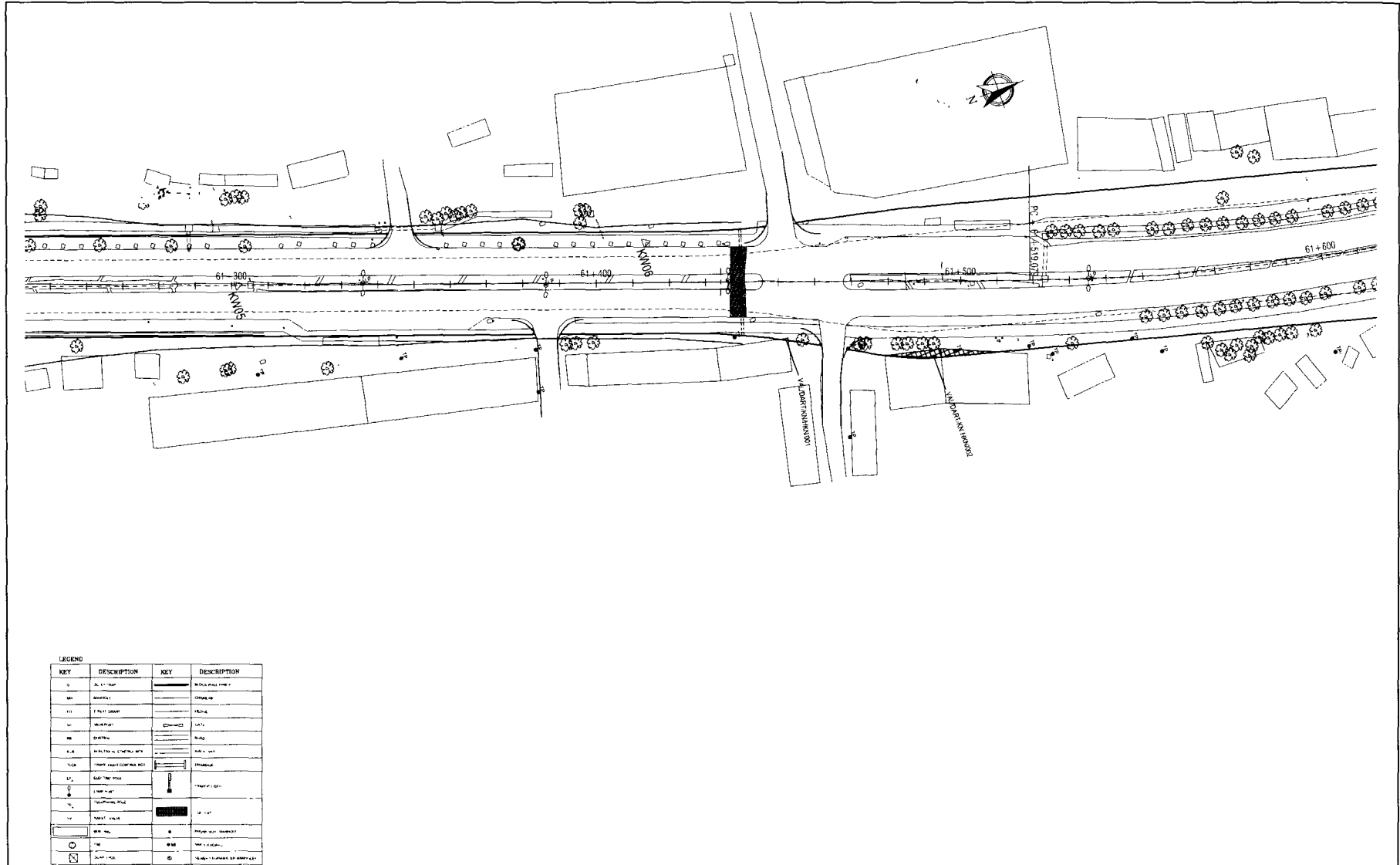
Client's approval
 SENIOR ENGINEER

Revision Reference:

No	Date	Description	Rev by

located by
 Surveyed by G. Mafak
 Designed H. Shoo
 Drawn by J. Uno
 Checked by Z.T. Maringe
 Approved by
 Scale 1:500

Project
DAR RAPID TRANSIT (DART)
 Subject
 LAND ACQUISITION IDENTIFICATION PLAN
 KAWAWA ROAD
 Date June 2007
 Drg No T014-CE RD LAP-01



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		SENIOR ENGINEER	No	Date	Description	Rev by	Date: June 2007 Drg No: T301-CE-RD-LAP-23 Scale: 1:500 REV